



UPPER UWCHLAN TOWNSHIP
BOARD OF SUPERVISORS MEETING
AGENDA
TUESDAY, AUGUST 12, 2025

6:00 p.m.

LOCATION: Township Building, 140 Pottstown Pike, Chester Springs PA 19425

I. CALL TO ORDER	Packet Page #
A. Salute to the Flag	
B. Moment of Silence	
C. Inquire If Any Attendee Plans to Audio or Video Record the Meeting	
D. Executive Session held July 30, 2025 re: Legal matters	
II. LAND DEVELOPMENT	
A. Byers Station Parcel 5C Lot 2B Amended PRD Plan ~ Phase 2 – Consider Approval	2
III. APPROVAL OF MINUTES: July 21, 2025 Board of Supervisors Meeting, Conditional Use Hearing	186
IV. APPROVAL OF PAYMENTS	190
V. TREASURER'S REPORT	214
A. Jeff Kowalczyk, Barbacane Thornton ~ 2024 Audit Report	
VI. SUPERVISORS' REPORT	
A. Police Liaison Report	
B. Calendar:	
August 31, 2025 1:00-4:00 p.m. Upland Farm Farmhouse Museum is Open ~ hosted by the Historical Commission	
September 1, 2025 Township Offices Closed ~ Labor Day	
September 7, 2025 4:30-7:00 p.m. Summer Concert #3 - Live Music by The MoonDawgs at Upland Farm Park ~ hosted by the Park & Recreation Board	
September 9, 2025 4:00 p.m. Semi-Annual Joint Boards & Commissions Workshop	
September 9, 2025 7:00 p.m. Historic Architectural Styles of Chester County ~ Lecture by Architectural Historian, Robert J. Wise, Jr. ~ hosted by the Historical Commission	
September 15, 2025 7:00 p.m. Board of Supervisors Meeting	
Yard Waste Collection Dates: August 20, September 10, September 24	
Do not use plastic bags as these materials are composted. Place materials curbside the night before to guarantee collection.	
VII. ADMINISTRATION REPORTS	247
A. Township Engineer's Report	251
B. Building and Codes Department Report	---
C. Police Chief's Report	253
D. Public Works Department Report	
VIII. ADMINISTRATION	
A. Conditional Use Application ~ 705 N. Reed Road ~ Consider Approval	255
B. General Obligation Bond Issuance ~ Adopt Ordinance to Authorize Incurrence of Debt	263
C. Technical Specifications for Traffic Signals, Revised January 2025 ~ Adopt Ordinance	288
D. 2026 Pension Plan Minimum Municipal Obligation (MMO)	318
IX. OPEN SESSION	
X. ADJOURNMENT	

**BEFORE THE BOARD OF SUPERVISORS
UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY, PENNSYLVANIA**

**APPLICATION OF PROSPERITY PROPERTY INVESTMENTS, LLC FOR
REVISED AMENDED FINAL PLANNED RESIDENTIAL DEVELOPMENT APPROVAL
FOR PARCEL 5C – LOT 2B OF THE BYERS STATION
PLANNED RESIDENTIAL DEVELOPMENT**

DECISION AND ORDER

BACKGROUND

A. In August 1998, Bryn Coed Farms submitted an application to Upper Uwchlan Township (the "Township") for tentative plan approval under the Township's Planned Residential Development Ordinance then in effect and Article VII of the Pennsylvania Municipalities Planning Code for property situated at the intersection of Route 100 and Byers Road in the Township known as the Byers Station Property (the "Property").

B. On June 7, 1999, the Township granted tentative planned residential development approval (the "Tentative Approval") for development of the Property as a planned residential development (the "Byers Station PRD"). The Tentative Approval approved a Tentative Plan that included six parcels identified as Parcels 1, 2, 3, 4, 5C and 6C.

C. On July 21, 2003, the Township granted final PRD approval for Parcels 1, 2, 3 and 4 of the Byers Station PRD as depicted in the approved Tentative Plan ("2003 Final Approval"). The 2003 Final Approval did not grant final plan approval for Parcels 5C and 6C of the Tentative Plan.

D. On May 23, 2016, Byers Residential Acquisition Limited Partnership and Byers Retail Acquisition Limited Partnership, then equitable owners of Parcel 5C, submitted an application for final PRD approval for a portion of Parcel 5C depicted in the application and referred to as Lot 1 ("the Lot 1 Application"). The Lot 1 Application proposed 121 residential townhouse units on Lot 1. The Lot 1 Application did not seek final PRD approval for the part of Parcel 5C depicted in the Lot 1 Application as Lot 2, consisting of approximately 13.439 acres fronting on Station Boulevard between Pottstown Pike and Graphite Mine Road.

E. The Township granted approval of the Lot 1 Application by Decision and Order dated March 17, 2017, as amended by Decision and Order dated May 15, 2017. The Lot 1 Application Decisions did not grant final approval for Parcel 5C, Lot 2. Lot 1 has been developed by Toll PA, II, L.P. with a 121-unit townhome community known as Villages at Chester Springs (formerly referred to as the Villages at Byers Station.)

F. On February 16, 2018, Byers Retail Acquisition Limited Partnership ("Byers Retail") filed with the Township an application for Final Approval of Parcel 5C, Lot 2, depicting approximately 81,000 square feet of commercial development on Lot 2 (the "2017 Commercial Plan"). Byers Retail chose not to request final approval of the 2017 Commercial Plan from the Board of Supervisors.

G. On April 1, 2019, Byers Retail filed an alternative final PRD plan for Lot 2, depicting 67 residential townhouse units on the east side of Lot 2 and 15,000 square feet of commercial development on the west side of Lot 2. Byers Retail sought approval of the Alternative Plan.

H. After advice by the Township that Lot 2 was limited to 55 residential units by the Tentative Approval, Byers Retail revised the Alternative Plan to depict 55 townhouse units.

I. The Alternative Plan was also revised to depict two commercial pad sites on the western side of Lot 2, one pad on the northwest corner depicting a one and one-half story retail building with a floor area of 2,793 square feet; and a second pad on the southeast corner depicting a two story building containing 2,000 square feet of restaurant and 9,980 square feet of retail on the first floor and 11,800 square feet of office on the second floor.

J. The Alternative Plan also proposed a subdivision of Lot 2 into two lots, Lot 2A comprising 9.468 acres containing the residential development; and Lot 2B comprising 3.971 acres containing the commercial development.

K. The Alternative Plan was titled "Amended Final PRD Plans for Byers Retail Acquisition , L. P. – Final Plan for Lot 2A & 2B of Parcel 5C" prepared by Bohler Engineering, dated March 25, 2019, last revised August 16, 2019, consisting of 42 plan sheets (the "Alternative Final Plan").

L. The Board approved the Alternative Final Plan in a Decision and Order approved on October 14, 2019 ("2019 Alternative Final Plan Decision").

M. The Minor Subdivision Plan for Lot 2A and Lot 2B was recorded in the Office of the Chester County Recorder of Deeds at Plan Book No. 20781.

N. Byers Retail sold Lot 2A of Parcel 5C to Toll Mid-Atlantic LP Company, Inc. who developed the lot with a 55-unit townhome community known as The Enclave at

Chester Springs in accordance with the Alternative Final Plan and the conditions imposed in the 2019 Alternative Final Plan Decision.

O. Byers Retail sold Lot 2B of Parcel 5C (identified as Chester County Tax Parcel No. 32-4-1090.1) to Prosperity Property Investments, LLC ("Applicant").

P. Applicant filed an amended final PRD plan for Lot 2B in accordance with the plan titled "Byers Station Amended Final PRD Plans (Final Plan from Lot 2B of Parcel 5C)" prepared by Bohler Engineering dated March 15, 2022, last revised May 25, 2022 (the "2022 Amended Final PRD Plan for Lot 2B of Parcel 5C") which proposed the development of Lot 2B with the following: (i) a 1,820 square foot freestanding eating and drinking establishment with a drive through lane; (ii) a 6,000 square foot retail pad; (iii) a 7,200 square foot retail pad; and (iv) a 10,500 square foot day care center with a 5,500 square foot outdoor play area.

Q. In a Decision and Order approved on September 19, 2022, the Board of Supervisors approved the 2022 Amended Final PRD Plan for Lot 2B of Parcel 5C with conditions. The Board also approved Applicant's request to reduce the number of parking spaces to allow a total of 130 spaces and shared parking.

R. Applicant did not proceed with recording the 2022 Amended Final PRD Plan for Lot 2B of Parcel 5C and instead on April 19, 2024, filed another application to amend the Final PRD Plan to develop a 10,500 square foot daycare center with a 5,500 square foot outdoor play area and a 10,500 square foot commercial building. Applicant also proposes to utilize shared parking pursuant to the terms of the Tentative Approval for the Byers Station PRD.

S. With its application, Applicant filed plans titled “Byers Station Amended Final PRD Plans for Lot 2B of Parcel 5C” prepared by Bohler dated March 15, 2022, last revised April 18, 2024 (“2024 Amended Final PRD Plan for Lot 2B of Parcel 5C”).

T. At its public meeting on May 20, 2024, the Board granted approval of the 2024 Amended Final PRD Plan for Lot 2B of Parcel 5C (“2024 Amended Final PRD Plan”), dated March 15, 2022, last revised April 18, 2024, subject to compliance with conditions set forth in the Order.

U. The 2024 Amended Final PRD Plan (last revised May 31, 2024) was recorded on September 13, 2024 and Applicant commenced construction of the first phase of the development with the day care facility and 10,500 square foot commercial building.

V. On or about June 4, 2025, Chester Springs REI, LLC, (“Owner”) c/o Celebree Property Investments, LLC as successor to Prosperity Property Investments, LLC and owner of Lot 2B of Parcel 5C (Parcel 32-4-1090.1) filed a new application seeking to amend the 2024 Amended Final PRD Plan to allow the development of a second 10,500 square foot commercial building located to the north of the day care and additional parking.

W. Owner proposes to use the new 10,500 square foot building for medical office or other commercial uses permitted on the Property pursuant to the Tentative Approval.

X. Owner filed new plans titled, “Byers Station Amended Final PRD Plans, Final Plan for Lot 2B of Parcel 5C”, prepared by Bohler, dated March 15, 2022, last revised May 30, 2025 (“2025 Amended Final PRD Plan”).

ORDER

AND NOW, the ____ day of August, 2025, the Board of Supervisors of Upper Uwchlan Township hereby grants approval of the 2025 Amended Final PRD Plan subject to compliance with the conditions set forth herein. Pursuant to Paragraph J.8 of the Tentative Plan Approval, the Board also approves the use of shared parking to allow a total of 143 off street parking spaces.

1. The 2025 Amended Final Plan shall be revised to address outstanding comments in Gilmore & Associates, Inc.'s review letter dated July 3, 2025.
2. Owner shall comply with the outstanding comments in the Township Fire Marshal's Review Memo dated July 21, 2025.
3. Applicant shall purchase sufficient sewer capacity for all of the uses on the 2025 Amended Final PRD Plan once capacity needs have been determined by the Upper Uwchlan Municipal Authority.
4. The architecture for the new 10,500 square foot commercial building depicted on the 2025 Amended Final PRD Plan shall be built substantially in conformance with the architecture for the ~~day care~~ 10,500 square foot commercial building which is currently being constructed on the Property.
5. All deliveries to the buildings on the Property shall be from SU-30 vehicles or smaller sized vehicles. There shall be no deliveries from tractor trailers.
6. Applicant shall designate a sufficient number of parking spaces around the buildings as designated loading and unloading spaces and shall install signage to reserve such spaces for deliveries during off peak hours. All deliveries to the site must occur

during off peak hours for the uses, preferably when the uses are closed; provided such time does not conflict with the Township Noise Ordinance.

7. Applicant shall obtain all required outside agency permitting and approvals (e.g., PaDEP Planning Module approval) prior to recordation of the 2025 Amended Final Plan.

8. To the extent that the 2025 Amended Final PRD Plan or any of the conditions imposed by this Decision and Order are inconsistent with any other conditions in the Tentative Approval as applicable to Parcel 5C, or the 2019 Alternative Final Plan Decision, 2022 Amended Alternative Final Plan Decision or 2024 Amended Final PRD Plan, the conditions of approval related to the development of Phase 2 of Lot 2B of Parcel 5C set forth herein shall control.

**BOARD OF SUPERVISORS
UPPER UWCHLAN TOWNSHIP**

Jennifer F. Baxter, Chair

Andrew P. Durkin, Vice-Chair

Sandra M. D'Amico, Supervisor

Gwen Jonik, Township Secretary

CERTIFICATION OF MAILING

I certify that an executed, true and correct copy of the foregoing Decision and Order of the Upper Uwchlan Township Board of Supervisors was mailed by USPS First Class mail, postage prepaid, to Alyson M. Zarro, Esquire, Riley Riper Hollin & Colagreco, P.O. Box 1265, 717 Constitution Drive, Suite 201, Exton, PA 19341, Applicant's legal counsel, on _____, 2024.

Gwen Jonik, Township Secretary

**BEFORE THE BOARD OF SUPERVISORS
UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY, PENNSYLVANIA**

**APPLICATION OF PROSPERITY PROPERTY INVESTMENTS, LLC FOR
REVISED AMENDED FINAL PLANNED RESIDENTIAL DEVELOPMENT APPROVAL
FOR PARCEL 5C – LOT 2B OF THE BYERS STATION
PLANNED RESIDENTIAL DEVELOPMENT**

DECISION AND ORDER

BACKGROUND

A. In August 1998, Bryn Coed Farms submitted an application to Upper Uwchlan Township (the “Township”) for tentative plan approval under the Township's Planned Residential Development Ordinance then in effect and Article VII of the Pennsylvania Municipalities Planning Code for property situated at the intersection of Route 100 and Byers Road in the Township known as the Byers Station Property (the “Property”).

B. On June 7, 1999, the Township granted tentative planned residential development approval (the “Tentative Approval”) for development of the Property as a planned residential development (the “Byers Station PRD”). The Tentative Approval approved a Tentative Plan that included six parcels identified as Parcels 1, 2, 3, 4, 5C and 6C.

C. On July 21, 2003, the Township granted final PRD approval for Parcels 1, 2, 3 and 4 of the Byers Station PRD as depicted in the approved Tentative Plan (“2003 Final Approval”). The 2003 Final Approval did not grant final plan approval for Parcels 5C and 6C of the Tentative Plan.

D. On May 23, 2016, Byers Residential Acquisition Limited Partnership and Byers Retail Acquisition Limited Partnership, then equitable owners of Parcel 5C, submitted an application for final PRD approval for a portion of Parcel 5C depicted in the application and referred to as Lot 1 (the Lot 1 Application"). The Lot 1 Application proposed 121 residential townhouse units on Lot 1. The Lot 1 Application did not seek final PRD approval for the part of Parcel 5C depicted in the Lot 1 Application as Lot 2, consisting of approximately 13.439 acres fronting on Station Boulevard between Pottstown Pike and Graphite Mine Road.

E. The Township granted approval of the Lot 1 Application by Decision and Order dated March 17, 2017, as amended by Decision and Order dated May 15, 2017. The Lot 1 Application Decisions did not grant final approval for Parcel 5C, Lot 2. Lot 1 has been developed by Toll PA, II, L.P. with a 121-unit townhome community known as Villages at Chester Springs (formerly referred to as the Villages at Byers Station.)

F. On February 16, 2018, Byers Retail Acquisition Limited Partnership ("Byers Retail") filed with the Township an application for Final Approval of Parcel 5C, Lot 2, depicting approximately 81,000 square feet of commercial development on Lot 2 (the "2017 Commercial Plan"). Byers Retail chose not to request final approval of the 2017 Commercial Plan from the Board of Supervisors.

G. On April 1, 2019, Byers Retail filed an alternative final PRD plan for Lot 2, depicting 67 residential townhouse units on the east side of Lot 2 and 15,000 square feet of commercial development on the west side of Lot 2. Byers Retail sought approval of the Alternative Plan.

H. After advice by the Township that Lot 2 was limited to 55 residential units by the Tentative Approval, Byers Retail revised the Alternative Plan to depict 55 townhouse units.

I. The Alternative Plan was also revised to depict two commercial pad sites on the western side of Lot 2, one pad on the northwest corner depicting a one and one-half story retail building with a floor area of 2,793 square feet; and a second pad on the southeast corner depicting a two story building containing 2,000 square feet of restaurant and 9,980 square feet of retail on the first floor and 11,800 square feet of office on the second floor.

J. The Alternative Plan also proposed a subdivision of Lot 2 into two lots, Lot 2A comprising 9.468 acres containing the residential development; and Lot 2B comprising 3.971 acres containing the commercial development.

K. The Alternative Plan was titled “Amended Final PRD Plans for Byers Retail Acquisition, L.P. – Final Plan for Lot 2A & 2B of Parcel 5C” prepared by Bohler Engineering, dated March 25, 2019, last revised August 16, 2019, consisting of 42 plan sheets (the “Alternative Final Plan”).

L. The Board approved the Alternative Final Plan in a Decision and Order approved on October 14, 2019 (“2019 Alternative Final Plan Decision”).

M. The Minor Subdivision Plan for Lot 2A and Lot 2B was recorded in the Office of the Chester County Recorder of Deeds at Plan Book No. 20781.

N. Byers Retail sold Lot 2A of Parcel 5C to Toll Mid-Atlantic LP Company, Inc. who developed the lot with a 55-unit townhome community known as The Enclave at

Chester Springs in accordance with the Alternative Final Plan and the conditions imposed in the 2019 Alternative Final Plan Decision.

O. Byers Retail sold Lot 2B of Parcel 5C (identified as Chester County Tax Parcel No. 32-4-1090.1) to Prosperity Property Investments, LLC (“Applicant”).

P. Applicant filed an amended final PRD plan for Lot 2B in accordance with the plan titled “Byers Station Amended Final PRD Plans (Final Plan from Lot 2B of Parcel 5C)” prepared by Bohler Engineering dated March 15, 2022, last revised May 25, 2022 (the “2022 Amended Final PRD Plan for Lot 2B of Parcel 5C”) which proposed the development of Lot 2B with the following: (i) a 1,820 square foot freestanding eating and drinking establishment with a drive through lane; (ii) a 6,000 square foot retail pad; (iii) a 7,200 square foot retail pad; and (iv) a 10,500 square foot day care center with a 5,500 square foot outdoor play area.

Q. In a Decision and Order approved on September 19, 2022, the Board of Supervisors approved the 2022 Amended Final PRD Plan for Lot 2B of Parcel 5C with conditions. The Board also approved Applicant’s request to reduce the number of parking spaces to allow a total of 130 spaces and shared parking.

R. Applicant did not proceed with recording the 2022 Amended Final PRD Plan for Lot 2B of Parcel 5C and instead on April 19, 2024, filed another application to amend the Final PRD Plan to develop a 10,500 square foot daycare center with a 5,500 square foot outdoor play area and a 10,500 square foot commercial building. Applicant also proposes to utilize shared parking pursuant to the terms of the Tentative Approval for the Byers Station PRD.

S. With its application, Applicant filed plans titled “Byers Station Amended Final PRD Plans for Lot 2B of Parcel 5C” prepared by Bohler dated March 15, 2022, last revised April 18, 2024 (“2024 Amended Final PRD Plan for Lot 2B of Parcel 5C”).

T. At its public meeting on May 20, 2024, the Board granted approval of the 2024 Amended Final PRD Plan for Lot 2B of Parcel 5C (“2024 Amended Final PRD Plan”), dated March 15, 2022, last revised April 18, 2024, subject to compliance with conditions set forth in the Order.

U. The 2024 Amended Final PRD Plan (last revised May 31, 2024) was recorded on September 13, 2024 and Applicant commenced construction of the first phase of the development with the day care facility and 10,500 square foot commercial building.

V. On or about June 4, 2025, Chester Springs REI LLC, (“Owner”) c/o Celebree Property Investments, LLC as successor to Prosperity Property Investments, LLC and owner of Lot 2B of Parcel 5C (Parcel 32-4-1090.1) filed a new application seeking to amend the 2024 Amended Final PRD Plan to allow the development of a second 10,500 square foot commercial building located to the north of the day care and additional parking.

W. Owner proposes to use the new 10,500 square foot building for medical office or other commercial uses permitted on the Property pursuant to the Tentative Approval.

X. Owner filed new plans titled, “Byers Station Amended Final PRD Plans, Final Plan for Lot 2B of Parcel 5C”, prepared by Bohler, dated March 15, 2022, last revised May 30, 2025 (“2025 Amended Final PRD Plan”).

ORDER

AND NOW, the ____ day of August, 2025, the Board of Supervisors of Upper Uwchlan Township hereby grants approval of the 2025 Amended Final PRD Plan subject to compliance with the conditions set forth herein. Pursuant to Paragraph J.8 of the Tentative Plan Approval, the Board also approves the use of shared parking to allow a total of 143 off street parking spaces.

1. The 2025 Amended Final Plan shall be revised to address outstanding comments in Gilmore & Associates, Inc.'s review letter dated July 3, 2025.
2. Owner shall comply with the outstanding comments in the Township Fire Marshal's Review Memo dated July 21, 2025.
3. Applicant shall purchase sufficient sewer capacity for all of the uses on the 2025 Amended Final PRD Plan once capacity needs have been determined by the Upper Uwchlan Municipal Authority.
4. The architecture for the new 10,500 square foot commercial building depicted on the 2025 Amended Final PRD Plan shall be built substantially in conformance with the architecture for the 10,500 square foot commercial building which is currently being constructed on the Property.
5. All deliveries to the buildings on the Property shall be from SU-30 vehicles or smaller sized vehicles. There shall be no deliveries from tractor trailers.
6. Applicant shall designate a sufficient number of parking spaces around the buildings as designated loading and unloading spaces and shall install signage to reserve such spaces for deliveries during off peak hours. All deliveries to the site must occur

during off peak hours for the uses, preferably when the uses are closed; provided such time does not conflict with the Township Noise Ordinance.

7. Applicant shall obtain all required outside agency permitting and approvals (e.g., PaDEP Planning Module approval) prior to recordation of the 2025 Amended Final Plan.

8. To the extent that the 2025 Amended Final PRD Plan or any of the conditions imposed by this Decision and Order are inconsistent with any other conditions in the Tentative Approval as applicable to Parcel 5C, or the 2019 Alternative Final Plan Decision, 2022 Amended Alternative Final Plan Decision or 2024 Amended Final PRD Plan, the conditions of approval related to the development of Phase 2 of Lot 2B of Parcel 5C set forth herein shall control.

**BOARD OF SUPERVISORS
UPPER UWCHLAN TOWNSHIP**

Jennifer F. Baxter, Chair

Andrew P. Durkin, Vice-Chair

Sandra M. D'Amico, Supervisor

Gwen Jonik, Township Secretary

CERTIFICATION OF MAILING

I certify that an executed, true and correct copy of the foregoing Decision and Order of the Upper Uwchlan Township Board of Supervisors was mailed by USPS First Class mail, postage prepaid, to Alyson M. Zarro, Esquire, Riley Riper Hollin & Colagreco, P.O. Box 1265, 717 Constitution Drive, Suite 201, Exton, PA 19341, Applicant's legal counsel, on _____, 2025.

Gwen Jonik, Township Secretary

ALYSON ZARRO
alyson@rrhc.com
extension 0202



June 4, 2025

Via email and hand delivery

Tony Scheivert, Township Manager
Upper Uwchlan Township
140 Pottstown Pike
Chester Springs, PA 19425
tscheivert@upperuwchlan-pa.gov

**Re: Byers Station Parcel 5C Lot 2B
Amended Final Planned Residential Development Plan Application – Phase 2**

Dear Tony:

As you know, I represent Chester Springs REI LLC c/o Celebree Property Investments, LLC (“Applicant”) in connection with Lot 2B of Parcel 5C in the Byers Station Planned Residential Development (“Property”). Applicant is the legal owner of the Property which is approximately 3.887 acres and is further identified as UPI Number 32-4-1090.1.

On September 22, 2022, Applicant received Amended Final Planned Residential Development Plan Approval for the development of the Property with a 1,820 sq. ft. eating and drinking establishment with a drive through, a 6,000 sq. ft. retail pad, a 7,200 sq. ft. retail pad, and a 10,500 sq. ft. daycare center with a 5,500 sq. ft. play area (“2022 Approval”). Subsequently, Applicant amended its plan to reconfigure the daycare center and play area, as well as to change the previously approved retail buildings such that a 10,500 sq. ft. building could be built along with the daycare center as a first phase of development on the Property. This amended plan was approved by the Board of Supervisors on May 20, 2024 and was subsequently recorded (“2024 Approval”). Construction pursuant to the 2024 Approval is ongoing.

Applicant now desires to amend the approvals for construction of a second phase of the development on the Property with a second 10, 500 sq. ft. commercial building and additional parking.

In connection with this amended plan, the Applicant is again proposing to utilize shared parking for the proposed uses pursuant to Section J.8 of the Tentative Approval for the Byers Station Planned Residential Development dated June 7, 1999.

Enclosed for filing in connection with this Amended Final PRD Plan Application (“Application”) for the Property are the following materials:

1. Four (4) copies of a plan entitled “ ‘Byers Station’ Amended Final PRD Plans (Final Plan for Lot 2B of Parcel 5C)” prepared by Bohler dated March 15, 2022, and last

revised May 30, 2025;

2. Four (4) copies of the General Project Description and Stormwater Management Calculations prepared by Bohler dated March 25, 2019, and last revised May 25, 2025;
3. Four (4) copies of a Parking and Traffic Analysis prepared by TPD dated June 2, 2025;
4. Four (4) copies of a Transportation Impact Assessment prepared by TPD dated June 2, 2025;
5. One (1) copy of the Township Reimbursement Agreement;
6. One (1) copy of the Chester County Act 247 Referral form;
7. A check in the amount of \$250.00 made payable to Upper Uwchlan Township, the final plan application fee;
8. A check in the amount of \$5,000.00 made payable to Upper Uwchlan Township for the Township consultant review fees escrow account; and
9. A check in the amount of \$1,194.50 made payable to the Chester County Planning Commission for its review.

The electronic version of this letter includes a link to download all of the materials comprising the Application.

As always, please feel free to contact me with any questions or if you require any additional information. Thank you for your assistance with this matter.

Very truly yours,

Alyson M. Zarro

Alyson M. Zarro

AMZ/kdj
Enclosures

cc: Gwen Jonik, Township Secretary *(via email, w/enclosures)*
Kristin Camp, Esquire, Township Solicitor *(via email, w/enclosures)*
Allan Greenberg, Celebree Property Investments, LLC *(via email, w/enclosures)*
Gary Large *(via email, w/enclosures)*
Chris Puzinas, P.E., Bohler Engineering *(via email, w/enclosures)*
Guy DiMartino, P.E., TPD *(via email, w/enclosures)*
Weston Blaney, JAM Architecture *(via email, w/enclosures)*

_____ FOR _____

PROPOSED

LOCATION OF SITE

1,000' OFF-SITE BOUNDARY

SITE

Windsor Cemetery

Pickering Valley Elementary School

100

Byers Rd

© 2022 Microsoft Corporation © 2021 TomTom



- ➔ **A** "STOP SIGN (R-1)"
- ➔ **B** "DO NOT ENTER SIGN (R-5)"
- ➔ **C** "RESERVED PARKING SIGN (R-7-4) & RESERVED PARKING PENALTIES SIGN (R-7-4.1)"
- ➔ **D** "RESERVED PARKING SIGN (R-7-4) & RESERVED PARKING PENALTIES SIGN (R-7-4.1) WITH "NO ACCESSIBLE" SIGN (R-7-4.6)"
- ➔ **E** "PEDESTRIAN CROSSING SIGN (W11-2) & DOWNWARD POINTING ARROW (W16-7P)"
- ➔ **F** "RIGHT TURN ONLY SIGN (R-10R)"
- ➔ **G** "TO ROUTE 100 (NORTHBOUND ONLY) SIGN (M1-4, M1-3, M1-5)"
- ➔ **H** "DESIGNATED OFF PEAK LOADING AREA SIGN"
- ➔ **I** "STREET NAME" SIGN (D3-1)

REVISION 8 - 05/30/2025

4 OF 36

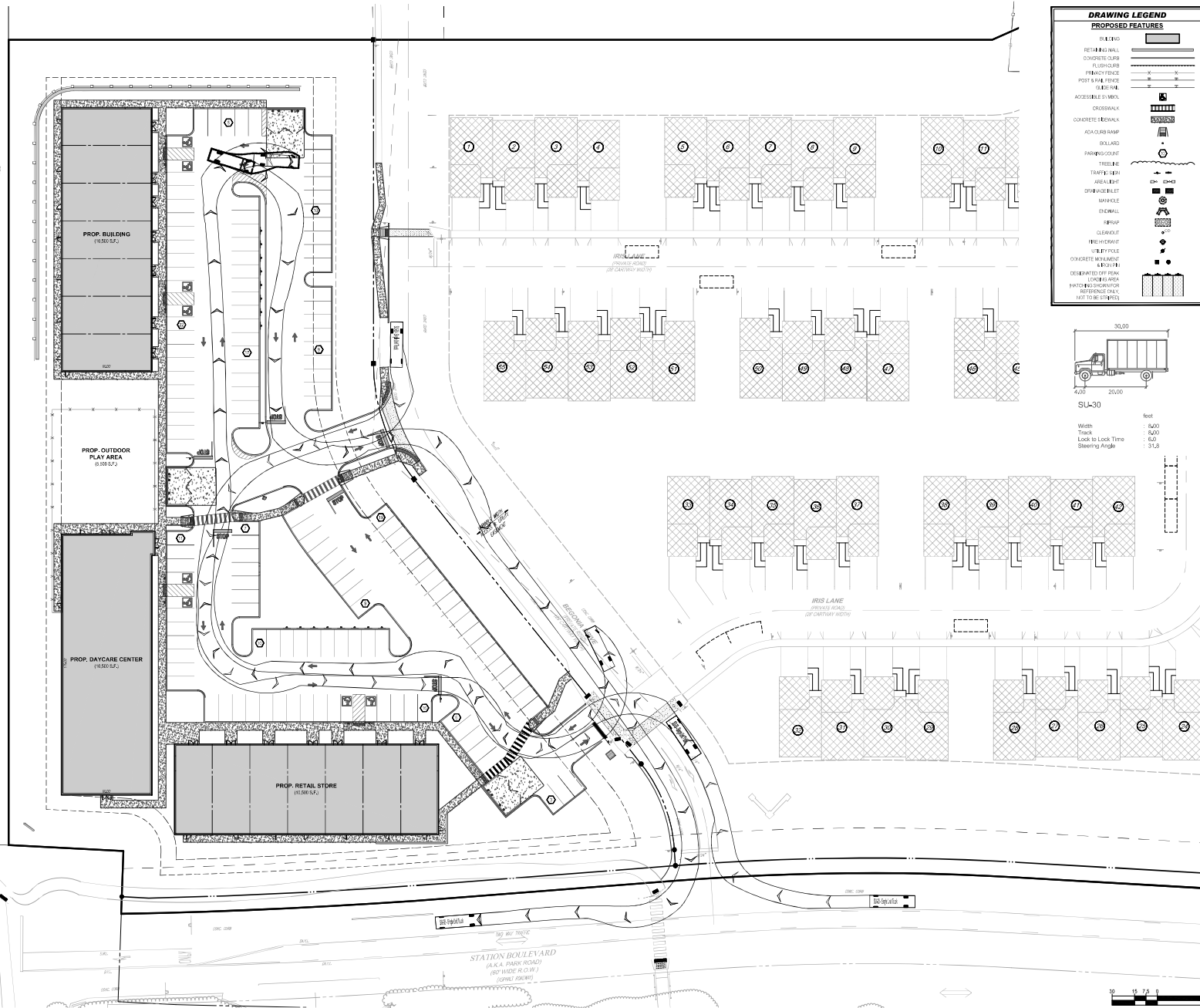
REVISION 8 - 05/30/2025

REVISION 8 - 05/30/2025

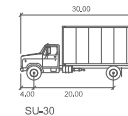




NOT TO SCALE
PROPORTION: 1"=100'-0"
(AS SHOWN ON SHEET 00)
(SEE SHEET 00 FOR
GENERAL NOTES)



DRAWING LEGEND	
PROPOSED FEATURES	
BUILDING	
RETAINING WALL	
CONCRETE CURB	
PURCHASER	
POST AND RAIL FENCE	
CRACK LINE	
ACCESSIBLE TO MARK	
CROSSWALK	
CONCRETE SIDEWALK	
ASPHALT RAMP	
BOLLARD	
PARKING LOT	
TRAP PILE	
WELLS	
SPRINKLER	
MANHOLE	
SEWER	
WATER	
CLEARWATER	
UTILITY POLE	
CONCRETE WALL	
LAND FILL	
DEBRIS OFF ROAD	
LAND FILL	
WATER SHED FOR	
REFERENCE ONLY	
NOT TO BE STREETS	



Width: 30.00
Track: 4.00
Look to Lock Time: 4.00
Steering Angle: 37.5

BOHLER
SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGEMENT
LANDSCAPE ARCHITECTURE
PERMITTING SERVICES
TRANSPORTATION SERVICES

REVISIONS

REV	DATE	COMMENT	BY
1	05/02/2024	REV. PER TOWNSHIP COMMENTS	LOU
2	07/08/2024	REV. PER TOWNSHIP COMMENTS	LOU
3	10/07/2024	REV. PER TOWNSHIP COMMENTS	LOU
4	10/07/2024	REV. PER TOWNSHIP COMMENTS	LOU
5	10/07/2024	REV. PER CLIENT COMMENTS	LOU
6	05/01/2024	REV. PER TOWNSHIP COMMENTS	LOU
7	10/07/2024	REV. PER TOWNSHIP COMMENTS	LOU
8	05/02/2024	REV. PER NEW BUILDING	LOU

811
Call before you dig.
YOU MUST CALL 811 BEFORE ANY EXCAVATION OR DRILLING.
WHEN THERE IS A PROBLEM WITH YOUR SERVICE, CALL 811.
SERIAL No. 20180313172

PROJECT No. PC211105
DRAWN BY: C.D.P.
CHECKED BY: C.D.P.
DATE: 05/02/2024
CAD ID: PC211105-01-PRD-ABA

PROJECT:
AMENDED FINAL PRD PLANS
FOR
PROSPERITY PROPERTY INVESTMENTS, LLC
"FINAL PLAN FOR LOT 2B OF PARCEL 5C"
PROPOSED COMMERCIAL DEVELOPMENT
GRAPHITE MINE ROAD & STATION BOULEVARD
UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY
COMMONWEALTH OF PENNSYLVANIA

BOHLER
1600 MANOR DRIVE, SUITE 200
CHALFONT, PA 18914
Phone: (717) 596-2100
Fax: (717) 596-4102
www.BohlerEngineering.com

C.D. PUZANAS
PROFESSIONAL ENGINEER
PENNSYLVANIA License No. 0001100

SHEET TITLE:
DELIVERY TRUCK CIRCULATION PLAN

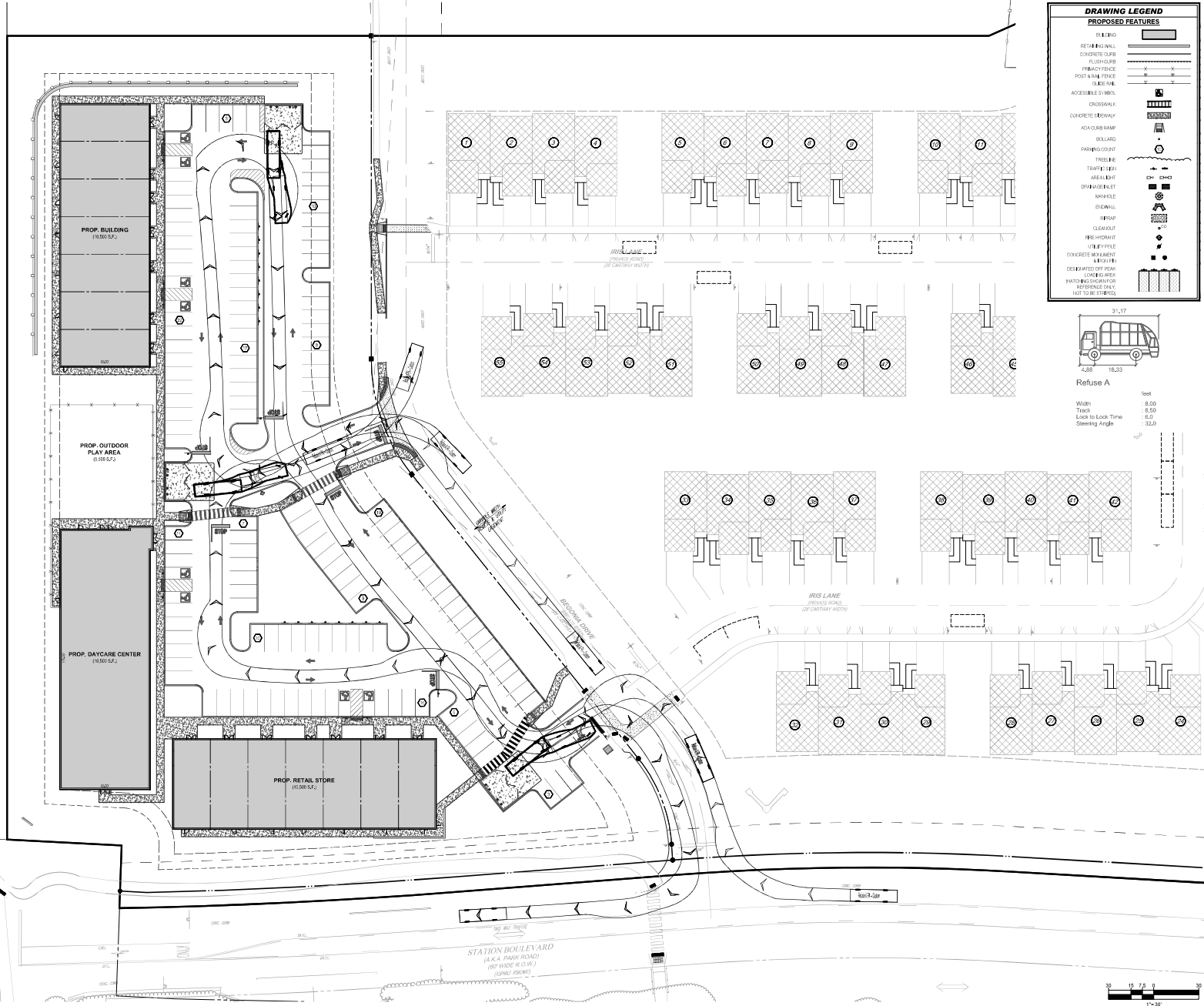
SHEET NUMBER:
C-303
1 OF 36

REVISION 8 - 05/30/2025

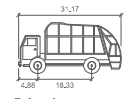
UNIFORM PARCEL IDENTIFIER: 52-4-1090.1



NOT TO SCALE
PROPORTION: 1" = 100'
(AS SHOWN ON SHEET 100)
(SEE SHEET 100 FOR
GENERAL NOTES)



DRAWING LEGEND	
PROPOSED FEATURES	
BUILDING	
RETAINING WALL	
CONCRETE CURB	
FLUSH CURB	
FRANCHISE FENCE	
POST AND RAIL FENCE	
CADASTRAL	
ACCESSIBLE TO MARK	
CROSSWALK	
CONCRETE SIDEWALK	
ASPHALT SIDEWALK	
BOLLARD	
PARKING COUNTER	
TREELINE	
TRAP AND PUMP	
WELLS	
SPRINKLER INLET	
MANHOLE	
SEWER	
STORM	
CLEARCUT	
PIPE HYDRANT	
UTILITY POLE	
CONCRETE RETAINMENT WALL	
DEBRIS OFF ROAD	
LOW WALL	
PAVING SIGNIFICANT	
REFERENCE ONLY	
HAD TO BE STORIES	



Refuse A

Width 8.00
Track 8.00
Look to Lock Time 6.0
Steering Angle 32.0

BOHLER

SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGEMENT
LANDSCAPE ARCHITECTURE
TRANSPORTATION SERVICES

REVISIONS		
REV	DATE	COMMENT
1	05/02/2024	REV. PER TOWNSHIP COMMENTS
2	07/08/2024	REV. PER TOWNSHIP COMMENTS
3	10/07/2024	REV. PER TOWNSHIP COMMENTS
4	04/01/2025	REV. PER TOWNSHIP COMMENTS
5	04/01/2025	REV. PER CLIENT COMMENTS
6	05/01/2025	REV. PER TOWNSHIP COMMENTS
7	10/19/2025	REV. PER NEW BUILDING
8	05/02/2025	REV. PER NEW BUILDING

811

Call before you dig.
Digging a hole? Call 811 first.
You must call 811 before you dig a hole.
Where there's a hole, there's a hole.
SERIAL NO. 20180313172

PROJECT NO.	PC211105
DRAWN BY	CU
CHECKED BY	C.D.P.
DATE	05/02/2025
CAD ID	PC211105-01V9-AB

PROJECT:
AMENDED FINAL PRD PLANS
FOR
PROSPERITY PROPERTY INVESTMENTS, LLC
"FINAL PLAN FOR LOT 2B OF PARCEL 5C"
PROPOSED COMMERCIAL DEVELOPMENT
GRAPHITE HIRE ROAD & STATION BOULEVARD
UPPER UICHLAN TOWNSHIP
CHESTER COUNTY
COMMONWEALTH OF PENNSYLVANIA

BOHLER

1600 MANOR DRIVE, SUITE 200
CHALFONT, PA 18914
Phone: (215) 996-2100
Fax: (215) 996-4102
www.BohlerEngineering.com

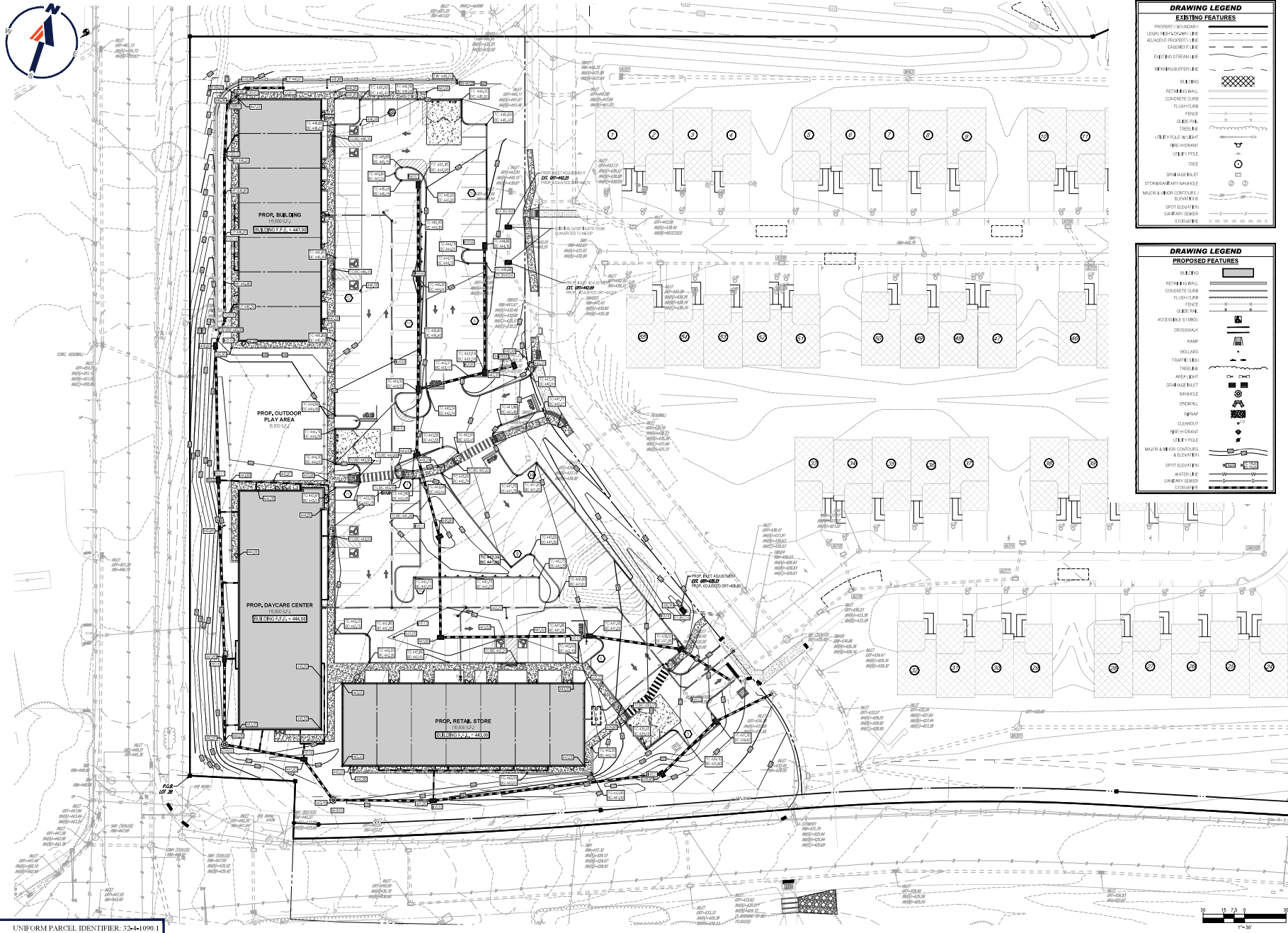
C.D. PUZINAS

PROFESSIONAL ENGINEER
PENNSYLVANIA LICENSE #20110

SHEET TITLE:
TRASH TRUCK CIRCULATION PLAN

SHEET NUMBER:
C-304
1 OF 36
REVISION 8 - 05/30/2025

UNIFORM PARCEL IDENTIFIER: 52-4-1090.1



DRAWING LEGEND	
EXISTING FEATURES	
PROPERTY BOUNDARY	---
LEGAL EASEMENT/RIGHT-OF-WAY LINE	---
ADVANCED PROPOSED LINE	---
EASEMENT LINE	---
EXISTING STREAM LINE	---
EXISTING UTILITY LINE	---
BLDG.	---
RETAINING WALL	---
CONCRETE CURB	---
FLUSH CURB	---
TRUCK	---
GRASS PINE	---
TREELINE	---
UTILITY POLE IN LIGHT	---
TRUCK HIGHWAY	---
UTILITY POLE	---
TREE	---
UPSIDE INLET	---
STORMSEWER MANHOLE	---
MAJOR & MINOR CONTROLS	---
SPOT ELEVATION	---
SANITARY SEWER	---
STREET	---

DRAWING LEGEND	
PROPOSED FEATURES	
BLDG.	---
RETAINING WALL	---
CONCRETE CURB	---
FLUSH CURB	---
TRUCK	---
GRASS PINE	---
ACCESSIBLE SYMBOL	---
CROSSWALK	---
RAMP	---
BOLLARD	---
TRAFFIC LIGHT	---
TREELINE	---
AREA LIGHT	---
STORMSEWER INLET	---
SPACED	---
ENDWALL	---
BRIDGE	---
CLEANOUT	---
STORMSEWER	---
UTILITY POLE	---
MAJOR & MINOR CONTROLS	---
ELEVATION	---
SPOT ELEVATION	---
WATER LINE	---
SANITARY SEWER	---
STREET	---

BOHLER

SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGEMENT
LANDSCAPE ARCHITECTURE
TRANSPORTATION SERVICES

REVISIONS		
REV	DATE	COMMENT
1	05/01/2024	REV. PER TOWNSHIP COMMENTS
2	07/08/2024	REV. PER TOWNSHIP COMMENTS
3	10/07/2024	REV. PER TOWNSHIP COMMENTS
4	04/01/2025	REV. PER TOWNSHIP COMMENTS
5	04/18/2025	REV. PER CLIENT COMMENTS
6	05/01/2025	REV. PER TOWNSHIP COMMENTS
7	10/19/2025	REV. PER TOWNSHIP COMMENTS
8	05/01/2025	REV. PER NEW BUILDING

811

Call before you dig.
Digging in the ground is dangerous. You must call 811 before you dig to find out where there are underground utilities. If you don't, you could damage a gas line, water line, or other underground utility. It could be dangerous to you and your property. Call 811 to find out where there are underground utilities. It's the safe way to dig.

PROJECT NO.	PC211105
DRAWN BY:	LOU
CHECKED BY:	C.D.P.
DATE:	05/01/2025
CAD ID:	PC211105-01V-04

PROJECT:

**AMENDED
FINAL PRD PLANS**

FOR
**PROSPERITY PROPERTY
INVESTMENTS, LLC**

"FINAL PLAN FOR LOT 2B OF
PARCEL 5C"
PROPOSED COMMERCIAL
DEVELOPMENT

GRAPHITE HIRE ROAD &
STATION BOULEVARD
UPPER MERIDIAN TOWNSHIP
CHESTER COUNTY
COMMONWEALTH OF PENNSYLVANIA

BOHLER

1600 MANOR DRIVE, SUITE 200
CHALFONT, PA 18914
Phone: (717) 396-2100
Fax: (717) 396-4102
www.BohlerEngineering.com

C.D. PUZMAS

PROFESSIONAL ENGINEER
PENNSYLVANIA LICENSE NO. 10010

SHEET TITLE:

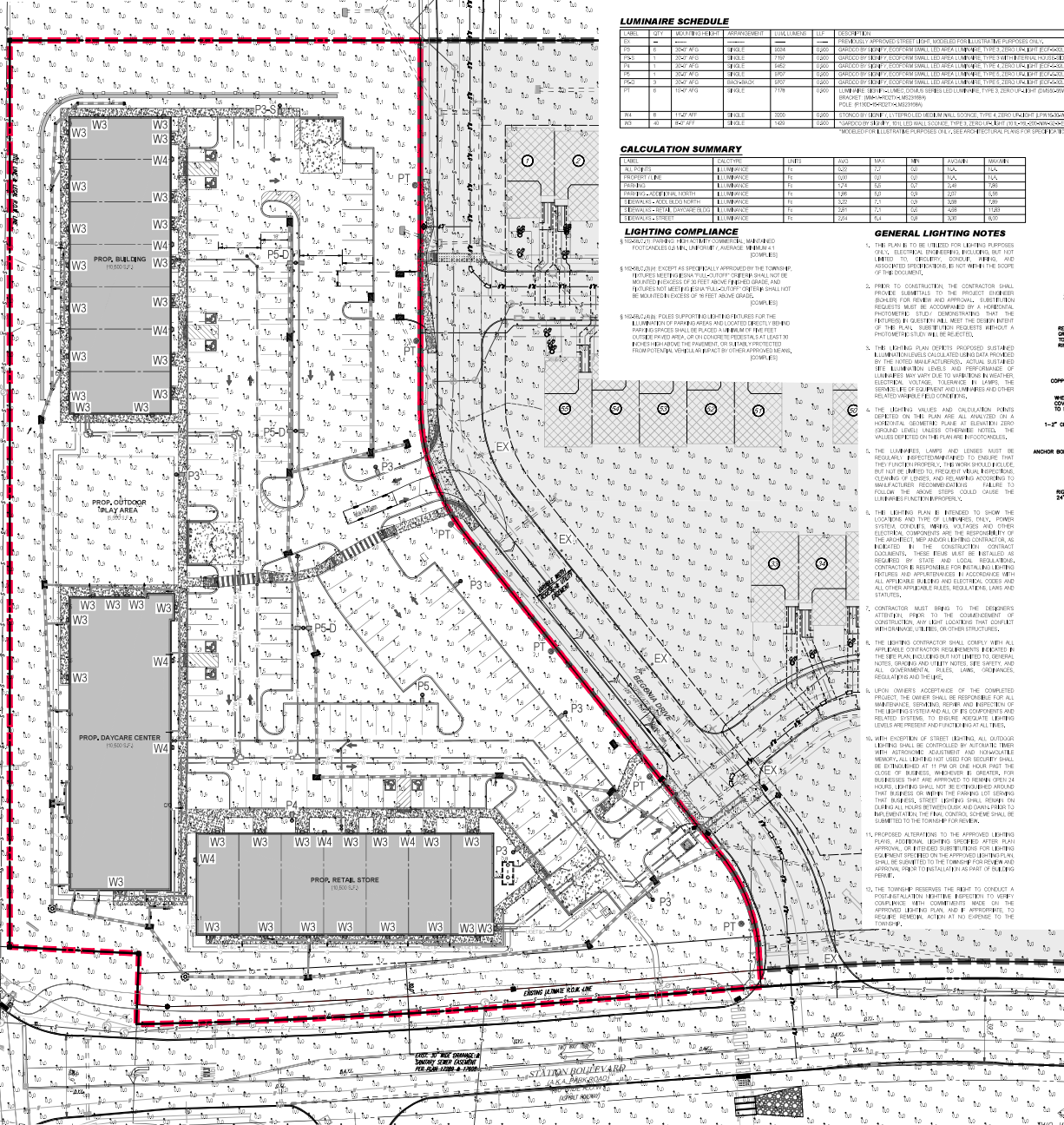
GRADING PLAN

SHEET NUMBER:
C-401
1 OF 36

REVISION 8 - 05/30/2025



UNIFORM PARCEL IDENTIFIER: 52-4-1090-1



LUMINAIRE SCHEDULE

LINE	TYPE	HEIGHT	SPACING	DESCRIPTION	FOUNDATION	WIRE TYPE
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"

CALCULATION SUMMARY

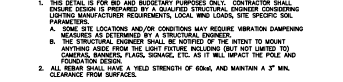
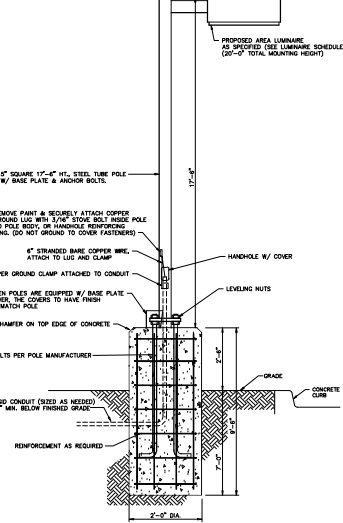
LINE	TYPE	HEIGHT	SPACING	DESCRIPTION	FOUNDATION	WIRE TYPE
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"
EX	STREET	10.0	10.0	STREET LIGHTING - 10.0' HGT. STREET LIGHTING	CONCRETE	1/2" X 1/2" X 1/2"

LIGHTING COMPLIANCE

1. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.
2. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.
3. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.

GENERAL LIGHTING NOTES

1. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.
2. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.
3. THE LIGHTING PLAN IS SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWNSHIP ENGINEER. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE. THE TOWNSHIP ENGINEER SHALL REVIEW THE PLAN FOR COMPLIANCE WITH THE TOWNSHIP LIGHTING ORDINANCE.



BOHLER
SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGER
LANDSCAPE ARCHITECTURE
PERMITTING SERVICES
TRANSPORTATION SERVICES

REVISIONS

REV	DATE	COMMENT	BY	CHK
1	05/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
2	07/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
3	10/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
4	04/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
5	01/01/2024	REV. PER CLIENT COMMENTS	LOU	LOU
6	05/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
7	07/01/2024	REV. PER TOWNSHIP COMMENTS	LOU	LOU
8	05/01/2024	REV. PER NEW BUILDING	LOU	LOU

811
Call before you dig.
YOU MUST CALL 811 BEFORE YOU DIG.
SERIAL NO. 2108231372

PROJECT NO. PC21160
DRAWN BY: C.P.P.
CHECKED BY: C.P.P.
DATE: 05/01/2024
PROJECT: PC21160-LT01-0A

AMENDED FINAL PRD PLANS
FOR
PROSPERITY PROPERTY INVESTMENTS, LLC
"FINAL PLAN FOR LOT 28 OF PARCEL 5C"
PROPOSED COMMERCIAL DEVELOPMENT
GRAPHITE HIRE ROAD & STATION BOULEVARD
UPPER UCHLAN TOWNSHIP
CHESTER COUNTY
COMMONWEALTH OF PENNSYLVANIA

BOHLER
1600 MANOR DRIVE, SUITE 200
CHALFONT, PA 18914
Phone: (215) 956-1100
Fax: (215) 956-1102
www.BohlerEngineering.com

D.T. NORTH
REGISTERED LANDSCAPE ARCHITECT
PC21160-LT01-0A

SHEET TITLE:
LIGHTING PLAN
SHEET NUMBER:
C-701
23 OF 36
REVISION 8 - 05/30/2025

[illegible]



1025 Andrew Drive, Suite 110, West Chester, PA 19380
610.326.3100 | INFO@TPDINC.COM

WWW.TPDINC.COM

June 2, 2025

Allan Greenberg, CFO
CELEBREE, Inc.
1306 Bellona Avenue
Lutherville, MD 21093

RE: Byers Station Lot 2B, Parcel 5C – Parking and Traffic Analysis

Upper Uwchlan Township, Chester County, PA

TPD No. CELE.00001

Dear Allan:

Traffic Planning and Design, Inc. (TPD) has completed a parking and traffic analysis for the proposed Byers Station Lot 2B, Parcel 5C, located on the northeast corner of the intersection of Pottstown Pike and Station Boulevard in Upper Uwchlan Township, Chester County, Pennsylvania. Land development approvals were previously obtained for the subject site, with the most recent approval in 2024 being for a daycare and a 10,500 s.f. of commercial/retail space. Under the current plan, a 10,500 s.f. medical office building (MOB) is proposed in addition to the daycare and 10,500 s.f. commercial/retail building. As such, the purpose of this analysis is to evaluate the parking and traffic for the currently proposed development of the site.

PARKING ANALYSIS

Under the current site plan proposes a total of 143 parking spaces. To determine if the proposed parking is sufficient to support the site, the peak demand of each use was evaluated based on the Institute of Transportation Engineers' (ITE) Parking Generation Manual, 6th Edition, 2023. For the current plan, Land Use Code 565 (Day Care), Land Use Code 930 (Fast Casual Restaurant), and Land Use Code 822 (Strip Retail Plaza) were used to calculate the peak parking demand. ***Please note, the parking analysis assumes that the 10,500 s.f. commercial/retail building will include two (2) restaurants at 1,500 s.f. each. In addition, although the MOB use is proposed for one of the 10,500 s.f. buildings, this parking analysis utilizes the Retail land use (#822), as this results in a higher peak parking demand than a MOB use (#720).***

Table 1 shows the average peak parking demand rates for each use, including the MOB rates for information/comparison purposes.

TABLE 1
ITE PEAK PARKING DEMAND

Land Use	ITE #	X	Average Rate	Peak Parking Demand
Day Care	565	140 students	$P = 0.25*(X)$	35
Restaurants	930	3.0 ksf	$P = 9.77*(X)$	29
Strip Retail	822	18.0 ksf	$P = 2.79*(X)$	51
MOB	720	---	$P = 2.63*(X)$	---
Total Peak Parking Demand				115

P = Peak Parking Demand; X = Size

As shown in **Table 1**, if all the uses on-site experienced their peak parking demand simultaneously, the peak parking demand for the site would be 115 occupied spaces. However, mixed-use sites such as the proposed development have opportunities for shared parking. As such, the parking occupancy time of day from the ITE Parking Generation Manual methodology was utilized to evaluate the shared parking for the site. As shown in **Table 2**, the time-of-day parking demand distribution is summarized for the proposed uses. **Table 3** applies the time-of-day distributions to peak parking demand noted in **Table 1**, in order to determine the shared parking demand on an hourly basis.

TABLE 2
WEEKDAY TIME-OF-DAY VARIATION IN PARKING DEMAND (%)

Hour of Day	Percent of Peak Period		
	Day Care ¹	Restaurants ²	Retail ³
6:00 AM	11%	--	--
7:00 AM	45%	--	--
8:00 AM	89%	--	19%
9:00 AM	93%	--	33%
10:00 AM	100%	14%	47%
11:00 AM	100%	22%	55%
12:00 PM	97%	100%	89%
1:00 PM	93%	78%	100%
2:00 PM	88%	40%	73%
3:00 PM	82%	32%	73%
4:00 PM	88%	26%	66%
5:00 PM	96%	46%	70%
6:00 PM	61%	77%	75%
7:00 PM	30%	63%	70%
8:00 PM	--	34%	54%
9:00 PM	--	22%	48%
10:00 PM	--	14%	--
11:00 PM	--	--	--

1. Time-of-day data provided from 6 AM – 6 PM. 7 PM assumed to be 50% of 6PM.

2. Time-of-day data provided from 8 AM – 10 PM, but assumed 0% for 8-10 AM since the store is not typically open.

3. Time-of-day data provided from 8 AM – 9 PM.

TABLE 3
WEEKDAY TIME-OF-DAY VARIATION IN PARKING DEMAND

Hour of Day	Occupied Parking Spaces			
	Day Care	Restaurants	Retail	Total
6:00 AM	4	0	0	4
7:00 AM	16	0	0	16
8:00 AM	31	1	10	42
9:00 AM	33	3	17	53
10:00 AM	35	4	24	63
11:00 AM	35	6	28	69
12:00 PM	34	29	45	108
1:00 PM	33	23	51	107
2:00 PM	31	12	37	80
3:00 PM	29	9	37	75
4:00 PM	31	8	34	73
5:00 PM	34	13	36	83
6:00 PM	21	22	38	81
7:00 PM	11	18	36	65
8:00 PM	0	10	28	38
9:00 PM	0	6	24	30
10:00 PM	0	4	0	4
11:00 PM	0	0	0	0

As shown in **Table 3**, per the referenced data, the maximum peak parking demand at any point during the day is anticipated to be 108 spaces, which is less than the proposed 143 parking spaces proposed for the current site.

If you have any questions or comments, please feel free to contact us.

Sincerely,

TPD



Guido W. DiMartino, P.E.

Regional Leader – Transportation Planning



June 2, 2025
TPD# CELE.00001

BYERS STATION

Transportation Impact Assessment

Upper Uwchlan Township, Chester County, PA

For Submission To:

Upper Uwchlan Township

BYERS STATION TRANSPORTATION IMPACT ASSESSMENT

FOR SUBMISSION TO:

Upper Uwchlan Township, Chester County, PA

Prepared For:

Celebree
8029 Corporate Drive
Nottingham, MD 21236

June 2, 2025

TPD # CELE.00001



Prepared By:

TPD

1025 Andrew Drive, Suite 110
West Chester, Pennsylvania 19380

Phone: (610) 326-3100

Fax: (610) 326-9410

E-mail: TPD@TPDinc.com

Website: www.TPDinc.com

TABLE OF CONTENTS

EXECUTIVE SUMMARY	i
INTRODUCTION	1
EXISTING ROADWAY NETWORK.....	1
EXISTING TRAFFIC CONDITIONS.....	3
BASE (NO-BUILD) CONDITIONS.....	4
SCHEDULED ROADWAY IMPROVEMENTS	5
PROPOSED SITE ACCESS	5
TRIP GENERATION	5
TRIP DISTRIBUTION	7
PROJECTED (BUILD) CONDITION TRAFFIC VOLUMES	8
LEVELS OF SERVICE FOR AN INTERSECTION	8
CAPACITY ANALYSIS METHODOLOGY	9
LEVELS OF SERVICE IN THE STUDY AREA	11
95TH PERCENTILE QUEUE ANALYSIS	12
AUXILIARY TURN LANE WARRANT ANALYSIS.....	13
RECOMMENDATIONS AND CONCLUSIONS.....	13

FIGURES 1 – 14

TECHNICAL APPENDICES

Appendix A:	Study Area Photographs
Appendix B:	Manual Traffic Count Printouts
Appendix C:	Volume Development Spreadsheets
Appendix D:	Capacity Analyses
Appendix E:	Traffic Signal Plans
Appendix F:	Turn Lane Warrants

EXECUTIVE SUMMARY

The purpose of this study is to examine the potential traffic impact associated with the proposed Byers Station development on the roadway network in Upper Uwchlan Township, Chester County, PA. Based on this evaluation, the following conclusions were reached:

- » The project site is located on the northeast corner of Pottstown Pike (SR 0100) and Station Boulevard/Park Road and is comprised of three (3) 10,500 s.f. buildings. One building is proposed as a daycare for 140 students, with the other buildings planned for commercial/retail space. One commercial building is planned as medical office building (MOB) space, and the other is planned a 7,500 s.f. of retail and 3,000 s.f. of restaurant space.
- » The site will be served by driveways to Begonia Drive, with external access to Station Boulevard via Begonia Drive. No direct access to Route 100 is proposed.
- » The proposed development is expected to generate 108 new external trips during the weekday A.M. peak hour, 144 new external trips during the weekday P.M. peak hour, and 133 new external trips during the Saturday midday peak hour.
- » Under all projected (build) conditions with the development of the proposed site and with site-related recommendations, all study area intersections will satisfy PennDOT ILOS Standards.
- » Levels of Service (LOS) for the study area intersections have been summarized in matrix form. **Table I** details the overall intersection LOS for each study area intersection.

TABLE I
OVERALL INTERSECTION LEVEL OF SERVICE SUMMARY

Intersection	Time Period	Existing	2026 Conditions		Meets LOS Requirements?
			Base	Projected	
Pottstown Pike (SR 0100) & Park Road/Station Boulevard	Weekday AM Peak	C (25.6)	C (27.4)	C (29.4)	YES
	Weekday PM Peak	C (22.8)	C (25.1)	C (27.2)	
	Saturday Peak	C (21.7)	C (24.3)	C (24.5)	
Graphite Mine Road & Station Boulevard	Weekday AM Peak	C (28.0)	C (27.9)	C (28.1)	YES
	Weekday PM Peak	B (15.8)	B (16.1)	B (17.4)	
	Saturday Peak	A (9.6)	A (9.7)	B (10.2)	
Station Boulevard & Begonia Drive	Weekday AM Peak	A (4.2)	A (4.1)	A (7.5)	YES
	Weekday PM Peak	A (1.3)	A (1.2)	A (5.0)	
	Saturday Peak	A (1.4)	A (1.3)	A (4.8)	

Base = No-Build scenario, Projected = Build scenario

INTRODUCTION

Traffic Planning and Design, Inc. (TPD) has completed a Transportation Impact Assessment (TIA) for the proposed Village at Byers Station development (formerly known as Byers Station Parcel 5C) on the roadway network in Upper Uwchlan Township, Chester County, PA. As shown in **Figure 1**, the project site is located on the northeast corner of Pottstown Pike (SR 0100) and Station Boulevard/Park Road. As shown in **Figure 2**, the proposed development is comprised of three (3) 10,500 s.f. buildings. One building is proposed as a daycare for 140 students, with the other buildings planned for commercial/retail space. One commercial building is planned as medical office building (MOB) space, and the other is planned a 7,500 s.f. of retail and 3,000 s.f. of restaurant space.

The subject site is part of the overall Byers Station PRD that was approved by the Township in 1999. Byers Station funded and constructed much of the roadway improvements in the area that are currently in place, including Graphite Mine Road, Route 100 widening, Park Road extension, etc. These roadway improvements in the PRD approval were developed based on a maximum of 311,000 s.f. of retail/office space, or 175,000 s.f. of retail/office space and 220 multi-family units.

This report has been prepared in accordance with PennDOT's *Policies and Procedures for Transportation Impact Studies*, found in PennDOT's Publication 282, Appendix A, dated February 2024.

Site Access Locations

The proposed development will primarily be served by the driveways to Begonia Drive, with external access to Station Boulevard via Begonia Drive.

EXISTING ROADWAY NETWORK

A field review of the existing roadway system in the study area was conducted. The existing roadway characteristics within the study area are summarized in **Table 1**. Photographs of the study area intersections are included in **Appendix A**.

TABLE 1
ROADWAY CHARACTERISTICS WITHIN STUDY AREA

Roadway	Ownership	Functional Classification/ Roadway Type	Predominant Directional Orientation	Average Daily Traffic ¹	Posted Speed Limit
Pottstown Pike	State (SR 0100)	Principal Arterial Highway	North-South	12,936	35 mph
Graphite Mine Road	Local		North-South	12,080	40 mph
Station Boulevard	Local		East-West	6,230	30 mph

¹ = Based on PennDOT TIRe data or peak hour traffic counts and a k-factor

Land Use Context

In Section 1.2 of the Design Manual, Part 2, there is guidance pertaining to defining the land use context(s) for a given area. Based upon review of this information, the land uses surrounding the proposed site best fits Suburban Neighborhood designation, as described below:

Suburban Neighborhood, "predominately low density residential communities... typically arranged in a curvilinear internal system of streets with limited connections to regional road network or surrounding streets. . . .Neighborhoods can include community facilities such as schools, churches, recreational facilities, and some other stores and offices. When suburban houses line and arterial roadway but have their primary access to frontage roads or rear access roads, it is possible to classify this area as a suburban corridor."

Roadway Type

In Section 1.2 of the Design Manual, Part 2, there is guidance pertaining to defining the transportation context(s) for a given area. Comparing the existing condition roadway characteristics to the various options presented in Table 1.2, the study area roadways best fit the following categories, as described below:

Regional Arterial, traffic volumes of 10,000 to 40,000 vehicles per day, intersection spacing of 660 to 1,320 feet, a desired operating speed of 30-55 mph, and a description as follows: "Roadways in this category would be considered "Principal Arterial" in traditional functional classification."

» Pottstown Pike (SR 0100)

Community Collector, traffic volumes of 5,000 to 15,000 vehicles per day, intersection spacing of 300 to 660 feet, a desired operating speed of 25-55 mph, and a description as follows: "often similar in appearance to a community arterial. Typically classified as Major Collector."

» Graphite Mine Road

Local Road, traffic volumes of <3,000 vehicles per day, intersection spacing of 000 to 660 feet, a desired operating speed of 20-30 mph.

» Station Boulevard

Bicycle and Pedestrian Facilities

Based on observations during field visits at the study area intersections, there are no bicycle specific accommodations present. The pedestrian accommodations are detailed below:

- » Pottstown Pike (SR 0100) and Station Boulevard/Park Road – The traffic signal is provided with pedestrian push buttons, pedestrian hand-man signals, pedestrian ramps, and marked crosswalks on all four legs of the intersection.
- » Graphite Mine Road and Station Boulevard – The traffic signal is provided with pedestrian push buttons, pedestrian hand-man signals, pedestrian ramps, and marked crosswalks on the westbound approach of the intersection. Pedestrian activity is restricted via signage on the eastbound, northbound, and southbound approaches.

Mass Transit Facilities

Chester County is provided with public transportation by SEPTA. There is no public transportation via train within ½ mile of the site, and none serving Upper Uwchlan Township.

Crash Data Investigation

Crash data were obtained from PennDOT for the study area intersections. PennDOT defines a reportable crash as follows, “A reportable (crash) is one in which an injury or fatality occurs or if at least one of the vehicles involved requires towing from the scene.” Reportable crashes were tabulated for the five-year time period beginning 01/01/2020 and ending 12/31/2024. For a given intersection, PennDOT considers a crash occurrence of 5 reportable, correctable crashes over a continuous twelve-month period during the past five years to be a threshold value, above which the intersection design should be reviewed to examine if corrective measures can be taken to enhance safety. The number of reportable crashes at the study area intersections is shown in **Table 2**.

TABLE 2
PENNDOT REPORTABLE CRASH DATA

Study Area Intersection	Number of Reportable Crashes				
	2020	2021	2022	2023	2024
Pottstown Pike (SR 0100) & Station Boulevard/Park Road	0	1	1	1	0
Graphite Mine Road & Station Boulevard	0	1	2	4	0
Station Boulevard & Begonia Drive	0	0	0	0	0

Based on a review of the crash data, there was one continuous twelve-month period during the past five years where 5 or more crashes occurred that were deemed correctable at the Graphite Mine Road/Station Boulevard intersection:

- » Graphite Mine Road & Station Boulevard – The signalized intersection had 5 or more crashes that had occurred between October 2022 and September 2023. The primary type of crash was the angle crash. The primary causations of the crashes were driver error, such as running redlight, distracted driving or inexperienced driver.

EXISTING TRAFFIC CONDITIONS

Manual traffic counts were conducted on 15-minute intervals during the weekday morning (7:00 to 9:00 A.M.), weekday evening (4:00 to 6:00 P.M.) and Saturday midday (11:00 A.M. to 1:00 P.M.) peak periods. Peak hours and count dates for the study area intersections are identified in **Table 3**.

TABLE 3
MANUAL TRAFFIC COUNT INFORMATION

Intersection	Date of Traffic Counts	Time Period	Intersection Peak Hour ¹
<i>Pottstown Pike (SR 0100) & Station Boulevard/Park Road</i>	Thursday, May 1, 2025	Weekday A.M.	8:00 to 9:00 A.M.
		Weekday P.M.	4:30 to 5:30 P.M.
	Saturday, May 3, 2025	Saturday Midday	12:00 A.M. to 1:00 P.M.
<i>Station Boulevard & Begonia Drive</i>	Thursday, May 1, 2025	Weekday A.M.	7:30 to 8:30 A.M.
		Weekday P.M.	5:00 to 6:00 P.M.
	Saturday, May 3, 2025	Saturday Midday	12:00 A.M. to 1:00 P.M.
<i>Graphite Mine Road & Station Boulevard</i>	Thursday, May 1, 2025	Weekday A.M.	8:00 to 9:00 A.M.
		Weekday P.M.	4:45 to 5:45 P.M.
	Saturday, May 3, 2025	Saturday Midday	12:00 A.M. to 1:00 P.M.

1. Peak Hour consists of the four consecutive 15-minute intervals where the highest traffic volumes occur.

Existing condition traffic volumes for the weekday A.M., weekday P.M., and Saturday peak hours are illustrated in **Figures 3-5**, respectively. Manual traffic count data sheets are provided in **Appendix B**.

BASE (NO-BUILD) CONDITIONS

Annual Background Growth

A background growth factor for the roadways in the study area was developed based on growth factors for August 2024 to July 2025 obtained from the PennDOT Bureau of Planning and Research (BPR). The PennDOT BPR suggests using a background growth trend factor of 0.44% per year in Chester County for urban non-interstate roadways.

Nearby Proposed Developments

Base (no-build) traffic conditions were calculated to include traffic volumes from proposed developments, which, though not operating under existing conditions, may be operating by the opening year (2026) of the proposed development. The following nearby planned developments were specifically included in this study:

Automotive Service Center is a proposed development consisting of 45,774 s.f. service center space with a 300 space car storage lot. The site is located on the eastern side of Pottstown Pike (SR 0100), opposite Font Road. Trip generation and distribution for this site were developed based on the 4/10/25 TIS prepared by TPD.

Byers Station Parcel 6C is a 100-unit Continuing Care Retirement Facility. The project site is located on the northeast corner of the intersection of Byers Road (SR 1022) and Graphite Mine Road. Trip generation and distribution for this site was developed based on the 3/30/17 TIA prepared by TPD.

100 Greenridge Road is a proposed residential development consisting of 64 single-family homes. The site is located on the north side of Greenridge Road, west of Font Road. Trip generation and distribution for this site were developed based ITE data and existing travel patterns, respectively.

McKee-Fetters Tract is a proposed age-restricted residential development consisting of 97 senior-adult detached homes, and 156 senior adult townhomes. The site is located on the eastern side of Milford Road, south of Patricia Drive and opposite Krauser Road. Trip generation and distributions for this site were obtained from the Transportation Impact Study prepared by TPD, last revised 5/18/17.

The additional traffic volumes due to background growth and background developments were added to the existing traffic data to produce 2026 base (no-build) condition traffic volumes. Base condition volumes for the weekday A.M., weekday P.M., and Saturday midday peak hours are illustrated in **Figures 6-8** for the 2026 future no-build conditions. Trip distributions for the background developments are shown in the volume development worksheets provided in **Appendix C**.

SCHEDULED ROADWAY IMPROVEMENTS

Upper Uwchlan Township Traffic Capital Improvement Program

The following is a summary of roadway improvements identified in the Township's Transportation Capital Improvement Plan at the study area intersection for the Act 209 pre-development and post-development conditions:

Pottstown Pike & Park Road/Station Boulevard

- *Post-Development Conditions*
 - Widen southbound Pottstown Pike to provide a separate right-turn lane
 - Widen westbound Station Boulevard to provide a separate right-turn lane.

Graphite Mine Road & Station Boulevard

- *Post-Development Conditions*
 - Widen northbound Graphite Mine Road for a second through lane;
 - Widen southbound Graphite Mine Road for a second through lane.

In order to provide a conservative evaluation, the above improvements were not included in the evaluation of the study area intersections.

PROPOSED SITE ACCESS

The site will be served by driveways to Begonia Drive, with external access to Station Boulevard via Begonia Drive. No direct access to Route 100 is proposed.

TRIP GENERATION

The trip generation rates for the proposed development were obtained from the manual *Trip Generation*, Eleventh Edition, 2021, an Institute of Transportation Engineers (ITE) Informational Report. For the proposed development, Land Use Codes 565 (Daycare), 822 (Strip Retail Plaza <40k), and 930 (Fast Casual Restaurant) from *Trip Generation* was used to calculate the number of vehicular trips the development will generate during the following time periods: (1) average weekday; (2) weekday A.M. peak hour; (3) weekday P.M. peak hour; and (4) Saturday midday peak hour.

Please note, this analysis assumes that the 10,500 s.f. commercial/retail building will include two (2) restaurants at 1,500 s.f. each. In addition, although the MOB use is proposed for one of the 10,500 s.f. buildings, this analysis utilizes the Retail land use (#822), as this results in a higher trip generation than a MOB use (#720) during the weekday P.M. peak hour and Saturday midday peak hour. During the weekday A.M. peak hour, the MOB use generates slightly more, yet a comparable number of trips, than the retail land use.

In addition, according to the *Trip Generation* manual, not all of the trips generated by the proposed development will be new to the surrounding area. A distinction was made between “new” trips, which are trips made to/from the study area for the express purpose of visiting the site, “pass-by” trips, which are trips made to the site by traffic passing the retail center on the adjacent roadways en route to another destination. The pass-by trips do not add any additional traffic to the study area intersections but will result in shifts in turning movement at the site driveway intersections. **Table 4** shows the trip generation rates, directional and pass-by percentages for the analyzed time periods.

TABLE 4
ITE TRIP GENERATION DATA

Land Use	ITE #	Time Period	Rate/Equation	Entering %	Pass By %
Day Care	565	Average Weekday	$T = 47.62(X)$	50%	--
		Weekday A.M. Peak Hour	$T = 0.66(X) + 8.42$	53%	34% ¹
		Weekday P.M. Peak Hour	$\ln(T) = 0.87\ln(X) + 0.29$	47%	44%
		Saturday Midday Peak Hour	$T = 0.11(X)$	63%	34% ¹
Fast Casual Restaurant	930	Average Weekday	$T = 97.14(X)$	50%	--
		Weekday A.M. Peak Hour	$T = 1.43(X)$	50%	33% ¹
		Weekday P.M. Peak Hour	$T = 12.55(X)$	55%	43% ²
		Saturday Midday Peak Hour	$T = 32.64(X)$	55%	33% ¹
Strip Retail Plaza (<40k)	822	Average Weekday	$T = 42.2(X) + 229.68$	50%	--
		Weekday A.M. Peak Hour	$\ln(T) = 0.66\ln(X) + 1.84$	60%	30% ³
		Weekday P.M. Peak Hour	$T = 6.59(X)$	50%	40% ³
		Saturday Midday Peak Hour	$T = 6.57(X)$	51%	31% ³

T = total trips; *X* = independent variable, ksf = 1000 square feet

¹ = No data available, used 10% less than PM Peak Hour

² = No data available, utilized pass-by rate for LUC 932

³ = No data available, utilized pass-by rate for LUC 821

Internal Trips

For mixed-use developments, the *Trip Generation* manual recommends applying an interaction factor to the site trip generation to determine the quantity of “external trips” (trips from external roadways) and “internal capture trips” (trips between the various uses onsite that do not utilize the external roadways). The calculated trip generation for the proposed development is shown in **Table 5**. Interaction worksheets are provided in **Appendix C**. Please note, this trip generation for this study does not account for any interaction between the adjacent Byers Station homes and proposed development. Interaction between the residential uses and the proposed development would result in a reduced external trip generation when compared to the values in **Table 5**.

TABLE 5
TRIP GENERATION SUMMARY

Land Use	Size	Total Trips	Internal Trips		External Trips			Pass By Trips			New External Trips		
			Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit
Weekday A.M. Peak Hour													
Fast Casual Restaurant	3 ksf	4	1	0	3	1	2	1	1	0	2	0	2
Day Care	10.5 ksf	116	0	0	116	61	55	39	20	19	77	41	36
Strip Retail Plaza (<40k)	18 ksf	42	0	1	41	25	16	12	6	6	29	19	10
Total	--	162	1	1	160	87	73	52	27	25	108	60	48
Weekday P.M. Peak Hour													
Fast Casual Restaurant	3 ksf	38	6	7	25	15	10	11	6	5	14	9	5
Day Care	10.5 ksf	117	0	0	117	55	62	51	26	25	66	29	37
Strip Retail Plaza (<40k)	18 ksf	119	7	6	106	52	54	42	21	21	64	31	33
Total	--	274	13	13	248	122	126	104	53	51	144	69	75
Saturday Midday Peak Hour													
Fast Casual Restaurant	3 ksf	98	21	9	68	33	35	22	11	11	46	22	24
Day Care	10.5 ksf	18	0	0	18	11	7	6	3	3	12	8	4
Strip Retail Plaza (<40k)	18 ksf	118	4	5	110	56	53	34	17	17	76	39	36
Total	--	234	25	14	195	100	95	62	31	31	133	69	64
Average Weekday Peak Hour													
Fast Casual Restaurant	3 ksf	291	54	41	196	92	104	--	--	--	--	--	--
Day Care	10.5 ksf	500	0	0	500	250	250	--	--	--	--	--	--
Strip Retail Plaza (<40k)	18 ksf	989	35	54	900	460	440	--	--	--	--	--	--
Total	--	1780	89	95	1596	802	794	--	--	--	--	--	--

Based on the trip generation analysis summarized in **Table 5**, the proposed development will generate approximately 108 new trips during the weekday A.M. peak hour, 144 new trips during the weekday P.M. peak hour, and 133 new trips during the Saturday midday peak hour.

TRIP DISTRIBUTION

The distribution of trips generated by the proposed development was based on the local road network, the existing traffic patterns, the proposed use of the site, and the site driveway location. The new trips for the proposed development were distributed to the local roadway network based on the percentages shown in **Table 6**.

TABLE 6
TRIP DISTRIBUTION PERCENTAGES – NEW TRIPS

Direction - To/From	Assignment (To/From)	Distribution Percentage
North	via Pottstown Pike	36%
	via Graphite Mine Road	
South	via Pottstown Pike	44%
	via Graphite Mine Road	
East	via Station Boulevard	8%
West	via Park Road	12%

Pass-by trips were established based on the existing traffic patterns in the vicinity of the site and the location and configuration of the site driveways. The percentages used for the distribution of pass-by trips to the project site are shown below in **Table 7**.

TABLE 7
TRIP DISTRIBUTION PERCENTAGES – PASS-BY TRIPS

Direction - To/From	Pass-by Trip Distribution Percentages		
	Weekday A.M.	Weekday P.M.	Saturday Midday
North via Pottstown Pike	24%	42%	32%
South via Pottstown Pike	62%	46%	58%
East via Station Boulevard	8%	4%	5%
West via Station Boulevard	6%	8%	5%

The assignment of site-generated trips for the proposed development during the weekday A.M., weekday P.M., and Saturday midday peak hours are shown in **Figures 9-11**, respectively.

PROJECTED (BUILD) CONDITION TRAFFIC VOLUMES

The site-generated trips for the proposed development were added to the 2026 base (no-build) condition traffic volumes to develop the 2026 projected (build) condition traffic volumes. The 2026 projected condition traffic volumes for the weekday A.M., weekday P.M., and Saturday midday peak hours are shown in **Figure 12-14** respectively.

LEVELS OF SERVICE FOR AN INTERSECTION

For analysis of intersections, level of service is defined in terms of delay, which is a measure of driver discomfort and frustration, fuel consumption, and lost travel time. LOS criteria is stated in terms of control delay per vehicle for a one-hour analysis period. Control delay includes initial deceleration delay, queue move-up time, stopped

delay, and final acceleration delay. The criteria are shown in **Table 8**. Delay, as it relates to level of service, is a complex measure and is dependent upon a number of variables. For signalized intersections, these variables include the quality of vehicle progression, the cycle length, the green time ratio, and the volume/capacity ratio for the lane group in question. For unsignalized intersections, delay is related to the availability of gaps in the flow of traffic on the major street and the driver's discretion in selecting an appropriate gap for a particular movement from the minor street (straight across, left or right turn).

TABLE 8
LEVEL OF SERVICE CRITERIA
UNSIGNALIZED AND SIGNALIZED INTERSECTIONS ¹

Level of Service	Control Delay Per Vehicle (Seconds)	
	Signalized	Unsignalized
A	< 10	< 10
B	> 10 and < 20	> 10 and < 15
C	> 20 and < 35	> 15 and < 25
D	> 35 and < 55	> 25 and < 35
E	> 55 and < 80	> 35 and < 50
F	> 80 or v/c > 1.0	> 50 or v/c > 1.0

¹ Obtained from Exhibits 18-4 and 19-1 of the Transportation Research Board's *Highway Capacity Manual 2010*

CAPACITY ANALYSIS METHODOLOGY

Capacity analyses were conducted for the weekday A.M., weekday P.M., and Saturday midday peak hours at the study area intersections. These analyses were conducted according to the methodologies contained in the 6th edition *Highway Capacity Manual* (HCM) using *Synchro 12* software, a Trafficware product.

The following conditions were analyzed, as applicable:

- Existing Conditions;
- 2026 Base Conditions (Build-out year without development);
- 2026 Projected Conditions (Build-out year with development).

It should be noted that based on methodologies contained in Chapter 10 of PennDOT's Publication 46, TPD adjusted the following 6th edition HCM default values in the *Synchro 12* capacity analysis. These adjustments were made at the signalized intersections within the study area for all time periods based on the study area location being classified as Suburban:

- Base saturation flow rates for signalized intersections. The saturation flow rate was changed from the default value of 1900 to 1800 based on Exhibit 10-9.
- Start-up lost time and extension of effective green time for signalized intersections. The startup lost time was changed from the default value of 2.0 seconds to 2.5 seconds. Based on the total clearance time (yellow plus all-red time) being greater than 5 seconds, the extension of green time was changed from the default value of 2 seconds to 3.5 seconds. These adjusted values were based on Exhibit 10-10.

In addition, capacity analyses were conducted at the proposed site driveway intersections under the 2026 projected conditions. The capacity analysis worksheets are included in **Appendix D**. The PennDOT-approved signal plans are included in **Appendix E**.

PennDOT's Transportation Impact Study Guidelines outlined PennDOT's *Policies and Procedures for Transportation Impact Studies*, found in PennDOT's Publication 282, Appendix A, dated September 2022 contain the following criteria regarding levels of service:

- Page 29 of the Guidelines state that if evaluation of the With Development Horizon Year Scenario to the Without Development Horizon Year Scenario indicates that the overall intersection level of service has dropped, the applicant will be required to mitigate the level of service if the increase in overall intersection delay is greater than 10-seconds. If the overall intersection delay increase is less than or equal to 10-seconds, mitigation of the intersection will not be required.
- Page 29 of the Guidelines state that for mitigation scenarios, applicants are expected to mitigate the overall intersection LOS to the original Without Development LOS; the 10-second delay variance is not applied to mitigation scenarios. Applicants may be required to address available storage and queue lengths at critical movements or approaches even if the overall LOS requirements are met.
- Page 31 of the Guidelines state that if signalization is the preferred alternative for mitigation, overall intersection LOS C in rural areas and LOS D in urban areas is acceptable.
- Page 31 of the Guidelines states new signalized or unsignalized intersection established to serve as access to the development shall be designed to operate at minimum LOS C for rural areas, and minimum LOS D for urban areas.

LEVELS OF SERVICE IN THE STUDY AREA

Level of service (LOS) matrices for the study area intersections are shown in **Table 10** for the weekday A.M., weekday P.M., and Saturday midday peak hours. Per PennDOT standards, the signal timings at the signalized study area intersections have been optimized under base conditions and projected conditions. Please note, the two (2) signalized study area intersections currently operate with traffic adaptive signal equipment which adjusts the signal timings in real-time based on the current traffic patterns.

TABLE 10
LEVEL OF SERVICE DELAY (SECONDS) SUMMARY

Intersection	Approach/ Movement	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Saturday Midday Peak Hour		
		Existing	2026 Conditions		Existing	2026 Conditions		Existing	2026 Conditions	
			Base	Projected		Base	Projected		Base	Projected
Pottstown Pike & Park Road/Station Boulevard	EB L	D	D	D	D	D	D	D	D	D
	EB TR	D	D	D	D	D	C	C	C	C
	WB L	D	D	D	C	C	C	C	C	C
	WB TR	D	D	D	C	C	C	C	C	C
	NB L	B	C	C	B	C	C	B	C	C
	NB TR	A	A	A	A	A	A	A	A	A
	SB L	A	B	B	B	B	B	A	A	A
	SB TR	C	C	C	C	C	C	C	C	C
	ILOS	C (25.6)	C (27.4)	C (29.4)	C (22.8)	C (25.1)	C (27.2)	C (21.7)	C (24.3)	C (24.5)
Graphite Mine & Station Boulevard	EB L	C	C	C	A	A	A	C	C	C
	EB TR	C	C	C	D	D	D	C	C	C
	WB L	D	D	D	E	E	E	C	C	C
	WB TR	C	C	C	D	D	D	C	C	C
	NB L	B	B	B	A	A	A	A	A	A
	NB T	B	B	B	A	A	A	A	A	A
	NB R	B	B	B	A	A	A	A	A	A
	SB L	C	C	C	B	B	B	A	A	A
	SB TR	B	B	B	A	A	A	A	A	A
	ILOS	C (28.0)	C (27.9)	C (28.1)	B (15.8)	B (16.1)	B (17.4)	A (9.6)	A (9.7)	B (10.2)
Station Boulevard & Begonia Drive	EB L	A	A	A	A	A	A	A	A	A
	SB LR	B	B	C	A	A	B	A	A	B
	ILOS	A (4.2)	A (4.1)	A (7.5)	A (1.3)	A (1.2)	A (5.0)	A (1.4)	A (1.3)	A (4.8)

Base = No-Build scenario; Projected = Build scenario; ILOS = Overall Intersection Level of Service

As shown in **Table 10**, all levels of service at the study area intersection comply with the requirements outlined in PennDOT's TIS Guidelines.

95TH PERCENTILE QUEUE ANALYSIS

Queue analyses were conducted at the study area intersections using *Synchro 12* software. For this analysis, the 95th percentile queue is defined as the queue length that is exceeded in 5% of the signal cycles. As an example, for a signal with a 90-second cycle, this means that the 95th percentile queue length will be exceeded during 2 of the 40 signal cycles that occur during the peak hour. The queue analysis results are summarized in **Tables 11** for the analyzed peak hours.

TABLE 11
95TH PERCENTILE QUEUE ANALYSIS

Intersection	Movement	Available Storage	2026 Conditions					
			Weekday A.M. Peak Hour		Weekday P.M. Peak Hour		Saturday Midday Peak Hour	
			Base	Projected	Base	Projected	Base	Projected
Pottstown Pike & Park Road/Station Boulevard	EB L	75	203	203	208	208	148	158
	EB TR	200+	115	120	70	75	65	73
	WB L	114	25	68	10	40	13	43
	WB TR	200+	103	148	83	118	53	93
	NB L	355	93	93	205	200	123	115
	NB TR	300+	83	100	195	225	80	90
	SB L	200	33	55	3	30	3	18
Graphite Mine & Station Boulevard	SB TR	700+	663	685	578	605	533	523
	EB L	100	0	0	0	0	0	0
	EB TR	200+	163	178	85	113	53	63
	WB L	210	398	400	243	258	85	85
	WB TR	100+	55	60	63	70	23	25
	NB L	95	33	43	25	30	13	20
	NB T	400+	255	260	353	353	133	143
	NB R	95	65	68	58	58	25	28
	SB L	90	0	0	5	5	5	5
Station Boulevard & Begonia Drive	SB TR	200+	40	48	48	50	60	68
	EB L	100+	3	8	0	8	0	5
	SB LR	100+	23	63	3	20	3	13

Base = No-Build scenario; Projected = Build scenario

As shown in **Table 11**, adequate queue storage will be provided for the turn lanes in 2026 with the proposed development or there will be no discernable change between base and projected conditions (i.e. one to two vehicles). In addition, the existing traffic adaptive signal equipment can adjust the signal timings in real-time based on the current traffic patterns. Queue analysis worksheets are included with the capacity analysis worksheets provided in **Appendix E**.

AUXILIARY TURN LANE WARRANT ANALYSIS

TPD evaluated auxiliary turn lane warrants at the Begonia Drive intersection with Station Boulevard. The warrant analysis methodology contained within Chapter 11 of PennDOT's *Publication 46*, Section 11.16 and Strike-Off Letter 470-08-07 was utilized for this evaluation. **Table 12** summarizes the results of the auxiliary turn lane analysis. The calculations for the auxiliary turn lane warrants are included in **Appendix F**.

TABLE 12
AUXILIARY TURN LANE ANALYSIS SUMMARY

Intersection	Auxiliary Lane	Warrant Satisfied?	Required Lane Length	Proposed Lane Length
Station Boulevard & Begonia Drive	EB Left-Turn Lane	No	--	Exists as a TWLTL
	WB Right-Turn Lane	No	--	--

CONCLUSIONS

The conclusions for this Transportation Impact Assessment are identified in the Executive Summary.

APPENDIX A:

Study Area Photographs





Direction / Road:	<u>Nb. Route 100</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Nb. Route 100</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Sb. Route 100</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Sb. Route 100</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Eb. Park Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Eb. Park Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Sb. Begonia Drive</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Sb. Begonia Drive</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Eb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Eb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Nb. Graphite Mill Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Nb. Graphite Mill Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Sb. Graphite Mill Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Sb. Graphite Mill Road</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Eb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Eb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>50 feet</u>



Direction / Road:	<u>Wb. Station Blvd</u>
Approach / Departure:	<u>Approach</u>
Distance:	<u>200 Feet</u>

APPENDIX B:

Manual Traffic Count Printouts





Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 1

Turning Movement Data

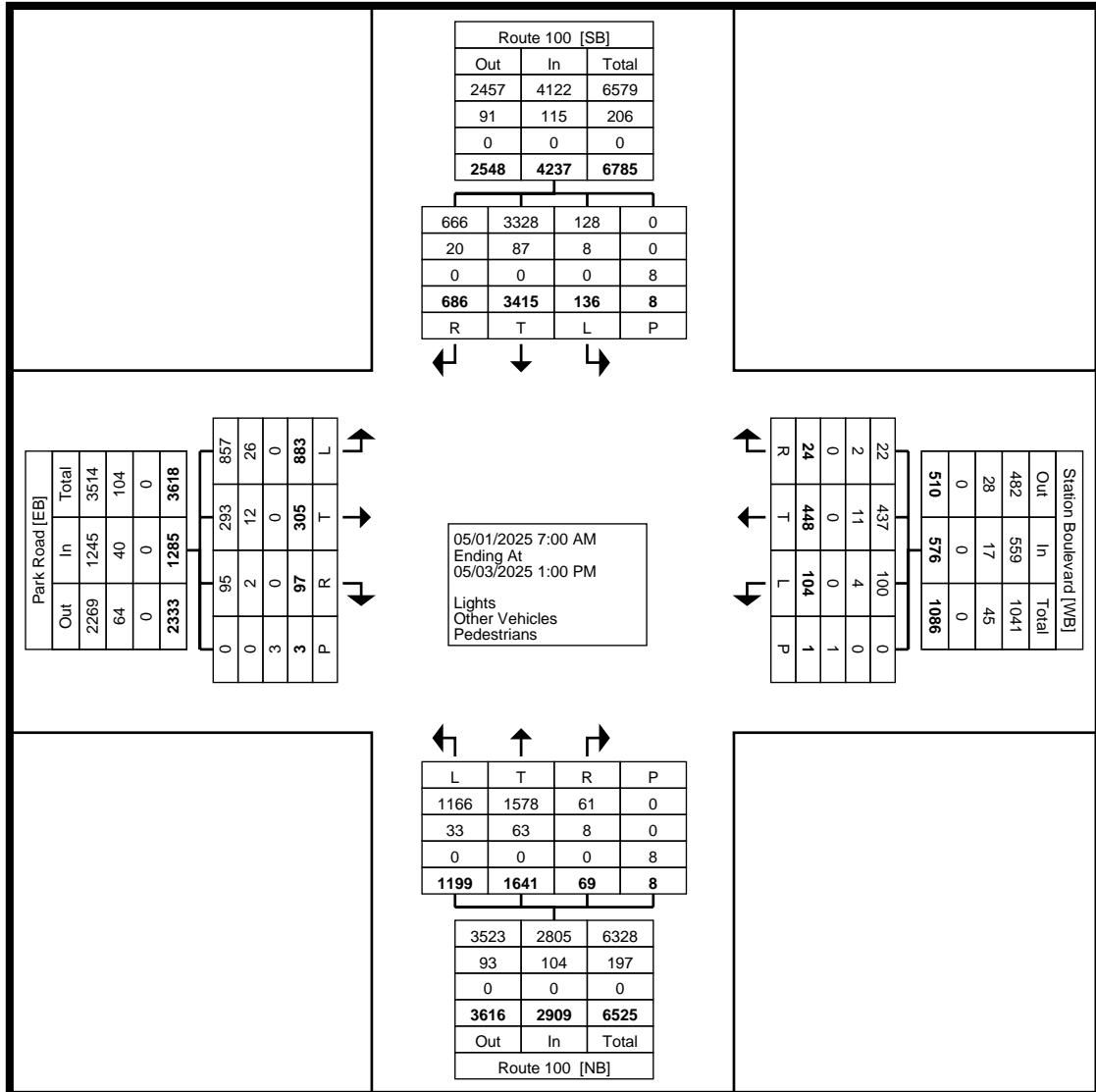
Start Time	Park Road Eastbound						Station Boulevard Westbound						Route 100 Northbound						Route 100 Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
7:00 AM	12	5	3	1	0	21	4	15	1	0	0	20	21	24	1	0	0	46	15	187	28	0	3	230	317
7:15 AM	31	11	2	0	0	44	6	17	1	0	0	24	7	32	0	0	0	39	13	151	19	2	0	185	292
7:30 AM	23	12	1	0	0	36	11	26	0	0	0	37	23	34	2	0	0	59	18	141	23	3	0	185	317
7:45 AM	32	18	4	0	0	54	5	18	0	0	0	23	17	50	0	0	0	67	8	160	13	2	0	183	327
Hourly Total	98	46	10	1	0	155	26	76	2	0	0	104	68	140	3	0	0	211	54	639	83	7	3	783	1253
8:00 AM	35	12	9	0	0	56	2	14	1	0	1	17	27	43	4	0	0	74	6	182	18	0	0	206	353
8:15 AM	31	20	5	0	0	56	3	13	0	0	0	16	31	45	0	0	0	76	18	171	13	0	0	202	350
8:30 AM	33	12	5	0	0	50	10	21	0	0	0	31	35	44	3	0	0	82	17	153	30	1	0	201	364
8:45 AM	43	14	8	0	0	65	3	21	0	0	0	24	33	50	10	0	0	93	18	164	19	3	0	204	386
Hourly Total	142	58	27	0	0	227	18	69	1	0	1	88	126	182	17	0	0	325	59	670	80	4	0	813	1453
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	42	18	3	0	0	63	9	27	1	2	0	39	78	92	6	0	2	176	3	107	23	1	0	134	412
4:15 PM	47	11	0	0	0	58	2	18	2	1	0	23	74	110	1	0	3	185	1	116	40	1	0	158	424
4:30 PM	45	16	3	0	0	64	4	19	2	1	0	26	72	102	3	0	0	177	0	123	32	1	0	156	423
4:45 PM	37	10	0	0	0	47	6	24	0	0	0	30	82	107	4	1	0	194	1	136	35	2	0	174	445
Hourly Total	171	55	6	0	0	232	21	88	5	4	0	118	306	411	14	1	5	732	5	482	130	5	0	622	1704
5:00 PM	34	17	3	0	0	54	1	36	1	0	0	38	65	107	5	0	0	177	0	141	34	0	0	175	444
5:15 PM	53	10	6	0	0	69	3	17	0	0	0	20	67	117	1	0	0	185	2	136	27	1	0	166	440
5:30 PM	36	14	2	0	0	52	5	28	0	0	0	33	79	78	6	0	1	163	2	111	38	0	0	151	399
5:45 PM	53	18	2	1	0	74	2	32	2	0	0	36	86	81	1	0	1	168	2	119	35	0	0	156	434
Hourly Total	176	59	13	1	0	249	11	113	3	0	0	127	297	383	13	0	2	693	6	507	134	1	0	648	1717
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
11:00 AM	44	10	8	0	0	62	3	15	3	0	0	21	61	62	1	0	0	124	3	129	25	4	0	161	368
11:15 AM	48	10	8	0	0	66	7	12	0	0	0	19	49	66	0	0	0	115	1	129	27	1	5	158	358
11:30 AM	27	10	4	1	0	42	3	10	0	0	0	13	56	72	2	0	0	130	1	129	29	3	0	162	347
11:45 AM	34	7	1	0	0	42	2	13	0	2	0	17	45	67	4	1	0	117	1	130	18	1	0	150	326
Hourly Total	153	37	21	1	0	212	15	50	3	2	0	70	211	267	7	1	0	486	6	517	99	9	5	631	1399
12:00 PM	40	13	3	0	0	56	5	9	2	1	0	17	45	66	0	1	0	112	2	166	29	1	0	198	383
12:15 PM	35	9	6	0	0	50	5	15	1	0	0	21	60	56	3	0	0	119	2	157	30	0	0	189	379
12:30 PM	33	15	4	0	0	52	3	12	0	0	0	15	40	70	3	1	0	114	1	134	43	2	0	180	361
12:45 PM	35	13	4	0	3	52	0	16	0	0	0	16	46	66	5	0	1	117	1	143	27	2	0	173	358
Hourly Total	143	50	17	0	3	210	13	52	3	1	0	69	191	258	11	2	1	462	6	600	129	5	0	740	1481
Grand Total	883	305	94	3	3	1285	104	448	17	7	1	576	1199	1641	65	4	8	2909	136	3415	655	31	8	4237	9007
Approach %	68.7	23.7	7.3	0.2	-	-	18.1	77.8	3.0	1.2	-	-	41.2	56.4	2.2	0.1	-	-	3.2	80.6	15.5	0.7	-	-	-
Total %	9.8	3.4	1.0	0.0	-	14.3	1.2	5.0	0.2	0.1	-	6.4	13.3	18.2	0.7	0.0	-	32.3	1.5	37.9	7.3	0.3	-	47.0	-
Lights	857	293	92	3	-	1245	100	437	15	7	-	559	1166	1578	58	3	-	2805	128	3328	637	29	-	4122	8731
% Lights	97.1	96.1	97.9	100.0	-	96.9	96.2	97.5	88.2	100.0	-	97.0	97.2	96.2	89.2	75.0	-	96.4	94.1	97.5	97.3	93.5	-	97.3	96.9
Other Vehicles	26	12	2	0	-	40	4	11	2	0	-	17	33	63	7	1	-	104	8	87	18	2	-	115	276
% Other Vehicles	2.9	3.9	2.1	0.0	-	3.1	3.8	2.5	11.8	0.0	-	3.0	2.8	3.8	10.8	25.0	-	3.6	5.9	2.5	2.7	6.5	-	2.7	3.1
Pedestrians	-	-	-	-	3	-	-	-	-	-	1	-	-	-	-	-	8	-	-	-	-	-	8	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



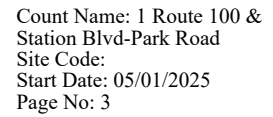
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 2



Turning Movement Data Plot

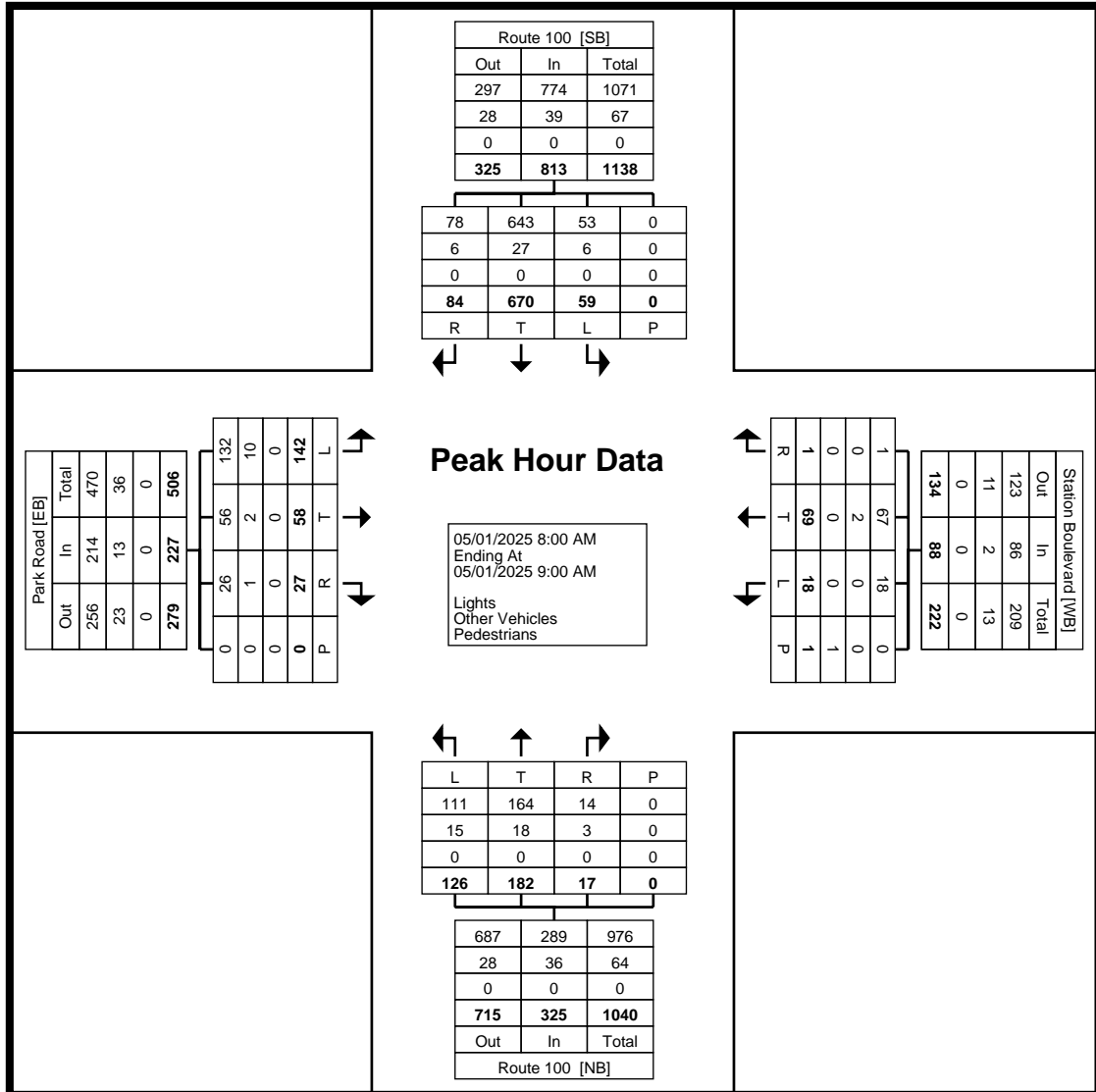




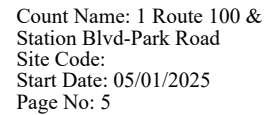
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 4



Turning Movement Peak Hour Data Plot (8:00 AM)

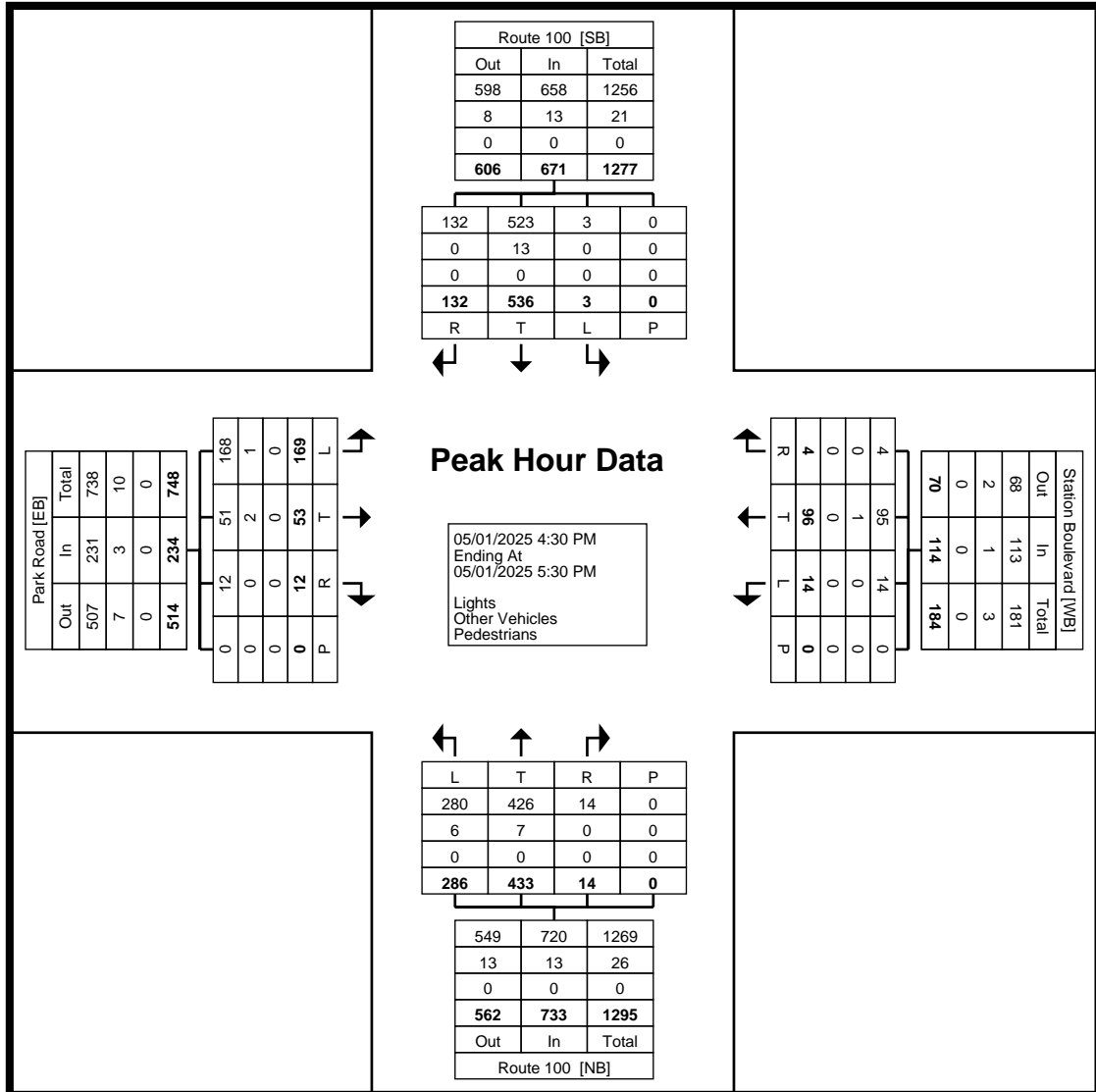
[illegible]



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 6



Turning Movement Peak Hour Data Plot (4:30 PM)



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 7

Turning Movement Peak Hour Data (12:00 PM)

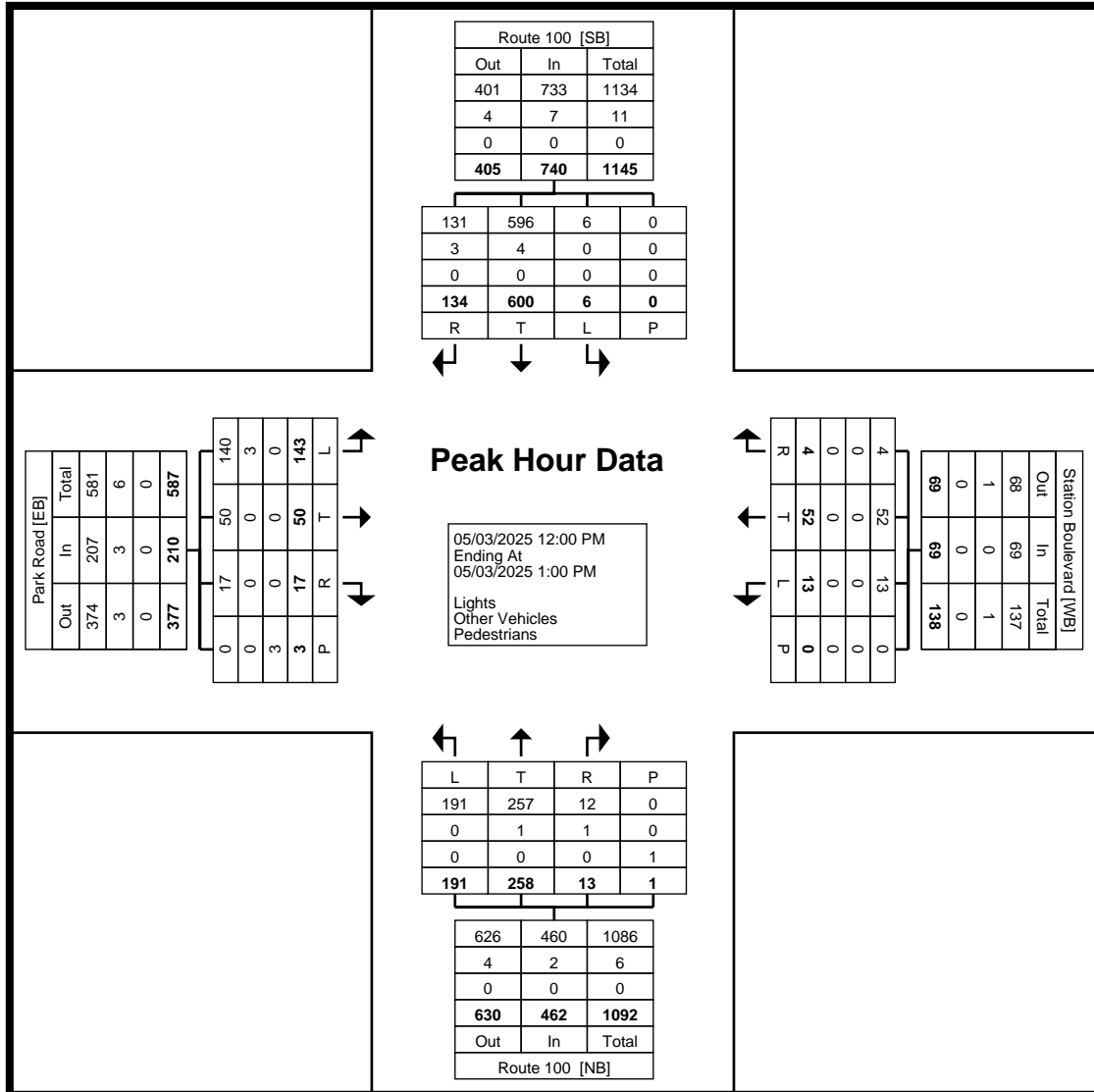
Start Time	Park Road Eastbound						Station Boulevard Westbound						Route 100 Northbound						Route 100 Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
12:00 PM	40	13	3	0	0	56	5	9	2	1	0	17	45	66	0	1	0	112	2	166	29	1	0	198	383
12:15 PM	35	9	6	0	0	50	5	15	1	0	0	21	60	56	3	0	0	119	2	157	30	0	0	189	379
12:30 PM	33	15	4	0	0	52	3	12	0	0	0	15	40	70	3	1	0	114	1	134	43	2	0	180	361
12:45 PM	35	13	4	0	3	52	0	16	0	0	0	16	46	66	5	0	1	117	1	143	27	2	0	173	358
Total	143	50	17	0	3	210	13	52	3	1	0	69	191	258	11	2	1	462	6	600	129	5	0	740	1481
Approach %	68.1	23.8	8.1	0.0	-	-	18.8	75.4	4.3	1.4	-	-	41.3	55.8	2.4	0.4	-	-	0.8	81.1	17.4	0.7	-	-	-
Total %	9.7	3.4	1.1	0.0	-	14.2	0.9	3.5	0.2	0.1	-	4.7	12.9	17.4	0.7	0.1	-	31.2	0.4	40.5	8.7	0.3	-	50.0	-
PHF	0.894	0.833	0.708	0.000	-	0.938	0.650	0.813	0.375	0.250	-	0.821	0.796	0.921	0.550	0.500	-	0.971	0.750	0.904	0.750	0.625	-	0.934	0.967
Lights	140	50	17	0	-	207	13	52	3	1	-	69	191	257	10	2	-	460	6	596	126	5	-	733	1469
% Lights	97.9	100.0	100.0	-	-	98.6	100.0	100.0	100.0	100.0	-	100.0	100.0	99.6	90.9	100.0	-	99.6	100.0	99.3	97.7	100.0	-	99.1	99.2
Other Vehicles	3	0	0	0	-	3	0	0	0	0	-	0	0	1	1	0	-	2	0	4	3	0	-	7	12
% Other Vehicles	2.1	0.0	0.0	-	-	1.4	0.0	0.0	0.0	0.0	-	0.0	0.0	0.4	9.1	0.0	-	0.4	0.0	0.7	2.3	0.0	-	0.9	0.8
Pedestrians	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 1 Route 100 &
Station Blvd-Park Road
Site Code:
Start Date: 05/01/2025
Page No: 8



Turning Movement Peak Hour Data Plot (12:00 PM)



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 2 Begonia Drive &
Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 1

Turning Movement Data

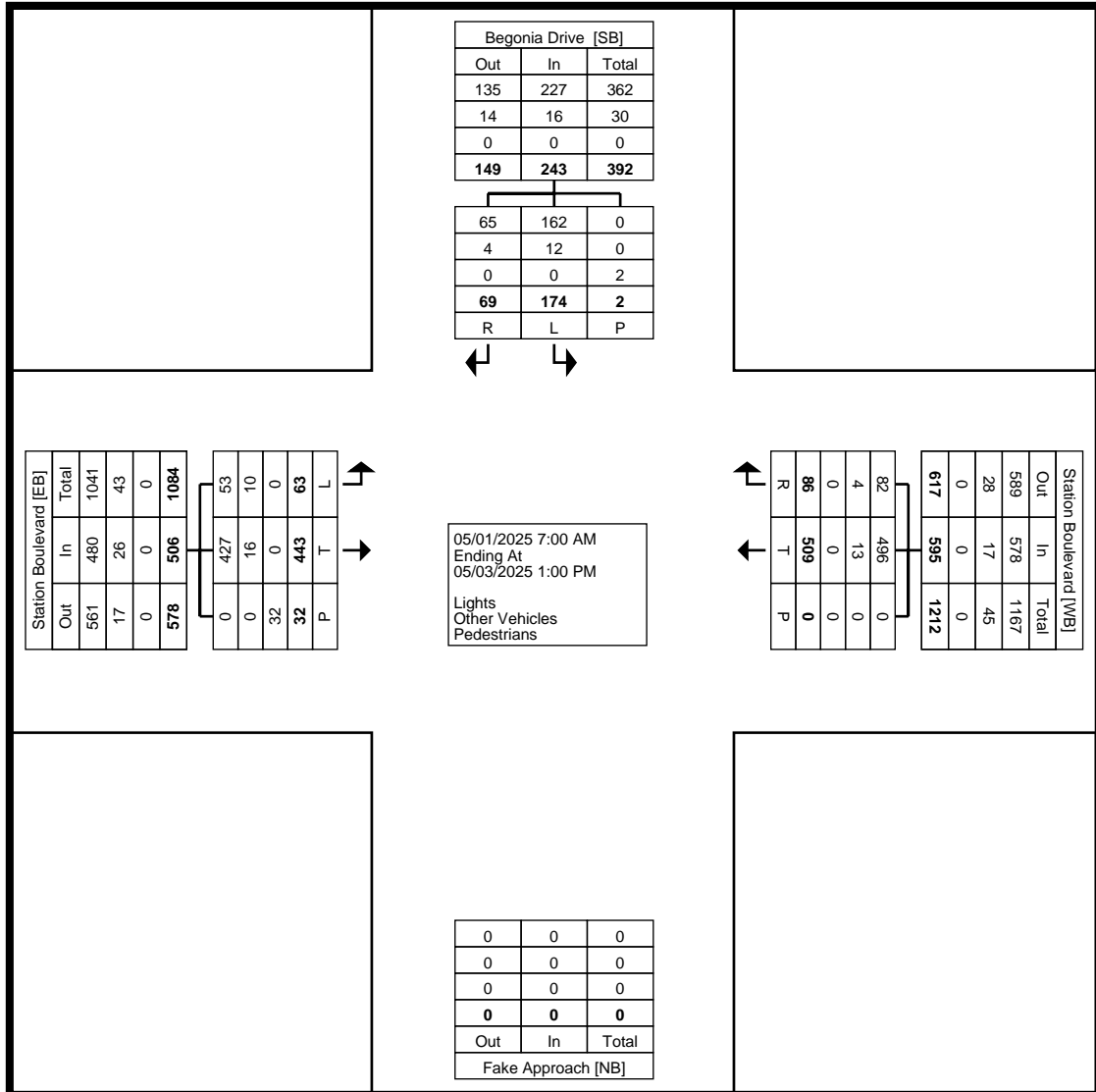
Start Time	Station Boulevard Eastbound				Station Boulevard Westbound				Begonia Drive Southbound				Int. Total
	Left	Thru	Peds	App. Total	Thru	Right	Peds	App. Total	Left	Right	Peds	App. Total	
7:00 AM	2	19	4	21	20	2	0	22	6	0	1	6	49
7:15 AM	0	23	2	23	17	2	0	19	11	8	0	19	61
7:30 AM	3	32	2	35	24	0	0	24	48	11	0	59	118
7:45 AM	5	22	2	27	21	8	0	29	6	1	0	7	63
Hourly Total	10	96	10	106	82	12	0	94	71	20	1	91	291
8:00 AM	6	16	1	22	17	3	0	20	12	2	0	14	56
8:15 AM	2	35	0	37	14	3	0	17	14	3	0	17	71
8:30 AM	3	25	2	28	22	2	0	24	16	7	0	23	75
8:45 AM	8	33	4	41	25	6	0	31	12	1	1	13	85
Hourly Total	19	109	7	128	78	14	0	92	54	13	1	67	287
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	3	24	4	27	28	7	0	35	1	6	0	7	69
4:15 PM	2	11	0	13	26	3	0	29	4	2	0	6	48
4:30 PM	2	17	0	19	22	7	0	29	2	3	0	5	53
4:45 PM	3	14	1	17	26	1	0	27	3	3	0	6	50
Hourly Total	10	66	5	76	102	18	0	120	10	14	0	24	220
5:00 PM	2	19	0	21	35	4	0	39	5	0	0	5	65
5:15 PM	3	9	1	12	22	7	0	29	2	1	0	3	44
5:30 PM	3	19	1	22	25	5	0	30	3	6	0	9	61
5:45 PM	2	18	2	20	36	4	0	40	2	4	0	6	66
Hourly Total	10	65	4	75	118	20	0	138	12	11	0	23	236
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-
11:00 AM	1	13	0	14	21	6	0	27	3	2	0	5	46
11:15 AM	0	12	0	12	16	4	0	20	5	1	0	6	38
11:30 AM	3	10	0	13	12	0	0	12	3	1	0	4	29
11:45 AM	2	10	0	12	14	4	0	18	2	3	0	5	35
Hourly Total	6	45	0	51	63	14	0	77	13	7	0	20	148
12:00 PM	3	15	5	18	13	3	0	16	5	3	0	8	42
12:15 PM	2	10	0	12	22	3	0	25	3	0	0	3	40
12:30 PM	3	17	0	20	13	1	0	14	5	1	0	6	40
12:45 PM	0	20	1	20	18	1	0	19	1	0	0	1	40
Hourly Total	8	62	6	70	66	8	0	74	14	4	0	18	162
Grand Total	63	443	32	506	509	86	0	595	174	69	2	243	1344
Approach %	12.5	87.5	-	-	85.5	14.5	-	-	71.6	28.4	-	-	-
Total %	4.7	33.0	-	37.6	37.9	6.4	-	44.3	12.9	5.1	-	18.1	-
Lights	53	427	-	480	496	82	-	578	162	65	-	227	1285
% Lights	84.1	96.4	-	94.9	97.4	95.3	-	97.1	93.1	94.2	-	93.4	95.6
Other Vehicles	10	16	-	26	13	4	-	17	12	4	-	16	59
% Other Vehicles	15.9	3.6	-	5.1	2.6	4.7	-	2.9	6.9	5.8	-	6.6	4.4
Pedestrians	-	-	32	-	-	-	0	-	-	-	2	-	-
% Pedestrians	-	-	100.0	-	-	-	-	-	-	-	100.0	-	-



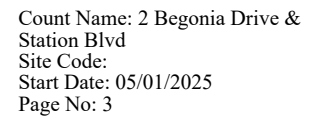
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 2 Begonia Drive &
Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 2



Turning Movement Data Plot

[illegible]



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 2 Begonia Drive &
Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 5

Turning Movement Peak Hour Data (5:00 PM)

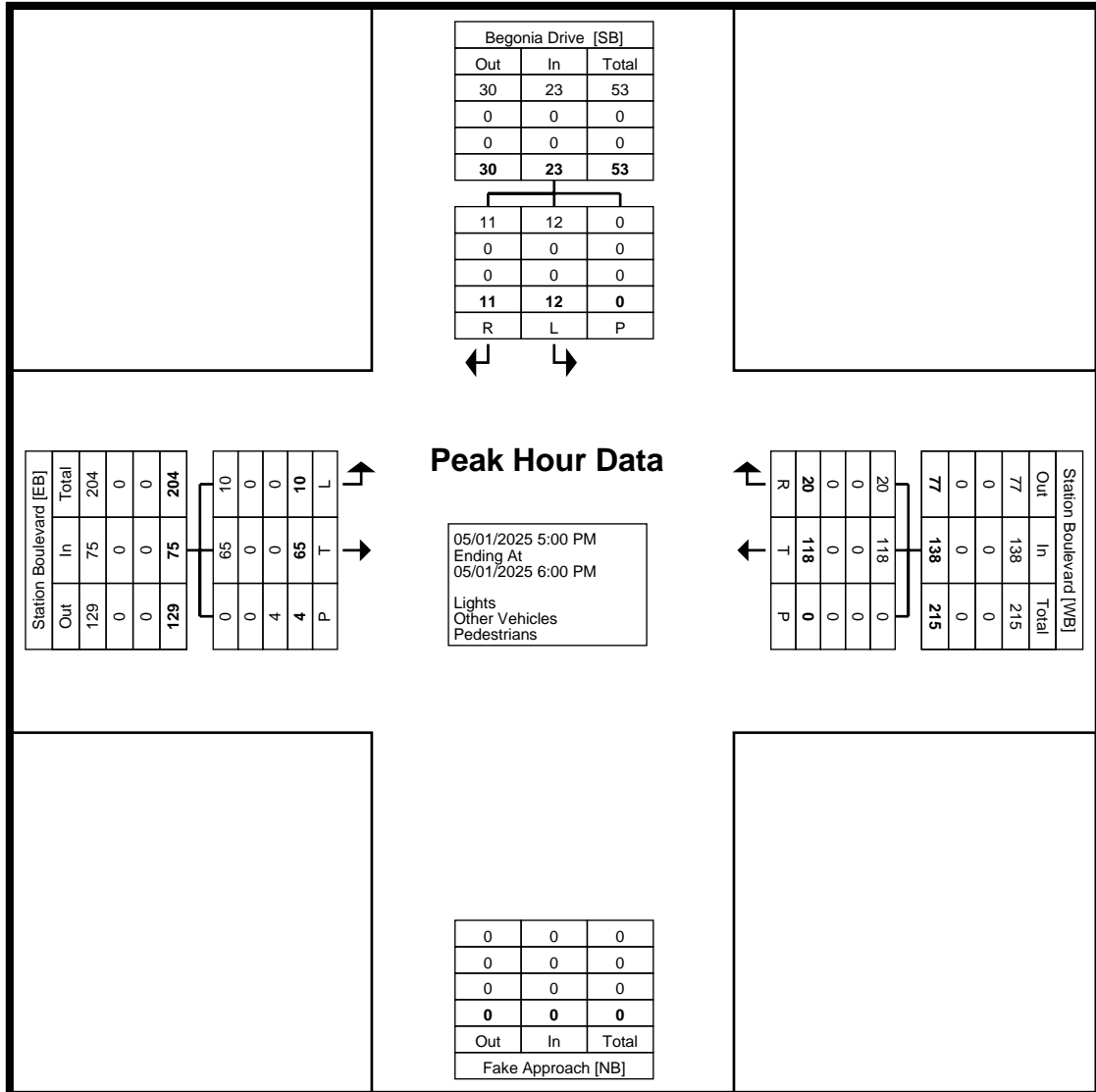
[illegible]



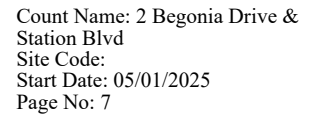
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 2 Begonia Drive &
Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 6



Turning Movement Peak Hour Data Plot (5:00 PM)

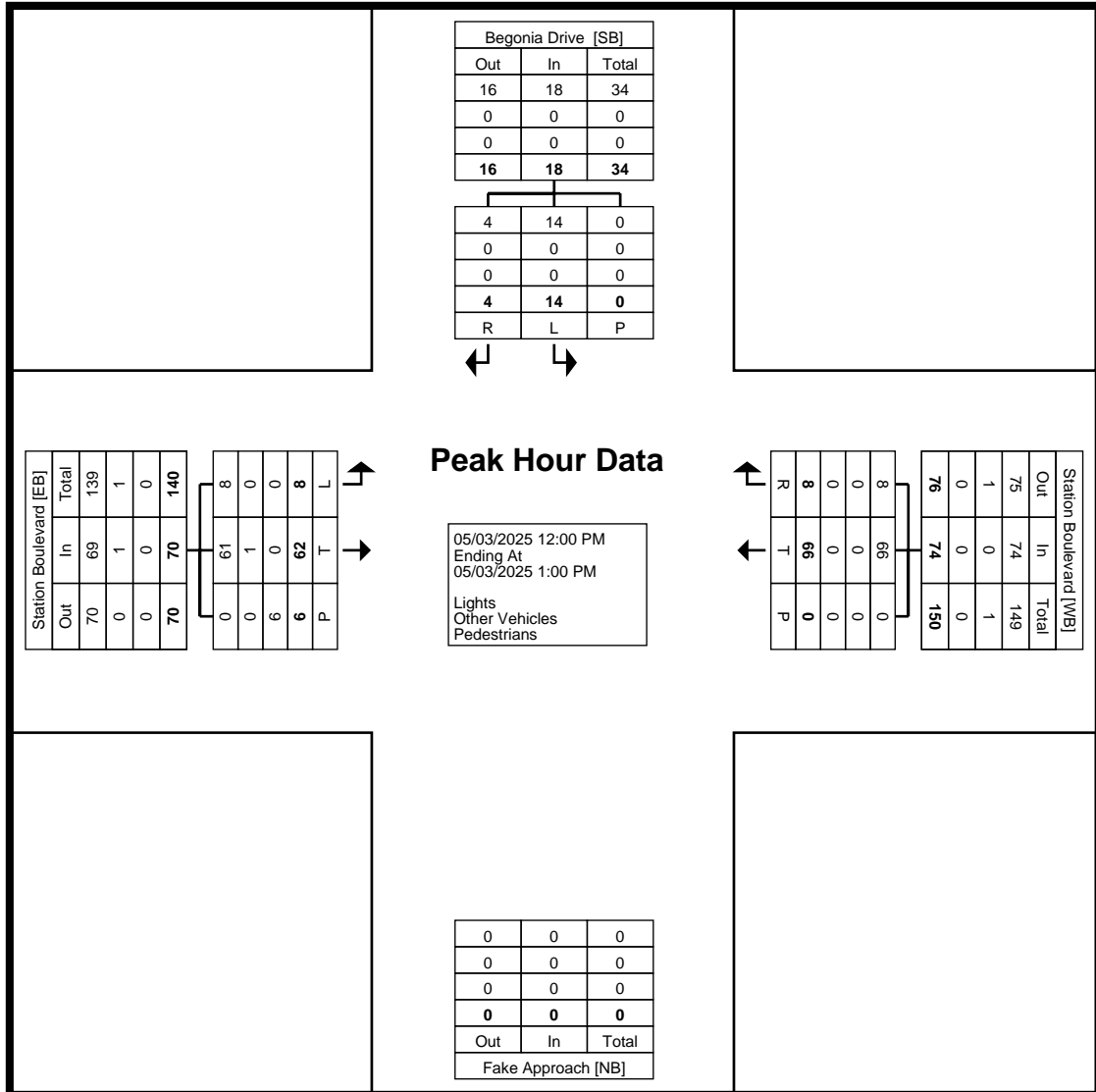
[illegible]



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 2 Begonia Drive &
Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 8



Turning Movement Peak Hour Data Plot (12:00 PM)



Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 3 Graphite Mine Road
& Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 1

Turning Movement Data

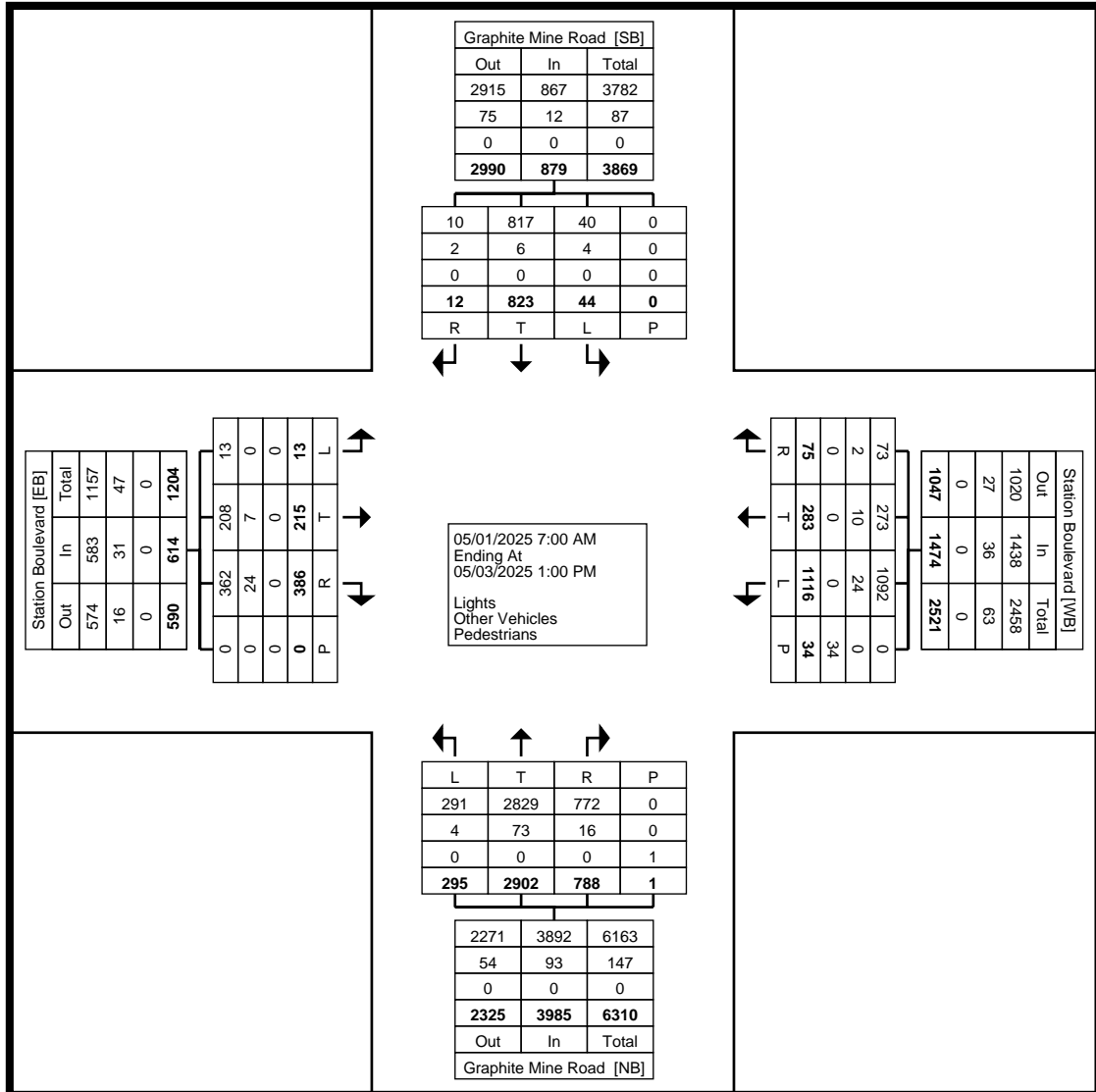
Start Time	Station Boulevard Eastbound						Station Boulevard Westbound						Graphite Mine Road Northbound						Graphite Mine Road Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
7:00 AM	0	2	14	8	0	24	51	19	4	1	1	75	2	44	9	1	0	56	0	7	0	1	0	8	163
7:15 AM	0	2	23	10	0	35	52	9	4	0	0	65	6	46	5	4	0	61	0	7	1	0	0	8	169
7:30 AM	0	3	49	29	0	81	78	20	1	0	3	99	6	54	7	4	0	71	2	23	0	0	0	25	276
7:45 AM	0	6	19	3	0	28	92	16	3	0	1	111	9	58	28	3	0	98	0	10	1	0	0	11	248
Hourly Total	0	13	105	50	0	168	273	64	12	1	5	350	23	202	49	12	0	286	2	47	2	1	0	52	856
8:00 AM	0	6	9	13	0	28	66	7	1	1	0	75	11	73	21	8	0	113	0	15	1	0	0	16	232
8:15 AM	0	8	26	10	0	44	65	8	3	2	1	78	10	71	15	4	0	100	1	5	0	0	0	6	228
8:30 AM	0	11	27	4	0	42	81	16	3	0	0	100	5	81	17	2	0	105	0	18	0	0	0	18	265
8:45 AM	1	7	28	9	0	45	86	15	3	1	1	105	16	74	22	3	0	115	0	12	0	0	0	12	277
Hourly Total	1	32	90	36	0	159	298	46	10	4	2	358	42	299	75	17	0	433	1	50	1	0	0	52	1002
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	0	20	3	2	0	25	30	20	3	0	0	53	18	172	29	5	0	224	2	34	0	0	0	36	338
4:15 PM	0	9	2	3	0	14	27	10	2	1	0	40	17	174	25	9	0	225	4	38	1	0	0	43	322
4:30 PM	1	9	6	2	0	18	30	12	5	1	0	48	15	178	35	7	0	235	2	29	0	0	0	31	332
4:45 PM	0	10	5	2	0	17	49	14	0	1	1	64	13	216	37	11	0	277	3	41	0	1	0	45	403
Hourly Total	1	48	16	9	0	74	136	56	10	3	1	205	63	740	126	32	0	961	11	142	1	1	0	155	1395
5:00 PM	0	14	4	6	0	24	37	12	3	0	2	52	29	202	46	7	0	284	3	42	0	0	0	45	405
5:15 PM	0	7	4	1	0	12	46	10	1	0	2	57	16	225	44	11	0	296	0	48	0	0	0	48	413
5:30 PM	0	13	4	4	0	21	37	12	3	0	3	52	18	186	45	8	0	257	2	31	0	0	0	33	363
5:45 PM	3	12	5	2	0	22	46	15	3	1	3	65	26	170	58	8	0	262	4	34	0	0	0	38	387
Hourly Total	3	46	17	13	0	79	166	49	10	1	10	226	89	783	193	34	0	1099	9	155	0	0	0	164	1568
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
11:00 AM	4	8	1	3	0	16	30	16	3	1	1	50	13	101	27	11	0	152	3	33	0	1	0	37	255
11:15 AM	2	6	6	2	0	16	37	11	2	2	1	52	8	108	19	5	0	140	1	46	0	0	0	47	255
11:30 AM	0	7	5	2	0	14	25	8	2	0	9	35	3	91	21	10	0	125	2	55	0	0	0	57	231
11:45 AM	0	8	2	0	0	10	39	8	4	1	1	52	10	110	22	10	1	152	1	41	0	0	0	42	256
Hourly Total	6	29	14	7	0	56	131	43	11	4	12	189	34	410	89	36	1	569	7	175	0	1	0	183	997
12:00 PM	0	10	6	4	0	20	28	5	2	0	1	35	10	119	34	0	0	163	2	49	1	0	0	52	270
12:15 PM	1	8	3	3	0	15	31	9	1	1	2	42	12	122	32	0	0	166	3	104	4	0	0	111	334
12:30 PM	1	12	3	5	0	21	21	7	1	1	0	30	8	113	26	0	0	147	2	56	0	0	0	58	256
12:45 PM	0	17	2	3	0	22	32	4	1	2	1	39	14	114	33	0	0	161	7	45	0	0	0	52	274
Hourly Total	2	47	14	15	0	78	112	25	5	4	4	146	44	468	125	0	0	637	14	254	5	0	0	273	1134
Grand Total	13	215	256	130	0	614	1116	283	58	17	34	1474	295	2902	657	131	1	3985	44	823	9	3	0	879	6952
Approach %	2.1	35.0	41.7	21.2	-	-	75.7	19.2	3.9	1.2	-	-	7.4	72.8	16.5	3.3	-	-	5.0	93.6	1.0	0.3	-	-	-
Total %	0.2	3.1	3.7	1.9	-	8.8	16.1	4.1	0.8	0.2	-	21.2	4.2	41.7	9.5	1.9	-	57.3	0.6	11.8	0.1	0.0	-	12.6	-
Lights	13	208	238	124	-	583	1092	273	56	17	-	1438	291	2829	643	129	-	3892	40	817	8	2	-	867	6780
% Lights	100.0	96.7	93.0	95.4	-	95.0	97.8	96.5	96.6	100.0	-	97.6	98.6	97.5	97.9	98.5	-	97.7	90.9	99.3	88.9	66.7	-	98.6	97.5
Other Vehicles	0	7	18	6	-	31	24	10	2	0	-	36	4	73	14	2	-	93	4	6	1	1	-	12	172
% Other Vehicles	0.0	3.3	7.0	4.6	-	5.0	2.2	3.5	3.4	0.0	-	2.4	1.4	2.5	2.1	1.5	-	2.3	9.1	0.7	11.1	33.3	-	1.4	2.5
Pedestrians	-	-	-	-	0	-	-	-	-	-	34	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



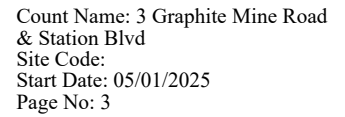
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 3 Graphite Mine Road
& Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 2



Turning Movement Data Plot

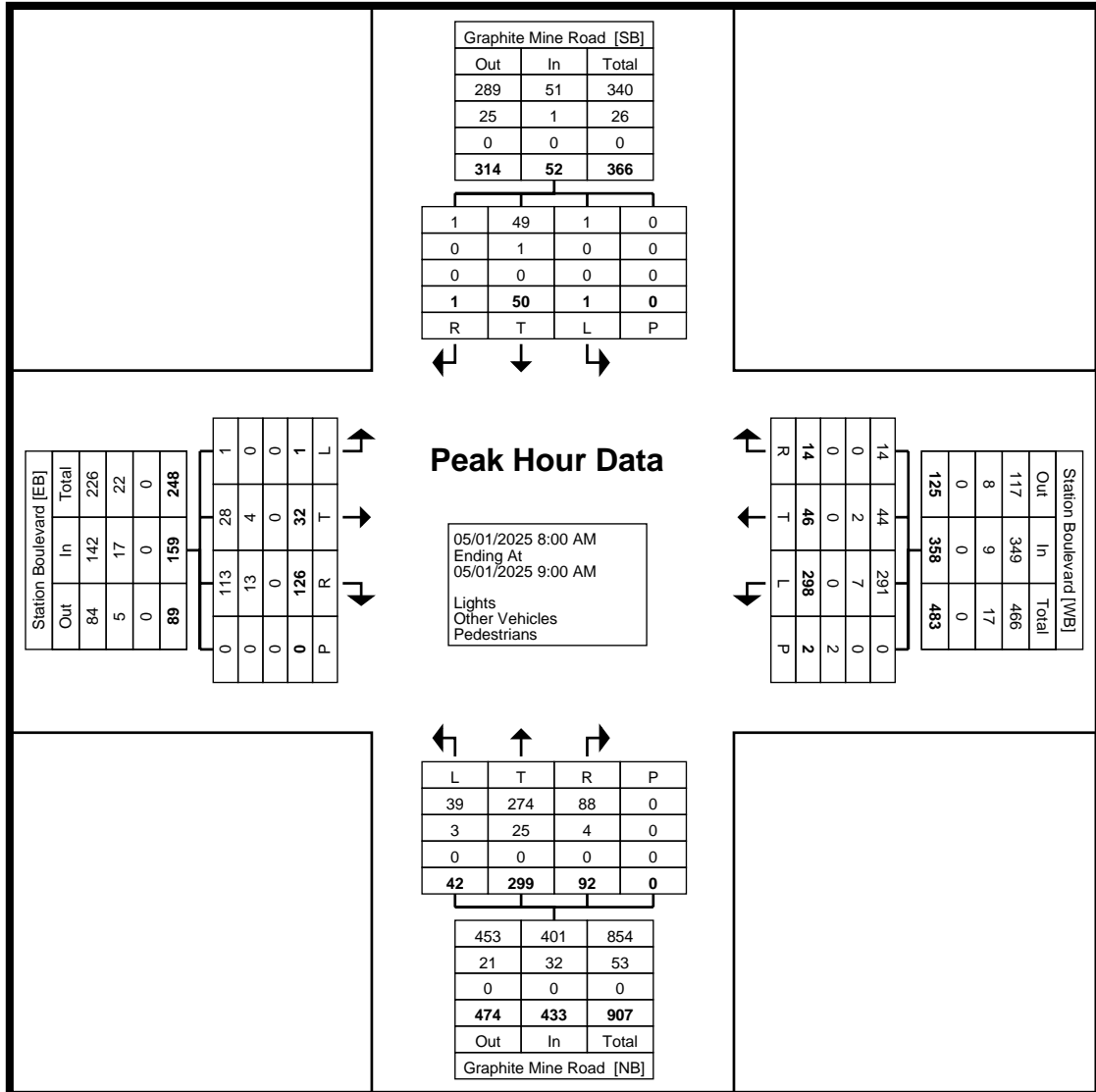
[illegible]



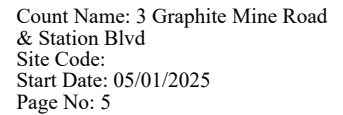
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 3 Graphite Mine Road
& Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 4



Turning Movement Peak Hour Data Plot (8:00 AM)

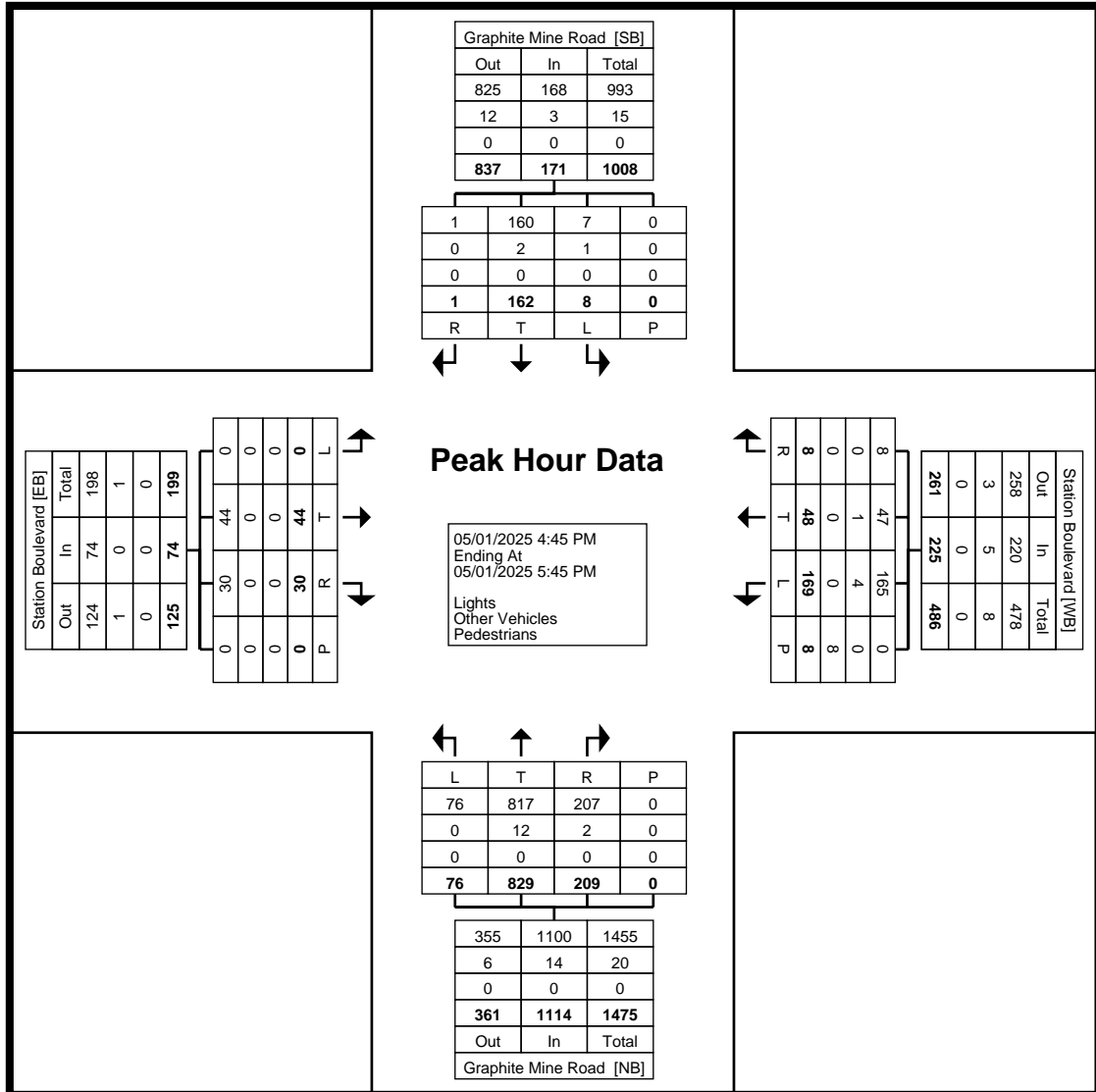
[illegible]



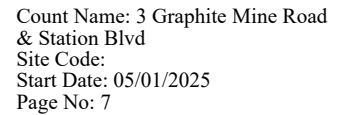
Counter: MIO:
Set up By JH::

Traffic Planning and Design, Inc
2500 East High Street
Suite 650
Pottstown, Pennsylvania, United States 19464
610.326.3100

Count Name: 3 Graphite Mine Road
& Station Blvd
Site Code:
Start Date: 05/01/2025
Page No: 6



Turning Movement Peak Hour Data Plot (4:45 PM)



APPENDIX C:

Volume Development Spreadsheets



TPD# CELE.00001

5/27/2025

Traffic Volumes Worksheet

Intersection:

Synchro Node:

Route 100 & Station Boulevard/Park Road									
1	Adjacent intersections:	West	0	East	2	North	0	South	5

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	142	58	27	18	69	1	126	182	17	59	670	84	1453
Balancing													0
Existing Volumes (Balanced)	142	58	27	18	69	1	126	182	17	59	670	84	1453
Base growth (0.44% per year over 1 year)	1	0	0	0	0	0	1	1	0	0	3	0	6
Automotive Service Center								13			12		25
McKee-Fetters Tract	3	2	4		1		4	5			6		24
Byers Station Parcel 6C		1			1								2
100 Greenridge Road Residential Development								2			14		16
2026 Base (No-Build) Volumes	146	61	31	18	71	1	131	203	17	59	705	84	1526
New Trips		7		11	6	17			13	14			
Pass By Trips				16		6		-6	6	17	-17		
Total Trip Distribution	0	7	0	27	6	23	0	-6	19	31	-17	0	90
2026 Projected (Build) Volumes	146	68	31	45	77	24	131	197	36	90	688	84	1616

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	169	53	12	14	96	4	286	433	14	3	536	132	1752
Balancing													0
Existing Volumes (Balanced)	169	53	12	14	96	4	286	433	14	3	536	132	1752
Base growth (0.44% per year over 1 year)	1	0	0	0	0	0	1	2	0	0	2	1	7
Automotive Service Center								7			23		30
McKee-Fetters Tract	2	1	2		2		7	9			5	1	29
Byers Station Parcel 6C		1			2								3
100 Greenridge Road Residential Development								7			9		16
2026 Base (No-Build) Volumes	172	55	14	14	100	4	294	458	14	3	575	134	1837
New Trips		8		17	9	27			15	16			
Pass By Trips				24		21		-22	22	25	-25		
Total Trip Distribution	0	8	0	41	9	48	0	-22	37	41	-25	0	137
2026 Projected (Build) Volumes	172	63	14	55	109	52	294	436	51	44	550	134	1974

Time Period: Saturday Midday Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	143	50	17	13	52	4	191	258	13	6	600	134	1481
Balancing													0
Existing Volumes (Balanced)	143	50	17	13	52	4	191	258	13	6	600	134	1481
Base growth (1.75% per year over 3 years)	1	0	0	0	0	0	1	1	0	0	3	1	7
Automotive Service Center								7			21		28
McKee-Fetters Tract	3	2	4		2		7	9			6		33
Byers Station Parcel 6C		1			2								3
100 Greenridge Road Residential Development								6			10		16
2026 Base (No-Build) Volumes	147	53	21	13	56	4	199	281	13	6	640	135	1568
New Trips		8		14	8	15			15	16			
Pass By Trips				18		10		-10	10	18	-18		
Total Trip Distribution	0	8	0	32	8	25	0	-10	25	34	-18	0	104
2026 Projected (Build) Volumes	147	61	21	45	64	29	199	271	38	40	622	135	1672

TPD# CELE.00001

5/27/2025

Traffic Volumes Worksheet

Intersection:

Synchro Node:

Graphite Mine Road & Station Boulevard									
2	Adjacent intersections:	West	0	East	2	North	0	South	5

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	1	32	126	298	46	14	42	299	92	1	50	1	1002
Balancing													0
Existing Volumes (Balanced)	1	32	126	298	46	14	42	299	92	1	50	1	1002
Base growth (0.44% per year over 1 year)	0	0	1	1	0	0	0	1	0	0	0	0	3
Automotive Service Center								21			1		22
McKee-Fetters Tract			2				1						3
Byers Station Parcel 6C			1	1			1	3	1		5		12
100 Greenridge Road Residential Development								4			1		5
2026 Base (No-Build) Volumes	1	32	130	300	46	14	44	328	93	1	57	1	1047
New Trips		4	10		5		13					8	
Pass By Trips													
Total Trip Distribution	0	4	10	0	5	0	13	0	0	0	0	8	40
2026 Projected (Build) Volumes	1	36	140	300	51	14	57	328	93	1	57	9	1087

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	0	44	30	169	48	8	76	829	209	8	162	1	1584
Balancing													0
Existing Volumes (Balanced)	0	44	30	169	48	8	76	829	209	8	162	1	1584
Base growth (0.44% per year over 1 year)	0	0	0	1	0	0	0	4	1	0	1	0	7
Automotive Service Center								13			7		20
McKee-Fetters Tract			1				2						3
Byers Station Parcel 6C			1	1			2	6			4		14
100 Greenridge Road Residential Development								13			3		16
2026 Base (No-Build) Volumes	0	44	32	171	48	8	80	865	210	8	177	1	1644
New Trips		6	16		6		15					9	
Pass By Trips													
Total Trip Distribution	0	6	16	0	6	0	15	0	0	0	0	9	52
2026 Projected (Build) Volumes	0	50	48	171	54	8	95	865	210	8	177	10	1696

Time Period: Saturday Midday Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2025 Existing Counts	2	47	29	112	25	9	44	468	125	14	254	5	1134
Balancing													0
Existing Volumes (Balanced)	2	47	29	112	25	9	44	468	125	14	254	5	1134
Base growth (0.44% per year over 1 year)	0	0	0	0	0	0	0	2	1	0	1	0	4
Automotive Service Center								13			9		22
McKee-Fetters Tract			2				2						4
Byers Station Parcel 6C			1	1			2	6	1		5		16
100 Greenridge Road Residential Development								10			4		14
2026 Base (No-Build) Volumes	2	47	32	113	25	9	48	499	127	14	273	5	1194
New Trips	8	5	14		6		15					9	
Pass By Trips													
Total Trip Distribution	8	5	14	0	6	0	15	0	0	0	0	9	57
2026 Projected (Build) Volumes	10	52	46	113	31	9	63	499	127	14	273	14	1251

TPD# CELE.00001

5/27/2025

Traffic Volumes Worksheet

Intersection:

Synchro Node:

Station Boulevard & Begonia Drive									
3	Adjacent intersections:	West	0	East	2	North	0	South	5

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	Volume
2025 Existing Counts	16	105			76	14				80		17	308
Balancing													0
Existing Volumes (Balanced)	16	105	0	0	76	14	0	0	0	80	0	17	308
Base growth (0.44% per year over 1 year)	0	0	0	0	0	0	0	0	0	0	0	0	0
Automotive Service Center													0
McKee-Fetters Tract		2			1								3
Byers Station Parcel 6C		1			1								2
100 Greenridge Road Residential Development													0
2026 Base (No-Build) Volumes	16	108	0	0	78	14	0	0	0	80	0	17	313
New Trips	34					26				14		34	
Pass By Trips	25	-2			-2	2				2		23	
Total Trip Distribution	59	-2	0	0	-2	28	0	0	0	16	0	57	156
2026 Projected (Build) Volumes	75	106	0	0	76	42	0	0	0	96	0	74	469

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	Volume
2025 Existing Counts	10	65			118	20				12		11	236
Balancing													0
Existing Volumes (Balanced)	10	65	0	0	118	20	0	0	0	12	0	11	236
Base growth (0.44% per year over 1 year)	0	0	0	0	1	0	0	0	0	0	0	0	1
Automotive Service Center													0
McKee-Fetters Tract		1			2								3
Byers Station Parcel 6C		1			2								3
100 Greenridge Road Residential Development													0
2026 Base (No-Build) Volumes	10	67	0	0	123	20	0	0	0	12	0	11	243
New Trips	39					30				22		53	
Pass By Trips	49	-2			-4	4				2		49	
Total Trip Distribution	88	-2	0	0	-4	34	0	0	0	24	0	102	242
2026 Projected (Build) Volumes	98	65	0	0	119	54	0	0	0	36	0	113	485

Time Period: Saturday Midday Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	Volume
2025 Existing Counts	8	62			66	8				14		4	162
Balancing													0
Existing Volumes (Balanced)	8	62	0	0	66	8	0	0	0	14	0	4	162
Base growth (0.44% per year over 1 year)	0	0	0	0	0	0	0	0	0	0	0	0	0
Automotive Service Center													0
McKee-Fetters Tract		2			2								4
Byers Station Parcel 6C		1			2								3
100 Greenridge Road Residential Development													0
2026 Base (No-Build) Volumes	8	65	0	0	70	8	0	0	0	14	0	4	169
New Trips	39					30				27		37	
Pass By Trips	30	-2			-1	1				1		30	
Total Trip Distribution	69	-2	0	0	-1	31	0	0	0	28	0	67	192
2026 Projected (Build) Volumes	77	63	0	0	69	39	0	0	0	42	0	71	361

NCHRP 684 Internal Trip Capture Estimation Tool					
Project Name:	CELE.01			Organization:	TPD
Project Location:	Upper Uwchlan Township			Performed By:	PG
Scenario Description:				Date:	
Analysis Year:				Checked By:	
Analysis Period:	AM Street Peak Hour			Date:	

Table 1-A: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips ³		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office						
Retail	822	18	KSF	989	495	494
Restaurant	930	3	KSF	291	146	145
Cinema/Entertainment				0		
Residential				0		
Hotel				0		
All Other Land Uses ²	565	11	KSF	85	42	43
				1,365	683	682

Table 2-A: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ. ⁴	% Transit	% Non-Motorized	Veh. Occ. ⁴	% Transit	% Non-Motorized
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						
All Other Land Uses ²						

Table 3-A: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-A: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		64	0	0	0
Restaurant	0	20		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	0	0	0		0
Hotel	0	0	0	0	0	

Table 5-A: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	1,365	683	682
Internal Capture Percentage	12%	12%	12%
External Vehicle-Trips ⁵	1,197	599	598
External Transit-Trips ⁶	0	0	0
External Non-Motorized Trips ⁶	0	0	0

Table 6-A: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	N/A	N/A
Retail	4%	13%
Restaurant	44%	14%
Cinema/Entertainment	N/A	N/A
Residential	N/A	N/A
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Manual*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site is not subject to internal trip capture computations in this estimator.

³Enter trips assuming no transit or non-motorized trips (as assumed in ITE *Trip Generation Manual*).

⁴Enter vehicle occupancy assumed in Table 1-A vehicle trips. If vehicle occupancy changes for proposed mixed-use project, manual adjustments must be made to Tables 5-A, 9-A (O and D). Enter transit, non-motorized percentages that will result with proposed mixed-use project complete.

⁵Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A.

⁶Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas A&M Transportation Institute - Version 2013.1

NCHRP 684 Internal Trip Capture Estimation Tool					
Project Name:	CELE.01			Organization:	TPD
Project Location:	Upper Uwchlan Township			Performed By:	PG
Scenario Description:	Projected Conditions			Date:	
Analysis Year:	2026			Checked By:	
Analysis Period:	PM Street Peak Hour			Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips ³		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office						
Retail	822	18	KSF	989	495	494
Restaurant	930	3	KSF	291	146	145
Cinema/Entertainment				0		
Residential				0		
Hotel				0		
All Other Land Uses ²	565	11	KSF	85	42	43
				1,365	683	682

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ. ⁴	% Transit	% Non-Motorized	Veh. Occ. ⁴	% Transit	% Non-Motorized
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						
All Other Land Uses ²						

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		42	0	0	0
Restaurant	0	59		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	0	0	0		0
Hotel	0	0	0	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	1,365	683	682
Internal Capture Percentage	15%	15%	15%
External Vehicle-Trips ⁵	1,163	582	581
External Transit-Trips ⁶	0	0	0
External Non-Motorized Trips ⁶	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	N/A	N/A
Retail	12%	9%
Restaurant	29%	41%
Cinema/Entertainment	N/A	N/A
Residential	N/A	N/A
Hotel	N/A	N/A

¹ Land Use Codes (LUCs) from <i>Trip Generation Manual</i> , published by the Institute of Transportation Engineers.
² Total estimate for all other land uses at mixed-use development site is not subject to internal trip capture computations in this estimator.
³ Enter trips assuming no transit or non-motorized trips (as assumed in ITE <i>Trip Generation Manual</i>).
⁴ Enter vehicle occupancy assumed in Table 1-P vehicle trips. If vehicle occupancy changes for proposed mixed-use project, manual adjustments must be
⁵ Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P.
⁶ Person-Trips
*Indicates computation that has been rounded to the nearest whole number.
Estimation Tool Developed by the Texas A&M Transportation Institute - Version 2013.1

APPENDIX D:

Capacity Analysis Worksheets























Existing Conditions



1: Pottstown Pike & Park Road/Station Boulevard

Existing Conditions

Timing Plan: Weekday AM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	142	58	27	18	69	1	126	182	17	59	670	84
Future Volume (vph)	142	58	27	18	69	1	126	182	17	59	670	84
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			405			444			409	
Travel Time (s)		12.7			9.2			8.6			8.0	
Confl. Peds. (#/hr)									1	1		
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	7%	3%	4%	0%	3%	0%	12%	10%	18%	10%	4%	7%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		10.0	30.0		30.0	30.0	
Total Split (s)	36.0	36.0		36.0	36.0		19.0	84.0		65.0	65.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		15.8%	70.0%		54.2%	54.2%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 120

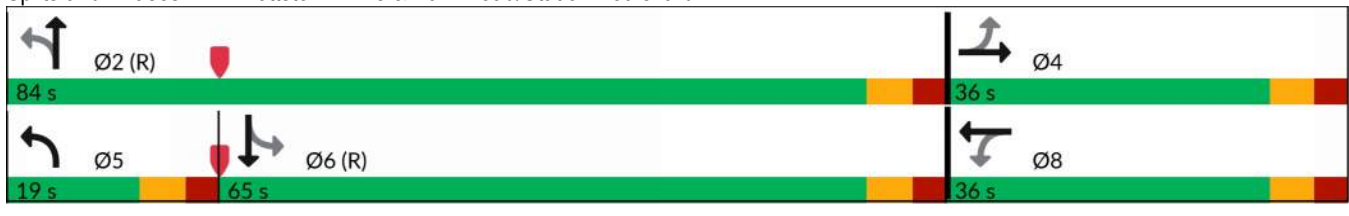
Actuated Cycle Length: 120

Offset: 74 (62%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 90

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XAM.syn




















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

Existing Conditions

Timing Plan: Weekday AM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	142	58	27	18	69	1	126	182	17	59	670	84
Future Volume (veh/h)	142	58	27	18	69	1	126	182	17	59	670	84
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1850	1983	1968	1750	1776	1820	1778	1807	1693	1609	1694	1652
Adj Flow Rate, veh/h	151	62	29	19	73	1	134	194	18	63	713	89
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	7	3	4	0	3	0	12	10	18	10	4	7
Cap, veh/h	251	231	108	235	316	4	320	1171	109	676	904	113
Arrive On Green	0.18	0.18	0.17	0.06	0.06	0.06	0.06	0.72	0.71	0.61	0.61	0.60
Sat Flow, veh/h	1311	1278	598	1221	1748	24	1694	1628	151	1006	1476	184
Grp Volume(v), veh/h	151	0	91	19	0	74	134	0	212	63	0	802
Grp Sat Flow(s),veh/h/ln	1311	0	1875	1221	0	1772	1694	0	1780	1006	0	1660
Q Serve(g_s), s	13.3	0.0	5.0	1.8	0.0	4.8	3.2	0.0	4.6	3.1	0.0	43.5
Cycle Q Clear(g_c), s	17.6	0.0	5.0	6.3	0.0	4.8	3.2	0.0	4.6	3.1	0.0	43.5
Prop In Lane	1.00		0.32	1.00		0.01	1.00		0.08	1.00		0.11
Lane Grp Cap(c), veh/h	251	0	339	235	0	321	320	0	1279	676	0	1017
V/C Ratio(X)	0.60	0.00	0.27	0.08	0.00	0.23	0.42	0.00	0.17	0.09	0.00	0.79
Avail Cap(c_a), veh/h	341	0	469	319	0	443	407	0	1279	676	0	1017
HCM Platoon Ratio	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	49.5	0.0	42.4	51.3	0.0	48.5	17.2	0.0	5.4	9.6	0.0	17.5
Incr Delay (d2), s/veh	2.3	0.0	0.4	0.1	0.0	0.4	0.9	0.0	0.3	0.3	0.0	6.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.0	0.0	4.2	1.0	0.0	4.0	3.0	0.0	2.9	1.3	0.0	23.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	51.8	0.0	42.9	51.5	0.0	48.8	18.0	0.0	5.7	9.9	0.0	23.7
LnGrp LOS	D		D	D		D	B		A	A		C
Approach Vol, veh/h	242			93			346			865		
Approach Delay, s/veh	48.5			49.4			10.5			22.7		
Approach LOS	D			D			B			C		
Timer - Assigned Phs	2			4	5	6	8					
Phs Duration (G+Y+Rc), s	92.3			27.7	12.8	79.5	27.7					
Change Period (Y+Rc), s	7.0			7.0	7.0	7.0	7.0					
Max Green Setting (Gmax), s	77.0			29.0	12.0	58.0	29.0					
Max Q Clear Time (g_c+11), s	6.6			20.1	5.7	45.5	8.8					
Green Ext Time (p_c), s	1.3			0.6	0.2	4.8	0.4					
Intersection Summary												
HCM 6th Ctrl Delay, s/veh	25.6											
HCM 6th LOS	C											

2: Graphite Mine Road & Station Boulevard

Existing Conditions
Timing Plan: Weekday AM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	32	126	298	46	14	42	299	92	1	50	1
Future Volume (vph)	1	32	126	298	46	14	42	299	92	1	50	1
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		601			192			486			211	
Travel Time (s)		13.7			5.2			8.3			3.6	
Confl. Peds. (#/hr)									2	2		
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	0%	13%	10%	2%	4%	0%	7%	8%	4%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	55.0	55.0		55.0	55.0		65.0	65.0	65.0	65.0	65.0	
Total Split (%)	45.8%	45.8%		45.8%	45.8%		54.2%	54.2%	54.2%	54.2%	54.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 9 (8%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings























C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XAM.syn

Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

Existing Conditions
Timing Plan: Weekday AM Peak











												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	1	32	126	298	46	14	42	299	92	1	50	1
Future Volume (veh/h)	1	32	126	298	46	14	42	299	92	1	50	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1682	1726	1766	1808	1866	1775	1761	1890	1778	1750	1778
Adj Flow Rate, veh/h	1	36	140	331	51	16	47	332	102	1	56	1
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	13	10	2	4	0	7	8	4	0	2	0
Cap, veh/h	539	118	460	419	519	163	712	922	837	434	897	16
Arrive On Green	0.39	0.39	0.38	0.39	0.39	0.38	0.52	0.52	0.52	0.52	0.52	0.52
Sat Flow, veh/h	1284	301	1171	1141	1319	414	1276	1761	1599	907	1713	31
Grp Volume(v), veh/h	1	0	176	331	0	67	47	332	102	1	0	57
Grp Sat Flow(s),veh/h/ln	1284	0	1471	1141	0	1733	1276	1761	1599	907	0	1744
Q Serve(g_s), s	0.1	0.0	10.0	33.6	0.0	2.9	2.2	13.3	3.9	0.1	0.0	1.9
Cycle Q Clear(g_c), s	2.5	0.0	10.0	43.1	0.0	2.9	3.7	13.3	3.9	13.4	0.0	1.9
Prop In Lane	1.00		0.80	1.00		0.24	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	539	0	579	419	0	682	712	922	837	434	0	913
V/C Ratio(X)	0.00	0.00	0.30	0.79	0.00	0.10	0.07	0.36	0.12	0.00	0.00	0.06
Avail Cap(c_a), veh/h	569	0	613	445	0	722	712	922	837	434	0	913
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	23.6	0.0	25.4	39.7	0.0	23.1	14.9	16.8	14.6	20.7	0.0	14.1
Incr Delay (d2), s/veh	0.0	0.0	0.3	8.9	0.0	0.1	0.2	1.1	0.3	0.0	0.0	0.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	6.4	15.7	0.0	2.2	1.2	9.2	2.6	0.0	0.0	1.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	23.6	0.0	25.7	48.6	0.0	23.1	15.0	17.9	14.9	20.7	0.0	14.2
LnGrp LOS	C		C	D		C	B	B	B	C		B
Approach Vol, veh/h		177			398			481			58	
Approach Delay, s/veh		25.7			44.3			17.0			14.3	
Approach LOS		C			D			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		67.8		52.2		67.8		52.2				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		59.0		49.0		59.0		49.0				
Max Q Clear Time (g_c+I1), s		15.8		12.0		15.9		45.6				
Green Ext Time (p_c), s		2.5		1.2		0.3		0.6				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				28.0								
HCM 6th LOS				C								

Notes

User approved pedestrian interval to be less than phase max green.






3: Station Boulevard & Driveway

Existing Conditions
Timing Plan: Weekday AM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	16	105	76	14	80	17
Future Volume (vph)	16	105	76	14	80	17
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		405	601		221	
Travel Time (s)		9.2	16.4		5.0	
Confl. Peds. (#/hr)						5
Peak Hour Factor	0.65	0.65	0.65	0.65	0.65	0.65
Heavy Vehicles (%)	44%	5%	4%	7%	8%	12%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

Existing Conditions
Timing Plan: Weekday AM Peak

Intersection						
Int Delay, s/veh	4.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	16	105	76	14	80	17
Future Vol, veh/h	16	105	76	14	80	17
Conflicting Peds, #/hr	0	0	0	0	0	5
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	44	5	4	7	8	12
Mvmt Flow	25	162	117	22	123	26
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	139	0	-	0	340	133
Stage 1	-	-	-	-	128	-
Stage 2	-	-	-	-	212	-
Critical Hdwy	4.54	-	-	-	6.48	6.32
Critical Hdwy Stg 1	-	-	-	-	5.48	-
Critical Hdwy Stg 2	-	-	-	-	5.48	-
Follow-up Hdwy	2.596	-	-	-	3.572	3.408
Pot Cap-1 Maneuver	1223	-	-	-	644	890
Stage 1	-	-	-	-	883	-
Stage 2	-	-	-	-	809	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1223	-	-	-	631	886
Mov Cap-2 Maneuver	-	-	-	-	631	-
Stage 1	-	-	-	-	865	-
Stage 2	-	-	-	-	809	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	1.1	0		12		
HCM LOS				B		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1223	-	-	-	665	
HCM Lane V/C Ratio	0.02	-	-	-	0.224	
HCM Control Delay (s/veh)	8	-	-	-	12	
HCM Lane LOS	A	-	-	-	B	
HCM 95th %tile Q (veh)	0.1	-	-	-	0.9	

1: Pottstown Pike & Park Road/Station Boulevard

Existing Conditions

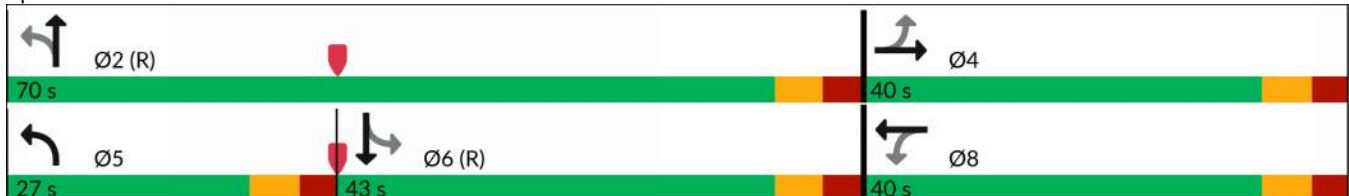
Timing Plan: Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	169	53	12	14	96	4	286	433	14	3	536	132
Future Volume (vph)	169	53	12	14	96	4	286	433	14	3	536	132
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			344			444			499	
Travel Time (s)		12.7			7.8			8.6			9.7	
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	1%	4%	0%	0%	1%	0%	2%	2%	0%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		11.0	30.0		30.0	30.0	
Total Split (s)	40.0	40.0		40.0	40.0		27.0	70.0		43.0	43.0	
Total Split (%)	36.4%	36.4%		36.4%	36.4%		24.5%	63.6%		39.1%	39.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 12 (11%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 90
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XPM.syn





















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard


Existing Conditions

Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	169	53	12	14	96	4	286	433	14	3	536	132
Future Volume (veh/h)	169	53	12	14	96	4	286	433	14	3	536	132
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1935	1968	2027	1750	1805	1820	1921	1921	1949	1750	1722	1750
Adj Flow Rate, veh/h	172	54	12	14	98	4	292	442	14	3	547	135
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	1	4	0	0	1	0	2	2	0	0	2	0
Cap, veh/h	282	306	68	280	338	14	424	1286	41	537	718	177
Arrive On Green	0.20	0.20	0.19	0.39	0.39	0.37	0.10	0.69	0.69	0.54	0.54	0.53
Sat Flow, veh/h	1337	1559	346	1249	1722	70	1829	1851	59	875	1333	329
Grp Volume(v), veh/h	172	0	66	14	0	102	292	0	456	3	0	682
Grp Sat Flow(s),veh/h/ln	1337	0	1905	1249	0	1793	1829	0	1910	875	0	1662
Q Serve(g_s), s	13.6	0.0	3.2	0.8	0.0	4.3	7.0	0.0	10.5	0.2	0.0	35.4
Cycle Q Clear(g_c), s	17.4	0.0	3.2	3.5	0.0	4.3	7.0	0.0	10.5	0.2	0.0	35.4
Prop In Lane	1.00		0.18	1.00		0.04	1.00		0.03	1.00		0.20
Lane Grp Cap(c), veh/h	282	0	374	280	0	352	424	0	1327	537	0	896
V/C Ratio(X)	0.61	0.00	0.18	0.05	0.00	0.29	0.69	0.00	0.34	0.01	0.00	0.76
Avail Cap(c_a), veh/h	433	0	589	421	0	554	587	0	1327	537	0	896
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	44.4	0.0	36.9	28.8	0.0	28.2	17.6	0.0	6.7	11.7	0.0	19.9
Incr Delay (d2), s/veh	2.1	0.0	0.2	0.1	0.0	0.5	2.0	0.0	0.7	0.0	0.0	6.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.1	0.0	2.7	0.4	0.0	3.2	6.5	0.0	7.2	0.1	0.0	20.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	46.5	0.0	37.1	28.9	0.0	28.7	19.6	0.0	7.4	11.8	0.0	26.0
LnGrp LOS	D		D	C		C	B		A	B		C
Approach Vol, veh/h		238			116			748			685	
Approach Delay, s/veh		43.9			28.7			12.2			26.0	
Approach LOS		D			C			B			C	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		82.4		27.6	17.2	65.3		27.6				
Change Period (Y+Rc), s		7.0		7.0	7.0	7.0		7.0				
Max Green Setting (Gmax), s		63.0		33.0	20.0	36.0		33.0				
Max Q Clear Time (g_c+l1), s		12.5		19.9	9.5	37.4		6.3				
Green Ext Time (p_c), s		3.1		0.7	0.6	0.0		0.6				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			22.8									
HCM 6th LOS			C									

2: Graphite Mine Road & Station Boulevard

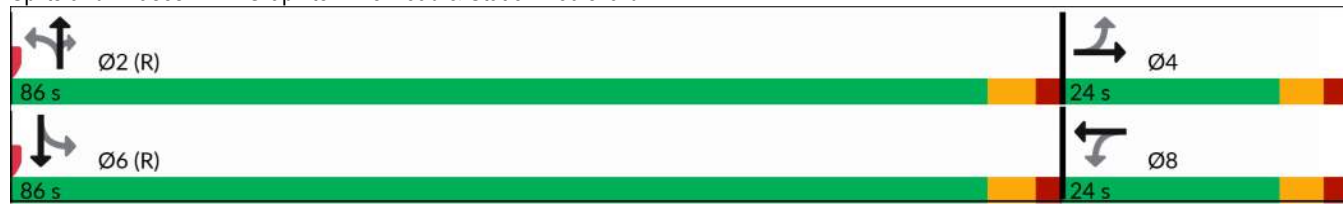
Existing Conditions
Timing Plan: Weekday PM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	44	30	169	48	8	76	829	209	8	162	1
Future Volume (vph)	0	44	30	169	48	8	76	829	209	8	162	1
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		661			192			486			211	
Travel Time (s)		15.0			5.2			8.3			3.6	
Confl. Peds. (#/hr)									8	8		
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	2%	0%	0%	1%	1%	13%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	24.0	24.0		24.0	24.0		86.0	86.0	86.0	86.0	86.0	
Total Split (%)	21.8%	21.8%		21.8%	21.8%		78.2%	78.2%	78.2%	78.2%	78.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 53 (48%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings























C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XPM.syn

Synchro 12 Report

05/27/2025











2: Graphite Mine Road & Station Boulevard

Existing Conditions
Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	44	30	169	48	8	76	829	209	8	162	1
Future Volume (veh/h)	0	44	30	169	48	8	76	829	209	8	162	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.99	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1872	1766	1837	1866	1875	1860	1935	1595	1764	1778
Adj Flow Rate, veh/h	0	46	31	176	50	8	79	864	218	8	169	1
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	0	0	2	2	0	0	1	1	13	1	0
Cap, veh/h	65	180	121	239	267	43	930	1370	1201	291	1290	8
Arrive On Green	0.00	0.17	0.16	0.17	0.17	0.16	0.74	0.74	0.74	0.74	0.74	0.73
Sat Flow, veh/h	1295	1043	703	1249	1545	247	1213	1860	1631	445	1751	10
Grp Volume(v), veh/h	0	0	77	176	0	58	79	864	218	8	0	170
Grp Sat Flow(s),veh/h/ln	1295	0	1746	1249	0	1792	1213	1860	1631	445	0	1762
Q Serve(g_s), s	0.0	0.0	4.2	15.3	0.0	3.0	2.2	25.1	4.5	1.0	0.0	3.1
Cycle Q Clear(g_c), s	0.0	0.0	4.2	19.0	0.0	3.0	4.8	25.1	4.5	26.1	0.0	3.1
Prop In Lane	1.00		0.40	1.00		0.14	1.00		1.00	1.00		0.01
Lane Grp Cap(c), veh/h	65	0	301	239	0	310	930	1370	1201	291	0	1297
V/C Ratio(X)	0.00	0.00	0.26	0.74	0.00	0.19	0.08	0.63	0.18	0.03	0.00	0.13
Avail Cap(c_a), veh/h	65	0	301	239	0	310	930	1370	1201	291	0	1297
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	39.6	47.5	0.0	39.0	4.9	7.1	4.4	13.6	0.0	4.2
Incr Delay (d2), s/veh	0.0	0.0	0.4	11.3	0.0	0.3	0.2	2.2	0.3	0.2	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	3.3	9.5	0.0	2.5	0.9	13.2	2.3	0.2	0.0	1.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	0.0	40.0	58.7	0.0	39.2	5.0	9.4	4.7	13.7	0.0	4.4
LnGrp LOS			D	E		D	A	A	A	B		A
Approach Vol, veh/h		77			234			1161			178	
Approach Delay, s/veh		40.0			53.9			8.2			4.9	
Approach LOS		D			D			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		86.0		24.0		86.0		24.0				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		80.0		18.0		80.0		18.0				
Max Q Clear Time (g_c+I1), s		27.6		6.2		28.6		21.5				
Green Ext Time (p_c), s		9.1		0.2		1.1		0.0				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				15.8								
HCM 6th LOS				B								






3: Station Boulevard & Driveway

Existing Conditions
Timing Plan: Weekday PM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	10	65	118	20	12	11
Future Volume (vph)	10	65	118	20	12	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		344	661		224	
Travel Time (s)		7.8	18.0		5.1	
Confl. Peds. (#/hr)						4
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

Existing Conditions
Timing Plan: Weekday PM Peak

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	10	65	118	20	12	11
Future Vol, veh/h	10	65	118	20	12	11
Conflicting Peds, #/hr	0	0	0	0	0	4
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	11	73	133	22	13	12
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	155	0	-	0	239	148
Stage 1	-	-	-	-	144	-
Stage 2	-	-	-	-	95	-
Critical Hdwy	4.1	-	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	-	3.5	3.3
Pot Cap-1 Maneuver	1438	-	-	-	754	904
Stage 1	-	-	-	-	888	-
Stage 2	-	-	-	-	934	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1438	-	-	-	748	901
Mov Cap-2 Maneuver	-	-	-	-	748	-
Stage 1	-	-	-	-	881	-
Stage 2	-	-	-	-	934	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	1	0		9.6		
HCM LOS	A					
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1438	-	-	-	814	
HCM Lane V/C Ratio	0.008	-	-	-	0.032	
HCM Control Delay (s/veh)	7.5	-	-	-	9.6	
HCM Lane LOS	A	-	-	-	A	
HCM 95th %tile Q (veh)	0	-	-	-	0.1	

1: Pottstown Pike & Park Road/Station Boulevard

Existing Conditions

Timing Plan: Saturday Midday

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	143	50	17	13	52	4	191	258	13	6	600	134
Future Volume (vph)	143	50	17	13	52	4	191	258	13	6	600	134
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			0%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			25			35			35	
Link Distance (ft)		650			383			444			509	
Travel Time (s)		12.7			10.4			8.6			9.9	
Confl. Peds. (#/hr)			1	1			3					3
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Heavy Vehicles (%)	2%	0%	0%	0%	0%	0%	0%	1%	8%	0%	1%	2%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	10.0	10.0		10.0	10.0		11.0	23.0		23.0	23.0	
Total Split (s)	22.0	22.0		22.0	22.0		13.0	68.0		55.0	55.0	
Total Split (%)	24.4%	24.4%		24.4%	24.4%		14.4%	75.6%		61.1%	61.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 90

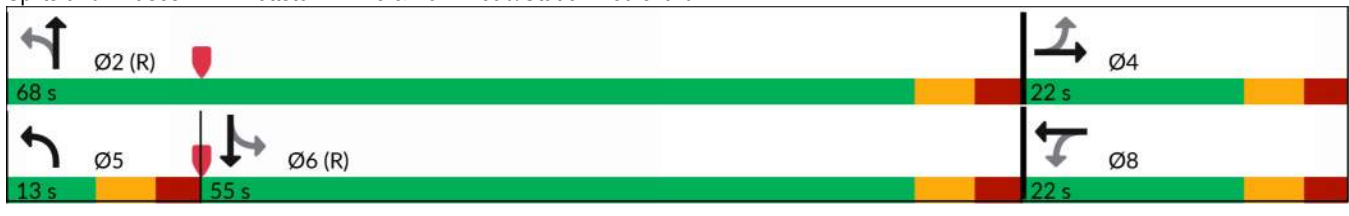
Actuated Cycle Length: 90

Offset: 26 (29%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 70

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XSAT.syn




















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

Existing Conditions


Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	143	50	17	13	52	4	191	258	13	6	600	134
Future Volume (veh/h)	143	50	17	13	52	4	191	258	13	6	600	134
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1921	2027	2027	1750	1820	1820	1800	1786	1688	1750	1736	1722
Adj Flow Rate, veh/h	147	52	18	13	54	4	197	266	13	6	619	138
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	2	0	0	0	0	0	0	1	8	0	1	2
Cap, veh/h	276	237	82	252	275	20	349	1185	58	653	766	171
Arrive On Green	0.16	0.16	0.15	0.16	0.16	0.15	0.08	0.70	0.69	0.56	0.56	0.55
Sat Flow, veh/h	1378	1438	498	1242	1673	124	1714	1688	83	1027	1373	306
Grp Volume(v), veh/h	147	0	70	13	0	58	197	0	279	6	0	757
Grp Sat Flow(s),veh/h/ln	1378	0	1935	1242	0	1797	1714	0	1771	1027	0	1680
Q Serve(g_s), s	9.2	0.0	2.8	0.8	0.0	2.5	3.9	0.0	5.0	0.2	0.0	32.7
Cycle Q Clear(g_c), s	11.2	0.0	2.8	3.1	0.0	2.5	3.9	0.0	5.0	0.2	0.0	32.7
Prop In Lane	1.00		0.26	1.00		0.07	1.00		0.05	1.00		0.18
Lane Grp Cap(c), veh/h	276	0	319	252	0	296	349	0	1243	653	0	937
V/C Ratio(X)	0.53	0.00	0.22	0.05	0.00	0.20	0.56	0.00	0.22	0.01	0.00	0.81
Avail Cap(c_a), veh/h	294	0	344	269	0	319	349	0	1243	653	0	937
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	37.1	0.0	32.7	33.7	0.0	32.5	15.2	0.0	4.8	8.9	0.0	16.1
Incr Delay (d2), s/veh	1.6	0.0	0.3	0.1	0.0	0.3	2.1	0.0	0.4	0.0	0.0	7.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	5.6	0.0	2.4	0.5	0.0	2.0	3.6	0.0	2.9	0.1	0.0	18.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	38.7	0.0	33.0	33.8	0.0	32.8	17.3	0.0	5.2	8.9	0.0	23.6
LnGrp LOS	D		C	C		C	B		A	A		C
Approach Vol, veh/h	217			71			476			763		
Approach Delay, s/veh	36.8			33.0			10.2			23.5		
Approach LOS	D			C			B			C		
Timer - Assigned Phs	2			4		5	6		8			
Phs Duration (G+Y+Rc), s	69.2			20.8		13.0	56.2		20.8			
Change Period (Y+Rc), s	7.0			7.0		7.0	7.0		7.0			
Max Green Setting (Gmax), s	61.0			15.0		6.0	48.0		15.0			
Max Q Clear Time (g_c+I1), s	7.0			13.7		6.4	34.7		5.6			
Green Ext Time (p_c), s	1.7			0.1		0.0	4.5		0.2			
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				21.7								
HCM 6th LOS				C								

2: Graphite Mine Road & Station Boulevard

Existing Conditions

Timing Plan: Saturday Midday

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	2	47	29	112	25	9	44	468	125	14	254	5
Future Volume (vph)	2	47	29	112	25	9	44	468	125	14	254	5
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		623			192			486			211	
Travel Time (s)		14.2			5.2			8.3			3.6	
Confl. Peds. (#/hr)									4	4		
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Heavy Vehicles (%)	0%	0%	3%	0%	0%	0%	0%	1%	2%	7%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	22.0	22.0		22.0	22.0		22.0	22.0	22.0	22.0	22.0	
Total Split (s)	22.0	22.0		22.0	22.0		41.0	41.0	41.0	41.0	41.0	
Total Split (%)	34.9%	34.9%		34.9%	34.9%		65.1%	65.1%	65.1%	65.1%	65.1%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 63

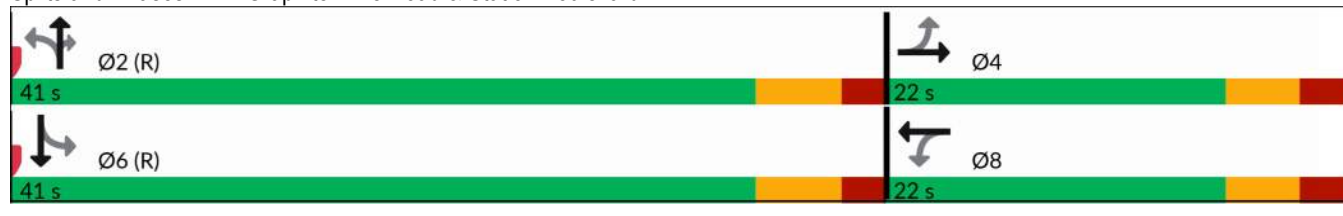
Actuated Cycle Length: 63

Offset: 14 (22%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 50

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\XSAT.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard











Existing Conditions

Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	47	29	112	25	9	44	468	125	14	254	5
Future Volume (veh/h)	2	47	29	112	25	9	44	468	125	14	254	5
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1828	1794	1866	1866	1875	1860	1920	1679	1764	1778
Adj Flow Rate, veh/h	2	55	34	132	29	11	52	551	147	16	299	6
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Percent Heavy Veh, %	0	0	3	0	0	0	0	1	2	7	1	0
Cap, veh/h	353	208	129	311	248	94	741	1207	1052	450	1118	22
Arrive On Green	0.19	0.19	0.18	0.19	0.19	0.18	0.65	0.65	0.65	0.65	0.65	0.63
Sat Flow, veh/h	1316	1082	669	1255	1289	489	1075	1860	1622	671	1723	35
Grp Volume(v), veh/h	2	0	89	132	0	40	52	551	147	16	0	305
Grp Sat Flow(s),veh/h/ln	1316	0	1752	1255	0	1778	1075	1860	1622	671	0	1757
Q Serve(g_s), s	0.1	0.0	2.7	6.2	0.0	1.2	1.3	9.3	2.2	0.8	0.0	4.6
Cycle Q Clear(g_c), s	0.8	0.0	2.7	8.5	0.0	1.2	5.5	9.3	2.2	10.1	0.0	4.6
Prop In Lane	1.00		0.38	1.00		0.28	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	353	0	337	311	0	342	741	1207	1052	450	0	1140
V/C Ratio(X)	0.01	0.00	0.26	0.42	0.00	0.12	0.07	0.46	0.14	0.04	0.00	0.27
Avail Cap(c_a), veh/h	455	0	473	408	0	480	741	1207	1052	450	0	1140
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	21.1	0.0	21.8	25.0	0.0	21.1	5.8	5.5	4.3	8.0	0.0	4.7
Incr Delay (d2), s/veh	0.0	0.0	0.4	0.9	0.0	0.2	0.2	1.2	0.3	0.1	0.0	0.6
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	2.0	3.4	0.0	0.9	0.5	4.7	1.0	0.2	0.0	2.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	21.1	0.0	22.2	25.9	0.0	21.3	5.9	6.8	4.5	8.2	0.0	5.3
LnGrp LOS	C		C	C		C	A	A	A	A		A
Approach Vol, veh/h		91			172			750			321	
Approach Delay, s/veh		22.2			24.9			6.3			5.4	
Approach LOS		C			C			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		45.9		17.1		45.9		17.1				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		35.0		16.0		35.0		16.0				
Max Q Clear Time (g_c+I1), s		11.8		4.7		12.6		11.0				
Green Ext Time (p_c), s		4.2		0.3		1.8		0.3				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			9.6									
HCM 6th LOS			A									

3: Station Boulevard & Driveway






Existing Conditions
Timing Plan: Saturday MIDDAY

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	8	62	66	8	14	4
Future Volume (vph)	8	62	66	8	14	4
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		383	623		277	
Travel Time (s)		8.7	17.0		6.3	
Confl. Peds. (#/hr)						6
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

Existing Conditions

Timing Plan: Saturday MIDDAY

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	8	62	66	8	14	4
Future Vol, veh/h	8	62	66	8	14	4
Conflicting Peds, #/hr	0	0	0	0	0	6
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	2	0	0	0	0
Mvmt Flow	8	65	69	8	15	4
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	77	0	-	0	154	79
Stage 1	-	-	-	-	73	-
Stage 2	-	-	-	-	81	-
Critical Hdwy	4.1	-	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	-	3.5	3.3
Pot Cap-1 Maneuver	1535	-	-	-	842	987
Stage 1	-	-	-	-	955	-
Stage 2	-	-	-	-	947	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1535	-	-	-	838	982
Mov Cap-2 Maneuver	-	-	-	-	838	-
Stage 1	-	-	-	-	950	-
Stage 2	-	-	-	-	947	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	0.8	0		9.2		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1535	-	-	-	866	
HCM Lane V/C Ratio	0.005	-	-	-	0.022	
HCM Control Delay (s/veh)	7.4	-	-	-	9.2	
HCM Lane LOS	A	-	-	-	A	
HCM 95th %tile Q (veh)	0	-	-	-	0.1	


Base Conditions



1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions

Timing Plan: Weekday AM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	146	61	31	18	71	1	131	203	17	59	705	84
Future Volume (vph)	146	61	31	18	71	1	131	203	17	59	705	84
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			405			444			409	
Travel Time (s)		12.7			9.2			8.6			8.0	
Confl. Peds. (#/hr)									1	1		
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	7%	3%	4%	0%	3%	0%	12%	10%	18%	10%	4%	7%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		10.0	30.0		30.0	30.0	
Total Split (s)	36.0	36.0		36.0	36.0		19.0	84.0		65.0	65.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		15.8%	70.0%		54.2%	54.2%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 120

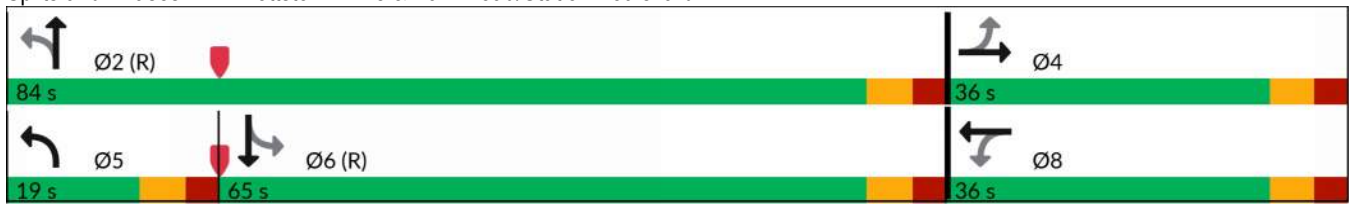
Actuated Cycle Length: 120

Offset: 74 (62%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 90

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BAM.syn





















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions


Timing Plan: Weekday AM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	146	61	31	18	71	1	131	203	17	59	705	84
Future Volume (veh/h)	146	61	31	18	71	1	131	203	17	59	705	84
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1850	1983	1968	1750	1776	1820	1778	1807	1693	1609	1694	1652
Adj Flow Rate, veh/h	155	65	33	19	76	1	139	216	18	63	750	89
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	7	3	4	0	3	0	12	10	18	10	4	7
Cap, veh/h	254	231	117	236	325	4	288	1175	98	657	900	107
Arrive On Green	0.19	0.19	0.18	0.06	0.06	0.06	0.06	0.71	0.71	0.61	0.61	0.60
Sat Flow, veh/h	1307	1240	629	1214	1749	23	1694	1645	137	986	1485	176
Grp Volume(v), veh/h	155	0	98	19	0	77	139	0	234	63	0	839
Grp Sat Flow(s), veh/h/ln	1307	0	1869	1214	0	1772	1694	0	1782	986	0	1662
Q Serve(g_s), s	13.7	0.0	5.4	1.8	0.0	5.0	3.4	0.0	5.2	3.2	0.0	48.3
Cycle Q Clear(g_c), s	18.2	0.0	5.4	6.7	0.0	5.0	3.4	0.0	5.2	3.2	0.0	48.3
Prop In Lane	1.00		0.34	1.00		0.01	1.00		0.08	1.00		0.11
Lane Grp Cap(c), veh/h	254	0	348	236	0	329	288	0	1272	657	0	1007
V/C Ratio(X)	0.61	0.00	0.28	0.08	0.00	0.23	0.48	0.00	0.18	0.10	0.00	0.83
Avail Cap(c_a), veh/h	338	0	467	314	0	443	373	0	1272	657	0	1007
HCM Platoon Ratio	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	49.3	0.0	42.1	51.4	0.0	48.2	20.0	0.0	5.7	10.0	0.0	18.9
Incr Delay (d2), s/veh	2.3	0.0	0.4	0.1	0.0	0.4	1.3	0.0	0.3	0.3	0.0	8.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.1	0.0	4.6	1.0	0.0	4.1	3.7	0.0	3.3	1.3	0.0	26.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	51.7	0.0	42.5	51.5	0.0	48.5	21.3	0.0	6.0	10.2	0.0	27.0
LnGrp LOS	D		D	D		D	C		A	B		C
Approach Vol, veh/h		253			96			373			902	
Approach Delay, s/veh		48.1			49.1			11.7			25.8	
Approach LOS		D			D			B			C	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		91.7		28.3	13.0	78.7		28.3				
Change Period (Y+Rc), s		7.0		7.0	7.0	7.0		7.0				
Max Green Setting (Gmax), s		77.0		29.0	12.0	58.0		29.0				
Max Q Clear Time (g_c+I1), s		7.2		20.7	5.9	50.3		9.2				
Green Ext Time (p_c), s		1.5		0.6	0.2	3.7		0.4				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				27.4								
HCM 6th LOS				C								

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions

Timing Plan: Weekday AM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	32	130	300	46	14	44	328	93	1	57	1
Future Volume (vph)	1	32	130	300	46	14	44	328	93	1	57	1
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		601			192			486			211	
Travel Time (s)		13.7			5.2			8.3			3.6	
Confl. Peds. (#/hr)									2	2		
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	0%	13%	10%	2%	4%	0%	7%	8%	4%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	55.0	55.0		55.0	55.0		65.0	65.0	65.0	65.0	65.0	
Total Split (%)	45.8%	45.8%		45.8%	45.8%		54.2%	54.2%	54.2%	54.2%	54.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 120

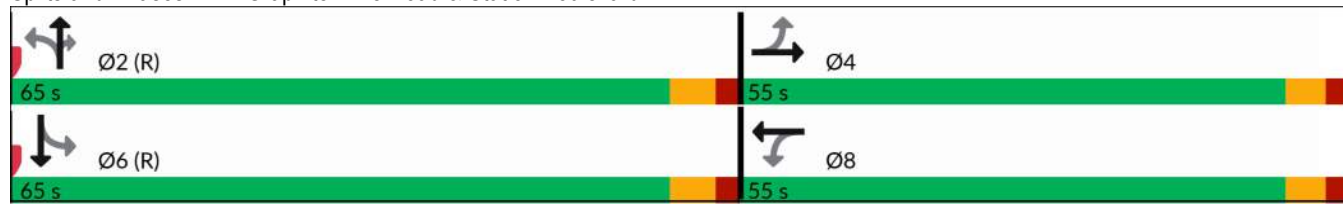
Actuated Cycle Length: 120

Offset: 9 (8%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BAM.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions











Timing Plan: Weekday AM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	1	32	130	300	46	14	44	328	93	1	57	1
Future Volume (veh/h)	1	32	130	300	46	14	44	328	93	1	57	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1682	1726	1766	1808	1866	1775	1761	1890	1778	1750	1778
Adj Flow Rate, veh/h	1	36	144	333	51	16	49	364	103	1	63	1
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	13	10	2	4	0	7	8	4	0	2	0
Cap, veh/h	544	117	467	420	524	164	700	915	831	407	892	14
Arrive On Green	0.40	0.40	0.39	0.40	0.40	0.39	0.52	0.52	0.52	0.52	0.52	0.51
Sat Flow, veh/h	1284	294	1176	1137	1319	414	1268	1761	1599	880	1717	27
Grp Volume(v), veh/h	1	0	180	333	0	67	49	364	103	1	0	64
Grp Sat Flow(s),veh/h/ln	1284	0	1470	1137	0	1733	1268	1761	1599	880	0	1745
Q Serve(g_s), s	0.1	0.0	10.2	34.0	0.0	2.9	2.4	15.0	4.0	0.1	0.0	2.2
Cycle Q Clear(g_c), s	2.5	0.0	10.2	43.6	0.0	2.9	4.1	15.0	4.0	15.1	0.0	2.2
Prop In Lane	1.00		0.80	1.00		0.24	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	544	0	584	420	0	689	700	915	831	407	0	906
V/C Ratio(X)	0.00	0.00	0.31	0.79	0.00	0.10	0.07	0.40	0.12	0.00	0.00	0.07
Avail Cap(c_a), veh/h	569	0	613	442	0	722	700	915	831	407	0	906
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	23.3	0.0	25.1	39.6	0.0	22.8	15.3	17.5	14.8	22.0	0.0	14.4
Incr Delay (d2), s/veh	0.0	0.0	0.3	9.2	0.0	0.1	0.2	1.3	0.3	0.0	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	6.5	15.9	0.0	2.2	1.3	10.2	2.6	0.0	0.0	1.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	23.3	0.0	25.4	48.8	0.0	22.8	15.5	18.8	15.1	22.1	0.0	14.5
LnGrp LOS	C		C	D		C	B	B	B	C		B
Approach Vol, veh/h		181			400			516			65	
Approach Delay, s/veh		25.4			44.4			17.7			14.7	
Approach LOS		C			D			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		67.3		52.7		67.3		52.7				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		59.0		49.0		59.0		49.0				
Max Q Clear Time (g_c+I1), s		17.5		12.2		17.6		46.1				
Green Ext Time (p_c), s		2.7		1.2		0.3		0.5				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				27.9								
HCM 6th LOS				C								
Notes												
User approved pedestrian interval to be less than phase max green.												

3: Station Boulevard & Driveway

2026 Base Conditions





Timing Plan: Weekday AM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	16	108	78	14	80	17
Future Volume (vph)	16	108	78	14	80	17
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		405	601		221	
Travel Time (s)		9.2	16.4		5.0	
Confl. Peds. (#/hr)						5
Peak Hour Factor	0.65	0.65	0.65	0.65	0.65	0.65
Heavy Vehicles (%)	44%	5%	4%	7%	8%	12%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Base Conditions

Timing Plan: Weekday AM Peak

Intersection						
Int Delay, s/veh	4.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	16	108	78	14	80	17
Future Vol, veh/h	16	108	78	14	80	17
Conflicting Peds, #/hr	0	0	0	0	0	5
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	44	5	4	7	8	12
Mvmt Flow	25	166	120	22	123	26
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	142	0	-	0	347	136
Stage 1	-	-	-	-	131	-
Stage 2	-	-	-	-	216	-
Critical Hdwy	4.54	-	-	-	6.48	6.32
Critical Hdwy Stg 1	-	-	-	-	5.48	-
Critical Hdwy Stg 2	-	-	-	-	5.48	-
Follow-up Hdwy	2.596	-	-	-	3.572	3.408
Pot Cap-1 Maneuver	1220	-	-	-	638	887
Stage 1	-	-	-	-	880	-
Stage 2	-	-	-	-	806	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1220	-	-	-	625	883
Mov Cap-2 Maneuver	-	-	-	-	625	-
Stage 1	-	-	-	-	862	-
Stage 2	-	-	-	-	806	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	1	0		12.1		
HCM LOS	B					
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1220	-	-	-	659	
HCM Lane V/C Ratio	0.02	-	-	-	0.226	
HCM Control Delay (s/veh)	8	-	-	-	12.1	
HCM Lane LOS	A	-	-	-	B	
HCM 95th %tile Q (veh)	0.1	-	-	-	0.9	

1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions

Timing Plan: Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	172	55	14	14	100	4	294	458	14	3	575	134
Future Volume (vph)	172	55	14	14	100	4	294	458	14	3	575	134
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			344			444			499	
Travel Time (s)		12.7			7.8			8.6			9.7	
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	1%	4%	0%	0%	1%	0%	2%	2%	0%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		11.0	30.0		30.0	30.0	
Total Split (s)	40.0	40.0		40.0	40.0		27.0	70.0		43.0	43.0	
Total Split (%)	36.4%	36.4%		36.4%	36.4%		24.5%	63.6%		39.1%	39.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 110

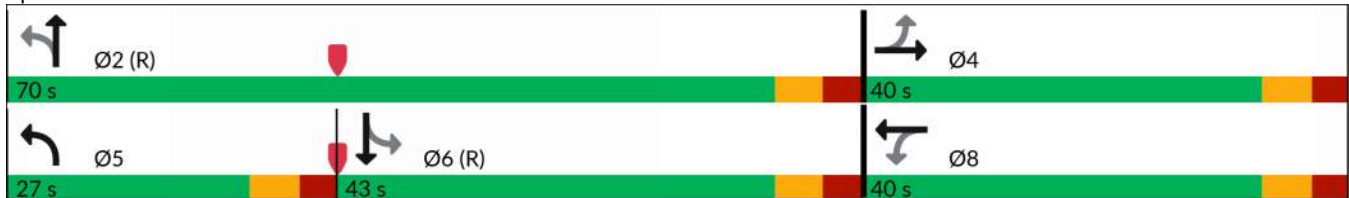
Actuated Cycle Length: 110

Offset: 12 (11%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 90

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BPM.syn





















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions























Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	172	55	14	14	100	4	294	458	14	3	575	134
Future Volume (veh/h)	172	55	14	14	100	4	294	458	14	3	575	134
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1935	1968	2027	1750	1805	1820	1921	1921	1949	1750	1722	1750
Adj Flow Rate, veh/h	176	56	14	14	102	4	300	467	14	3	587	137
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	1	4	0	0	1	0	2	2	0	0	2	0
Cap, veh/h	285	305	76	283	346	14	387	1280	38	519	717	167
Arrive On Green	0.20	0.20	0.19	0.40	0.40	0.38	0.10	0.69	0.68	0.53	0.53	0.52
Sat Flow, veh/h	1332	1520	380	1245	1725	68	1829	1855	56	855	1350	315
Grp Volume(v), veh/h	176	0	70	14	0	106	300	0	481	3	0	724
Grp Sat Flow(s), veh/h/ln	1332	0	1899	1245	0	1793	1829	0	1911	855	0	1665
Q Serve(g_s), s	14.0	0.0	3.4	0.8	0.0	4.4	7.4	0.0	11.5	0.2	0.0	39.7
Cycle Q Clear(g_c), s	17.9	0.0	3.4	3.7	0.0	4.4	7.4	0.0	11.5	0.2	0.0	39.7
Prop In Lane	1.00		0.20	1.00		0.04	1.00		0.03	1.00		0.19
Lane Grp Cap(c), veh/h	285	0	381	283	0	360	387	0	1319	519	0	884
V/C Ratio(X)	0.62	0.00	0.18	0.05	0.00	0.29	0.77	0.00	0.36	0.01	0.00	0.82
Avail Cap(c_a), veh/h	430	0	587	418	0	554	545	0	1319	519	0	884
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	44.2	0.0	36.6	28.4	0.0	27.7	20.4	0.0	7.1	12.1	0.0	21.5
Incr Delay (d2), s/veh	2.2	0.0	0.2	0.1	0.0	0.5	4.5	0.0	0.8	0.0	0.0	8.4
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.3	0.0	2.8	0.4	0.0	3.3	8.2	0.0	7.8	0.1	0.0	23.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	46.3	0.0	36.8	28.4	0.0	28.1	24.9	0.0	7.8	12.2	0.0	29.9
LnGrp LOS	D		D	C		C	C		A	B		C
Approach Vol, veh/h		246			120			781			727	
Approach Delay, s/veh		43.6			28.2			14.4			29.8	
Approach LOS		D			C			B			C	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		81.9		28.1	17.5	64.4		28.1				
Change Period (Y+Rc), s		7.0		7.0	7.0	7.0		7.0				
Max Green Setting (Gmax), s		63.0		33.0	20.0	36.0		33.0				
Max Q Clear Time (g_c+I1), s		13.5		20.4	9.9	41.7		6.4				
Green Ext Time (p_c), s		3.3		0.7	0.6	0.0		0.6				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			25.1									
HCM 6th LOS			C									

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions

Timing Plan: Weekday PM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	44	32	171	48	8	80	865	210	8	177	1
Future Volume (vph)	0	44	32	171	48	8	80	865	210	8	177	1
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		661			192			486			211	
Travel Time (s)		15.0			5.2			8.3			3.6	
Confl. Peds. (#/hr)									8	8		
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	2%	0%	0%	1%	1%	13%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	24.0	24.0		24.0	24.0		86.0	86.0	86.0	86.0	86.0	
Total Split (%)	21.8%	21.8%		21.8%	21.8%		78.2%	78.2%	78.2%	78.2%	78.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 110

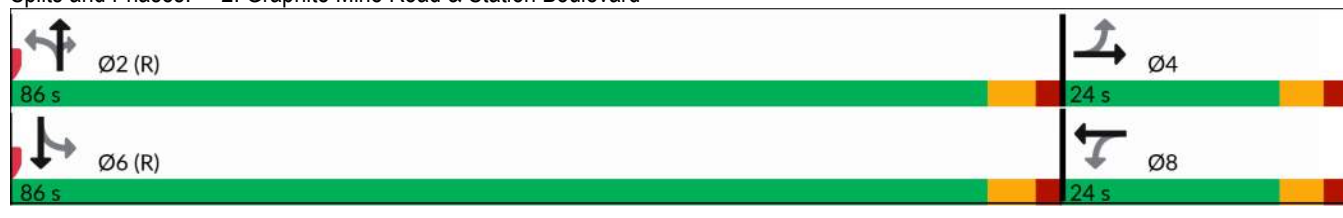
Actuated Cycle Length: 110

Offset: 53 (48%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BPM.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions











Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	44	32	171	48	8	80	865	210	8	177	1
Future Volume (veh/h)	0	44	32	171	48	8	80	865	210	8	177	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.99	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1872	1766	1837	1866	1875	1860	1935	1595	1764	1778
Adj Flow Rate, veh/h	0	46	33	178	50	8	83	901	219	8	184	1
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	0	0	2	2	0	0	1	1	13	1	0
Cap, veh/h	65	175	126	237	267	43	915	1370	1201	275	1290	7
Arrive On Green	0.00	0.17	0.16	0.17	0.17	0.16	0.74	0.74	0.74	0.74	0.74	0.73
Sat Flow, veh/h	1295	1014	727	1246	1545	247	1197	1860	1631	429	1752	10
Grp Volume(v), veh/h	0	0	79	178	0	58	83	901	219	8	0	185
Grp Sat Flow(s),veh/h/ln	1295	0	1741	1246	0	1792	1197	1860	1631	429	0	1762
Q Serve(g_s), s	0.0	0.0	4.3	15.2	0.0	3.0	2.4	27.2	4.5	1.1	0.0	3.4
Cycle Q Clear(g_c), s	0.0	0.0	4.3	19.0	0.0	3.0	5.3	27.2	4.5	28.3	0.0	3.4
Prop In Lane	1.00		0.42	1.00		0.14	1.00		1.00	1.00		0.01
Lane Grp Cap(c), veh/h	65	0	301	237	0	310	915	1370	1201	275	0	1297
V/C Ratio(X)	0.00	0.00	0.26	0.75	0.00	0.19	0.09	0.66	0.18	0.03	0.00	0.14
Avail Cap(c_a), veh/h	65	0	301	237	0	310	915	1370	1201	275	0	1297
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	39.6	47.7	0.0	39.0	5.0	7.4	4.4	14.6	0.0	4.3
Incr Delay (d2), s/veh	0.0	0.0	0.5	12.5	0.0	0.3	0.2	2.5	0.3	0.2	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	3.4	9.7	0.0	2.5	1.0	14.1	2.3	0.2	0.0	1.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	0.0	40.1	60.2	0.0	39.2	5.2	9.9	4.7	14.8	0.0	4.5
LnGrp LOS			D	E		D	A	A	A	B		A
Approach Vol, veh/h		79			236			1203			193	
Approach Delay, s/veh		40.1			55.0			8.6			4.9	
Approach LOS		D			E			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		86.0		24.0		86.0		24.0				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		80.0		18.0		80.0		18.0				
Max Q Clear Time (g_c+I1), s		29.7		6.3		30.8		21.5				
Green Ext Time (p_c), s		9.8		0.2		1.2		0.0				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				16.1								
HCM 6th LOS				B								

3: Station Boulevard & Driveway

2026 Base Conditions






Timing Plan: Weekday PM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	10	67	123	20	12	11
Future Volume (vph)	10	67	123	20	12	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		344	661		224	
Travel Time (s)		7.8	18.0		5.1	
Confl. Peds. (#/hr)						4
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Base Conditions

Timing Plan: Weekday PM Peak

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	10	67	123	20	12	11
Future Vol, veh/h	10	67	123	20	12	11
Conflicting Peds, #/hr	0	0	0	0	0	4
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	11	75	138	22	13	12

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	160	0	0 246 153
Stage 1	-	-	- 149 -
Stage 2	-	-	- 97 -
Critical Hdwy	4.1	-	- 6.4 6.2
Critical Hdwy Stg 1	-	-	- 5.4 -
Critical Hdwy Stg 2	-	-	- 5.4 -
Follow-up Hdwy	2.2	-	- 3.5 3.3
Pot Cap-1 Maneuver	1432	-	- 747 898
Stage 1	-	-	- 884 -
Stage 2	-	-	- 932 -
Platoon blocked, %		-	-
Mov Cap-1 Maneuver	1432	-	- 741 895
Mov Cap-2 Maneuver	-	-	- 741 -
Stage 1	-	-	- 877 -
Stage 2	-	-	- 932 -


Approach	EB	WB	SB
HCM Control Delay, s/v	1	0	9.6
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1432	-	-	-	807
HCM Lane V/C Ratio	0.008	-	-	-	0.032
HCM Control Delay (s/veh)	7.5	-	-	-	9.6
HCM Lane LOS	A	-	-	-	A
HCM 95th %tile Q (veh)	0	-	-	-	0.1

1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions

Timing Plan: Saturday Midday

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	147	53	21	13	56	4	199	281	13	6	640	135
Future Volume (vph)	147	53	21	13	56	4	199	281	13	6	640	135
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			0%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			25			35			35	
Link Distance (ft)		650			383			444			509	
Travel Time (s)		12.7			10.4			8.6			9.9	
Confl. Peds. (#/hr)			1	1			3					3
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Heavy Vehicles (%)	2%	0%	0%	0%	0%	0%	0%	1%	8%	0%	1%	2%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	10.0	10.0		10.0	10.0		11.0	23.0		23.0	23.0	
Total Split (s)	22.0	22.0		22.0	22.0		13.0	68.0		55.0	55.0	
Total Split (%)	24.4%	24.4%		24.4%	24.4%		14.4%	75.6%		61.1%	61.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 90

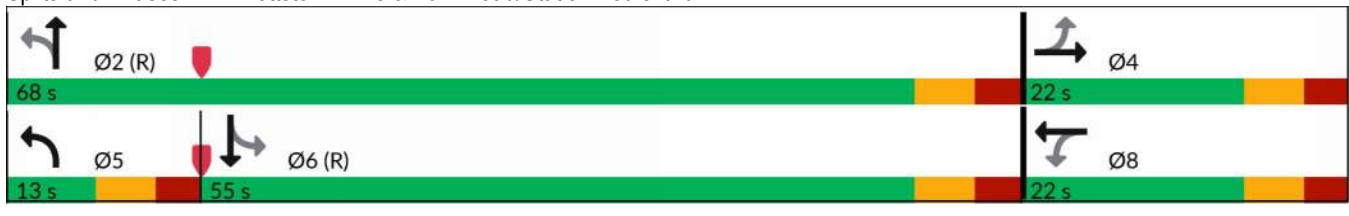
Actuated Cycle Length: 90

Offset: 26 (29%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 80

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BSAT.syn




















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Base Conditions























Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	147	53	21	13	56	4	199	281	13	6	640	135
Future Volume (veh/h)	147	53	21	13	56	4	199	281	13	6	640	135
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1921	2027	2027	1750	1820	1820	1800	1786	1688	1750	1736	1722
Adj Flow Rate, veh/h	152	55	22	13	58	4	205	290	13	6	660	139
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	2	0	0	0	0	0	0	1	8	0	1	2
Cap, veh/h	280	234	94	254	286	20	313	1182	53	635	767	162
Arrive On Green	0.17	0.17	0.16	0.17	0.17	0.16	0.08	0.70	0.69	0.55	0.55	0.54
Sat Flow, veh/h	1373	1376	550	1234	1682	116	1714	1696	76	1005	1389	293
Grp Volume(v), veh/h	152	0	77	13	0	62	205	0	303	6	0	799
Grp Sat Flow(s),veh/h/ln	1373	0	1926	1234	0	1798	1714	0	1772	1005	0	1682
Q Serve(g_s), s	9.6	0.0	3.1	0.8	0.0	2.7	4.2	0.0	5.6	0.2	0.0	36.5
Cycle Q Clear(g_c), s	11.7	0.0	3.1	3.4	0.0	2.7	4.2	0.0	5.6	0.2	0.0	36.5
Prop In Lane	1.00		0.29	1.00		0.06	1.00		0.04	1.00		0.17
Lane Grp Cap(c), veh/h	280	0	327	254	0	306	313	0	1235	635	0	929
V/C Ratio(X)	0.54	0.00	0.24	0.05	0.00	0.20	0.66	0.00	0.25	0.01	0.00	0.86
Avail Cap(c_a), veh/h	291	0	342	263	0	320	313	0	1235	635	0	929
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	36.9	0.0	32.4	33.6	0.0	32.1	17.8	0.0	5.0	9.1	0.0	17.3
Incr Delay (d2), s/veh	1.9	0.0	0.4	0.1	0.0	0.3	4.9	0.0	0.5	0.0	0.0	10.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	5.9	0.0	2.6	0.5	0.0	2.1	4.9	0.0	3.2	0.1	0.0	21.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	38.8	0.0	32.8	33.7	0.0	32.5	22.7	0.0	5.5	9.1	0.0	27.5
LnGrp LOS	D		C	C		C	C		A	A		C
Approach Vol, veh/h	229			75			508			805		
Approach Delay, s/veh	36.8			32.7			12.4			27.4		
Approach LOS	D			C			B			C		
Timer - Assigned Phs	2			4		5	6		8			
Phs Duration (G+Y+Rc), s	68.7			21.3		13.0	55.7		21.3			
Change Period (Y+Rc), s	7.0			7.0		7.0	7.0		7.0			
Max Green Setting (Gmax), s	61.0			15.0		6.0	48.0		15.0			
Max Q Clear Time (g_c+I1), s	7.6			14.2		6.7	38.5		5.9			
Green Ext Time (p_c), s	1.9			0.1		0.0	3.9		0.2			
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				24.3								
HCM 6th LOS				C								

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions

Timing Plan: Saturday MIDDAY

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	2	47	32	113	25	9	48	499	127	14	273	5
Future Volume (vph)	2	47	32	113	25	9	48	499	127	14	273	5
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		623			192			486			211	
Travel Time (s)		14.2			5.2			8.3			3.6	
Confl. Peds. (#/hr)									4	4		
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Heavy Vehicles (%)	0%	0%	3%	0%	0%	0%	0%	1%	2%	7%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	22.0	22.0		22.0	22.0		22.0	22.0	22.0	22.0	22.0	
Total Split (s)	22.0	22.0		22.0	22.0		41.0	41.0	41.0	41.0	41.0	
Total Split (%)	34.9%	34.9%		34.9%	34.9%		65.1%	65.1%	65.1%	65.1%	65.1%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 63

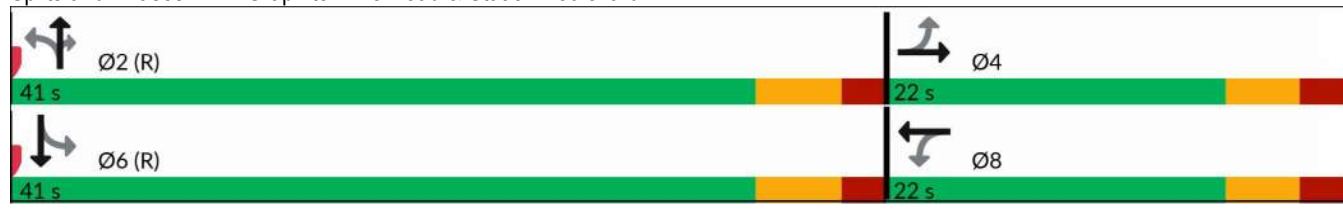
Actuated Cycle Length: 63

Offset: 14 (22%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 50

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\BSAT.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Base Conditions











Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	47	32	113	25	9	48	499	127	14	273	5
Future Volume (veh/h)	2	47	32	113	25	9	48	499	127	14	273	5
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1828	1794	1866	1866	1875	1860	1920	1679	1764	1778
Adj Flow Rate, veh/h	2	55	38	133	29	11	56	587	149	16	321	6
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Percent Heavy Veh, %	0	0	3	0	0	0	0	1	2	7	1	0
Cap, veh/h	358	202	139	312	252	96	718	1201	1047	427	1114	21
Arrive On Green	0.20	0.20	0.18	0.20	0.20	0.18	0.65	0.65	0.65	0.65	0.65	0.63
Sat Flow, veh/h	1316	1031	713	1250	1289	489	1054	1860	1622	648	1725	32
Grp Volume(v), veh/h	2	0	93	133	0	40	56	587	149	16	0	327
Grp Sat Flow(s),veh/h/ln	1316	0	1744	1250	0	1778	1054	1860	1622	648	0	1758
Q Serve(g_s), s	0.1	0.0	2.9	6.3	0.0	1.2	1.5	10.3	2.3	0.8	0.0	5.1
Cycle Q Clear(g_c), s	0.8	0.0	2.9	8.7	0.0	1.2	6.1	10.3	2.3	11.1	0.0	5.1
Prop In Lane	1.00		0.41	1.00		0.28	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	358	0	341	312	0	348	718	1201	1047	427	0	1135
V/C Ratio(X)	0.01	0.00	0.27	0.43	0.00	0.11	0.08	0.49	0.14	0.04	0.00	0.29
Avail Cap(c_a), veh/h	455	0	471	405	0	480	718	1201	1047	427	0	1135
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	21.0	0.0	21.7	25.0	0.0	21.0	6.1	5.8	4.4	8.7	0.0	4.9
Incr Delay (d2), s/veh	0.0	0.0	0.4	0.9	0.0	0.1	0.2	1.4	0.3	0.2	0.0	0.6
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	2.1	3.4	0.0	0.9	0.5	5.3	1.0	0.2	0.0	2.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	21.0	0.0	22.1	25.9	0.0	21.1	6.3	7.2	4.6	8.8	0.0	5.5
LnGrp LOS	C		C	C		C	A	A	A	A		A
Approach Vol, veh/h		95			173			792			343	
Approach Delay, s/veh		22.1			24.8			6.7			5.7	
Approach LOS		C			C			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		45.7		17.3		45.7		17.3				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		35.0		16.0		35.0		16.0				
Max Q Clear Time (g_c+I1), s		12.8		4.9		13.6		11.2				
Green Ext Time (p_c), s		4.4		0.3		1.9		0.2				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			9.7									
HCM 6th LOS			A									

3: Station Boulevard & Driveway

2026 Base Conditions






Timing Plan: Saturday MIDDAY

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	8	65	70	8	14	4
Future Volume (vph)	8	65	70	8	14	4
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		383	623		277	
Travel Time (s)		8.7	17.0		6.3	
Confl. Peds. (#/hr)						6
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Base Conditions

Timing Plan: Saturday MIDDAY

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	8	65	70	8	14	4
Future Vol, veh/h	8	65	70	8	14	4
Conflicting Peds, #/hr	0	0	0	0	0	6
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	2	0	0	0	0
Mvmt Flow	8	68	73	8	15	4
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	81	0	-	0	161	83
Stage 1	-	-	-	-	77	-
Stage 2	-	-	-	-	84	-
Critical Hdwy	4.1	-	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	-	3.5	3.3
Pot Cap-1 Maneuver	1529	-	-	-	835	982
Stage 1	-	-	-	-	951	-
Stage 2	-	-	-	-	944	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1529	-	-	-	831	977
Mov Cap-2 Maneuver	-	-	-	-	831	-
Stage 1	-	-	-	-	946	-
Stage 2	-	-	-	-	944	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	0.8	0		9.3		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1529	-	-	-	860	
HCM Lane V/C Ratio	0.005	-	-	-	0.022	
HCM Control Delay (s/veh)	7.4	-	-	-	9.3	
HCM Lane LOS	A	-	-	-	A	
HCM 95th %tile Q (veh)	0	-	-	-	0.1	





















Projected Conditions



1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions

Timing Plan: Weekday AM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	146	68	31	45	77	24	131	197	36	90	688	84
Future Volume (vph)	146	68	31	45	77	24	131	197	36	90	688	84
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			405			444			409	
Travel Time (s)		12.7			9.2			8.6			8.0	
Confl. Peds. (#/hr)									1	1		
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	7%	3%	4%	0%	3%	0%	12%	10%	18%	10%	4%	7%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		10.0	30.0		30.0	30.0	
Total Split (s)	36.0	36.0		36.0	36.0		19.0	84.0		65.0	65.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		15.8%	70.0%		54.2%	54.2%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 120

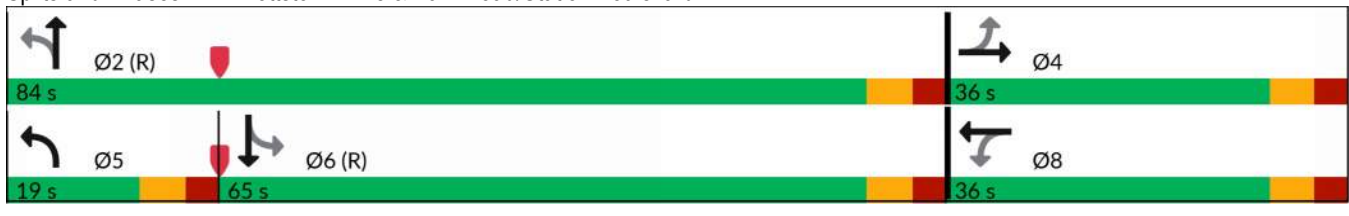
Actuated Cycle Length: 120

Offset: 74 (62%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 90

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PAM.syn




















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions

Timing Plan: Weekday AM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	146	68	31	45	77	24	131	197	36	90	688	84
Future Volume (veh/h)	146	68	31	45	77	24	131	197	36	90	688	84
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1850	1983	1968	1750	1776	1820	1778	1807	1693	1609	1694	1652
Adj Flow Rate, veh/h	155	72	33	48	82	26	139	210	38	96	732	89
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	7	3	4	0	3	0	12	10	18	10	4	7
Cap, veh/h	252	267	122	258	268	85	275	1031	187	627	863	105
Arrive On Green	0.21	0.21	0.20	0.07	0.07	0.07	0.06	0.69	0.68	0.58	0.58	0.57
Sat Flow, veh/h	1271	1287	590	1206	1292	410	1694	1489	269	973	1481	180
Grp Volume(v), veh/h	155	0	105	48	0	108	139	0	248	96	0	821
Grp Sat Flow(s),veh/h/ln	1271	0	1876	1206	0	1702	1694	0	1758	973	0	1661
Q Serve(g_s), s	14.1	0.0	5.7	4.6	0.0	7.2	3.6	0.0	6.1	5.5	0.0	49.0
Cycle Q Clear(g_c), s	20.9	0.0	5.7	9.7	0.0	7.2	3.6	0.0	6.1	5.5	0.0	49.0
Prop In Lane	1.00		0.31	1.00		0.24	1.00		0.15	1.00		0.11
Lane Grp Cap(c), veh/h	252	0	389	258	0	353	275	0	1218	627	0	968
V/C Ratio(X)	0.61	0.00	0.27	0.19	0.00	0.31	0.51	0.00	0.20	0.15	0.00	0.85
Avail Cap(c_a), veh/h	306	0	469	310	0	426	357	0	1218	627	0	968
HCM Platoon Ratio	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	49.2	0.0	40.1	51.3	0.0	47.7	21.3	0.0	6.6	11.6	0.0	20.7
Incr Delay (d2), s/veh	2.5	0.0	0.4	0.3	0.0	0.5	1.4	0.0	0.4	0.5	0.0	9.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.1	0.0	4.8	2.7	0.0	5.9	3.7	0.0	4.0	2.2	0.0	27.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	51.8	0.0	40.4	51.6	0.0	48.2	22.7	0.0	7.0	12.1	0.0	29.9
LnGrp LOS	D		D	D		D	C		A	B		C
Approach Vol, veh/h	260			156			387			917		
Approach Delay, s/veh	47.2			49.3			12.7			28.0		
Approach LOS	D			D			B			C		
Timer - Assigned Phs	2			4			5			6		
Phs Duration (G+Y+Rc), s	89.1			30.9			13.2			75.9		
Change Period (Y+Rc), s	7.0			7.0			7.0			7.0		
Max Green Setting (Gmax), s	77.0			29.0			12.0			58.0		
Max Q Clear Time (g_c+I1), s	8.1			23.4			6.1			51.0		
Green Ext Time (p_c), s	1.6			0.5			0.2			3.5		
Intersection Summary												
HCM 6th Ctrl Delay, s/veh	29.4											
HCM 6th LOS	C											

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions

Timing Plan: Weekday AM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	36	140	300	51	14	57	328	93	1	57	9
Future Volume (vph)	1	36	140	300	51	14	57	328	93	1	57	9
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		601			192			486			211	
Travel Time (s)		13.7			5.2			8.3			3.6	
Confl. Peds. (#/hr)									2	2		
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	0%	13%	10%	2%	4%	0%	7%	8%	4%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	55.0	55.0		55.0	55.0		65.0	65.0	65.0	65.0	65.0	
Total Split (%)	45.8%	45.8%		45.8%	45.8%		54.2%	54.2%	54.2%	54.2%	54.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 120

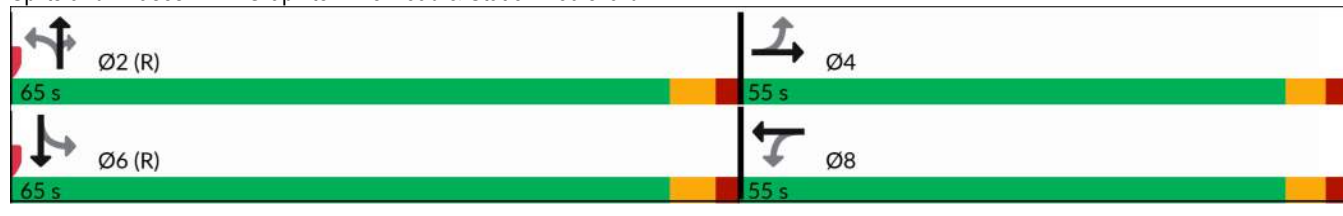
Actuated Cycle Length: 120

Offset: 9 (8%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PAM.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions











Timing Plan: Weekday AM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	1	36	140	300	51	14	57	328	93	1	57	9
Future Volume (veh/h)	1	36	140	300	51	14	57	328	93	1	57	9
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1682	1726	1766	1808	1866	1775	1761	1890	1778	1750	1778
Adj Flow Rate, veh/h	1	40	156	333	57	16	63	364	103	1	63	10
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	13	10	2	4	0	7	8	4	0	2	0
Cap, veh/h	552	122	477	418	553	155	678	897	815	396	751	119
Arrive On Green	0.41	0.41	0.40	0.41	0.41	0.40	0.51	0.51	0.51	0.51	0.51	0.50
Sat Flow, veh/h	1277	300	1171	1121	1358	381	1257	1761	1599	880	1473	234
Grp Volume(v), veh/h	1	0	196	333	0	73	63	364	103	1	0	73
Grp Sat Flow(s),veh/h/ln	1277	0	1471	1121	0	1739	1257	1761	1599	880	0	1707
Q Serve(g_s), s	0.1	0.0	11.0	34.5	0.0	3.1	3.2	15.3	4.1	0.1	0.0	2.6
Cycle Q Clear(g_c), s	2.7	0.0	11.0	45.1	0.0	3.1	5.4	15.3	4.1	15.4	0.0	2.6
Prop In Lane	1.00		0.80	1.00		0.22	1.00		1.00	1.00		0.14
Lane Grp Cap(c), veh/h	552	0	599	418	0	708	678	897	815	396	0	870
V/C Ratio(X)	0.00	0.00	0.33	0.80	0.00	0.10	0.09	0.41	0.13	0.00	0.00	0.08
Avail Cap(c_a), veh/h	564	0	613	429	0	725	678	897	815	396	0	870
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	22.7	0.0	24.6	39.5	0.0	22.1	16.3	18.2	15.4	23.0	0.0	15.1
Incr Delay (d2), s/veh	0.0	0.0	0.3	9.9	0.0	0.1	0.3	1.4	0.3	0.0	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	7.1	16.0	0.0	2.4	1.7	10.4	2.7	0.0	0.0	1.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	22.7	0.0	24.9	49.4	0.0	22.1	16.6	19.6	15.7	23.0	0.0	15.3
LnGrp LOS	C		C	D		C	B	B	B	C		B
Approach Vol, veh/h		197			406			530			74	
Approach Delay, s/veh		24.9			44.5			18.5			15.4	
Approach LOS		C			D			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		66.1		53.9		66.1		53.9				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		59.0		49.0		59.0		49.0				
Max Q Clear Time (g_c+I1), s		17.8		13.0		17.9		47.6				
Green Ext Time (p_c), s		2.8		1.3		0.4		0.3				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				28.1								
HCM 6th LOS				C								
Notes												
User approved pedestrian interval to be less than phase max green.												

3: Station Boulevard & Driveway

2026 Projected Conditions






Timing Plan: Weekday AM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	75	106	77	42	96	74
Future Volume (vph)	75	106	77	42	96	74
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		405	601		221	
Travel Time (s)		9.2	16.4		5.0	
Confl. Peds. (#/hr)						5
Peak Hour Factor	0.65	0.65	0.65	0.65	0.65	0.65
Heavy Vehicles (%)	44%	5%	4%	7%	8%	12%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Projected Conditions

Timing Plan: Weekday AM Peak

Intersection						
Int Delay, s/veh	7.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	75	106	77	42	96	74
Future Vol, veh/h	75	106	77	42	96	74
Conflicting Peds, #/hr	0	0	0	0	0	5
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	44	5	4	7	8	12
Mvmt Flow	115	163	118	65	148	114
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	183	0	-	0	544	156
Stage 1	-	-	-	-	151	-
Stage 2	-	-	-	-	393	-
Critical Hdwy	4.54	-	-	-	6.48	6.32
Critical Hdwy Stg 1	-	-	-	-	5.48	-
Critical Hdwy Stg 2	-	-	-	-	5.48	-
Follow-up Hdwy	2.596	-	-	-	3.572	3.408
Pot Cap-1 Maneuver	1175	-	-	-	490	864
Stage 1	-	-	-	-	862	-
Stage 2	-	-	-	-	669	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1175	-	-	-	442	860
Mov Cap-2 Maneuver	-	-	-	-	442	-
Stage 1	-	-	-	-	778	-
Stage 2	-	-	-	-	669	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	3.5	0		16.9		
HCM LOS				C		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1175	-	-	-	561	
HCM Lane V/C Ratio	0.098	-	-	-	0.466	
HCM Control Delay (s/veh)	8.4	-	-	-	16.9	
HCM Lane LOS	A	-	-	-	C	
HCM 95th %tile Q (veh)	0.3	-	-	-	2.5	

1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions

Timing Plan: Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	169	63	14	55	109	52	294	436	51	44	550	134
Future Volume (vph)	169	63	14	55	109	52	294	436	51	44	550	134
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			-4%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			30			35			35	
Link Distance (ft)		650			344			444			499	
Travel Time (s)		12.7			7.8			8.6			9.7	
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	1%	4%	0%	0%	1%	0%	2%	2%	0%	0%	2%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	30.0	30.0		30.0	30.0		11.0	30.0		30.0	30.0	
Total Split (s)	40.0	40.0		40.0	40.0		27.0	70.0		43.0	43.0	
Total Split (%)	36.4%	36.4%		36.4%	36.4%		24.5%	63.6%		39.1%	39.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 110

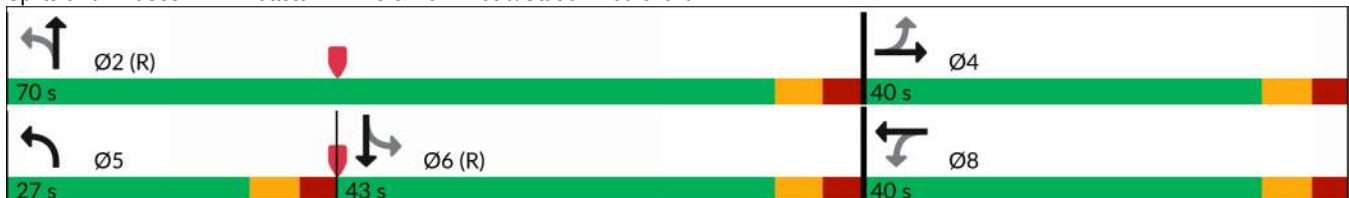
Actuated Cycle Length: 110

Offset: 12 (11%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 90

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PPM.syn





















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions


Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	169	63	14	55	109	52	294	436	51	44	550	134
Future Volume (veh/h)	169	63	14	55	109	52	294	436	51	44	550	134
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1935	1968	2027	1750	1805	1820	1921	1921	1949	1750	1722	1750
Adj Flow Rate, veh/h	172	64	14	56	111	53	300	445	52	45	561	137
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	1	4	0	0	1	0	2	2	0	0	2	0
Cap, veh/h	279	358	78	313	264	126	373	1117	131	484	665	162
Arrive On Green	0.23	0.23	0.22	0.46	0.46	0.44	0.11	0.66	0.65	0.50	0.50	0.49
Sat Flow, veh/h	1264	1564	342	1236	1155	551	1829	1688	197	843	1337	326
Grp Volume(v), veh/h	172	0	78	56	0	164	300	0	497	45	0	698
Grp Sat Flow(s),veh/h/ln	1264	0	1906	1236	0	1706	1829	0	1885	843	0	1663
Q Serve(g_s), s	14.4	0.0	3.6	3.3	0.0	7.2	8.0	0.0	13.3	3.1	0.0	40.0
Cycle Q Clear(g_c), s	21.1	0.0	3.6	6.4	0.0	7.2	8.0	0.0	13.3	3.1	0.0	40.0
Prop In Lane	1.00		0.18	1.00		0.32	1.00		0.10	1.00		0.20
Lane Grp Cap(c), veh/h	279	0	437	313	0	391	373	0	1248	484	0	827
V/C Ratio(X)	0.62	0.00	0.18	0.18	0.00	0.42	0.81	0.00	0.40	0.09	0.00	0.84
Avail Cap(c_a), veh/h	380	0	589	412	0	527	521	0	1248	484	0	827
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	44.0	0.0	34.2	25.7	0.0	25.2	21.2	0.0	8.6	14.7	0.0	24.0
Incr Delay (d2), s/veh	2.2	0.0	0.2	0.3	0.0	0.7	6.3	0.0	1.0	0.4	0.0	10.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	8.1	0.0	3.0	1.6	0.0	4.8	8.0	0.0	9.0	1.1	0.0	23.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	46.2	0.0	34.4	26.0	0.0	25.9	27.5	0.0	9.5	15.1	0.0	34.3
LnGrp LOS	D		C	C		C	C		A	B		C
Approach Vol, veh/h		250			220			797			743	
Approach Delay, s/veh		42.5			25.9			16.3			33.2	
Approach LOS		D			C			B			C	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		78.8		31.2	18.1	60.7		31.2				
Change Period (Y+Rc), s		7.0		7.0	7.0	7.0		7.0				
Max Green Setting (Gmax), s		63.0		33.0	20.0	36.0		33.0				
Max Q Clear Time (g_c+I1), s		15.3		23.6	10.5	42.0		9.2				
Green Ext Time (p_c), s		3.5		0.6	0.6	0.0		1.1				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			26.8									
HCM 6th LOS			C									

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions

Timing Plan: Weekday PM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	50	48	171	54	8	95	865	210	8	177	10
Future Volume (vph)	0	50	48	171	54	8	95	865	210	8	177	10
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		661			192			486			211	
Travel Time (s)		15.0			5.2			8.3			3.6	
Confl. Peds. (#/hr)									8	8		
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	2%	0%	0%	1%	1%	13%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	9.0	9.0		9.0	9.0		33.0	33.0	33.0	33.0	33.0	
Total Split (s)	24.0	24.0		24.0	24.0		86.0	86.0	86.0	86.0	86.0	
Total Split (%)	21.8%	21.8%		21.8%	21.8%		78.2%	78.2%	78.2%	78.2%	78.2%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 110

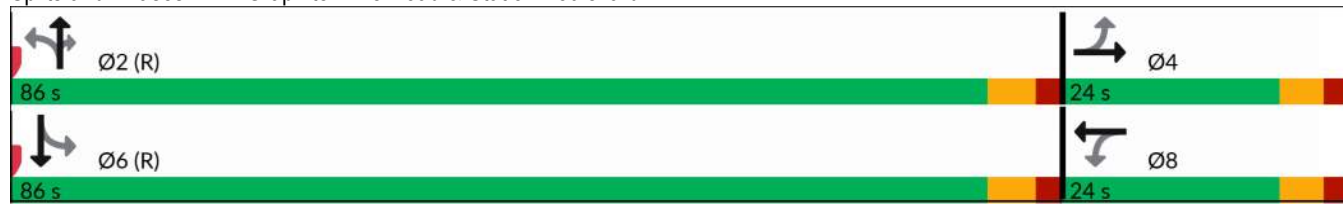
Actuated Cycle Length: 110

Offset: 53 (48%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PPM.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions











Timing Plan: Weekday PM Peak

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	50	48	171	54	8	95	865	210	8	177	10
Future Volume (veh/h)	0	50	48	171	54	8	95	865	210	8	177	10
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.99	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1872	1766	1837	1866	1875	1860	1935	1595	1764	1778
Adj Flow Rate, veh/h	0	52	50	178	56	8	99	901	219	8	184	10
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	0	0	2	2	0	0	1	1	13	1	0
Cap, veh/h	65	151	146	218	272	39	906	1370	1201	275	1220	66
Arrive On Green	0.00	0.17	0.16	0.17	0.17	0.16	0.74	0.74	0.74	0.74	0.74	0.73
Sat Flow, veh/h	1288	877	843	1221	1572	225	1187	1860	1631	429	1657	90
Grp Volume(v), veh/h	0	0	102	178	0	64	99	901	219	8	0	194
Grp Sat Flow(s),veh/h/ln	1288	0	1720	1221	0	1797	1187	1860	1631	429	0	1747
Q Serve(g_s), s	0.0	0.0	5.8	13.7	0.0	3.4	2.9	27.2	4.5	1.1	0.0	3.6
Cycle Q Clear(g_c), s	0.0	0.0	5.8	19.0	0.0	3.4	6.1	27.2	4.5	28.3	0.0	3.6
Prop In Lane	1.00		0.49	1.00		0.13	1.00		1.00	1.00		0.05
Lane Grp Cap(c), veh/h	65	0	297	218	0	310	906	1370	1201	275	0	1286
V/C Ratio(X)	0.00	0.00	0.34	0.82	0.00	0.21	0.11	0.66	0.18	0.03	0.00	0.15
Avail Cap(c_a), veh/h	65	0	297	218	0	310	906	1370	1201	275	0	1286
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	40.2	49.1	0.0	39.1	5.1	7.4	4.4	14.6	0.0	4.3
Incr Delay (d2), s/veh	0.0	0.0	0.7	20.9	0.0	0.3	0.2	2.5	0.3	0.2	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	4.5	10.3	0.0	2.8	1.2	14.1	2.3	0.2	0.0	2.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	0.0	40.9	70.0	0.0	39.4	5.4	9.9	4.7	14.8	0.0	4.6
LnGrp LOS			D	E		D	A	A	A	B		A
Approach Vol, veh/h		102			242			1219			202	
Approach Delay, s/veh		40.9			61.9			8.6			5.0	
Approach LOS		D			E			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		86.0		24.0		86.0		24.0				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		80.0		18.0		80.0		18.0				
Max Q Clear Time (g_c+I1), s		29.7		7.8		30.8		21.5				
Green Ext Time (p_c), s		9.9		0.3		1.2		0.0				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				17.4								
HCM 6th LOS				B								

3: Station Boulevard & Driveway

2026 Projected Conditions






Timing Plan: Weekday PM Peak

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	98	65	119	54	36	113
Future Volume (vph)	98	65	119	54	36	113
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		344	661		224	
Travel Time (s)		7.8	18.0		5.1	
Confl. Peds. (#/hr)						4
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Projected Conditions

Timing Plan: Weekday PM Peak

Intersection						
Int Delay, s/veh	5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	98	65	119	54	36	113
Future Vol, veh/h	98	65	119	54	36	113
Conflicting Peds, #/hr	0	0	0	0	0	4
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	110	73	134	61	40	127
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	195	0	-	0	458	169
Stage 1	-	-	-	-	165	-
Stage 2	-	-	-	-	293	-
Critical Hdwy	4.1	-	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	-	3.5	3.3
Pot Cap-1 Maneuver	1390	-	-	-	565	880
Stage 1	-	-	-	-	869	-
Stage 2	-	-	-	-	762	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1390	-	-	-	520	877
Mov Cap-2 Maneuver	-	-	-	-	520	-
Stage 1	-	-	-	-	800	-
Stage 2	-	-	-	-	762	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	4.7	0		11.2		
HCM LOS	B					
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1390	-	-	-	752	
HCM Lane V/C Ratio	0.079	-	-	-	0.223	
HCM Control Delay (s/veh)	7.8	-	-	-	11.2	
HCM Lane LOS	A	-	-	-	B	
HCM 95th %tile Q (veh)	0.3	-	-	-	0.8	

1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions

Timing Plan: Saturday Midday

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	147	61	21	45	64	29	199	271	38	40	622	135
Future Volume (vph)	147	61	21	45	64	29	199	271	38	40	622	135
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	11	13	13	12	14	14	11	11	11	11	11	11
Grade (%)		-4%			3%			0%			3%	
Storage Length (ft)	75		0	114		0	355		0	200		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		35			25			35			35	
Link Distance (ft)		650			383			444			509	
Travel Time (s)		12.7			10.4			8.6			9.9	
Confl. Peds. (#/hr)			1	1			3					3
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Heavy Vehicles (%)	2%	0%	0%	0%	0%	0%	0%	1%	8%	0%	1%	2%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			8		5	2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		3.0	15.0		15.0	15.0	
Minimum Split (s)	10.0	10.0		10.0	10.0		11.0	23.0		23.0	23.0	
Total Split (s)	22.0	22.0		22.0	22.0		13.0	68.0		55.0	55.0	
Total Split (%)	24.4%	24.4%		24.4%	24.4%		14.4%	75.6%		61.1%	61.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
All-Red Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0		6.0	6.0	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 90

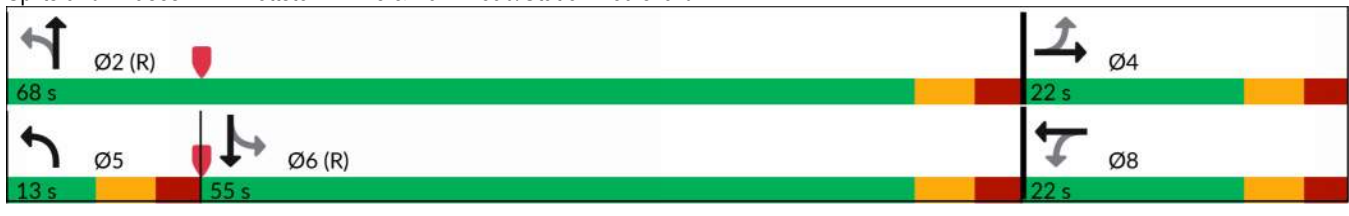
Actuated Cycle Length: 90

Offset: 26 (29%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 80

Control Type: Actuated-Coordinated

Splits and Phases: 1: Pottstown Pike & Park Road/Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PSAT.syn





















Synchro 12 Report

05/27/2025

1: Pottstown Pike & Park Road/Station Boulevard

2026 Projected Conditions

Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	147	61	21	45	64	29	199	271	38	40	622	135
Future Volume (veh/h)	147	61	21	45	64	29	199	271	38	40	622	135
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1921	2027	2027	1750	1820	1820	1800	1786	1688	1750	1736	1722
Adj Flow Rate, veh/h	152	63	22	46	66	30	205	279	39	41	641	139
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	2	0	0	0	0	0	0	1	8	0	1	2
Cap, veh/h	259	255	89	258	210	96	317	1056	148	620	752	163
Arrive On Green	0.18	0.18	0.17	0.18	0.18	0.17	0.08	0.69	0.68	0.54	0.54	0.53
Sat Flow, veh/h	1332	1434	501	1225	1183	538	1714	1533	214	991	1381	300
Grp Volume(v), veh/h	152	0	85	46	0	96	205	0	318	41	0	780
Grp Sat Flow(s), veh/h/ln	1332	0	1935	1225	0	1721	1714	0	1747	991	0	1681
Q Serve(g_s), s	10.0	0.0	3.4	3.0	0.0	4.4	4.3	0.0	6.3	1.8	0.0	35.5
Cycle Q Clear(g_c), s	13.9	0.0	3.4	5.9	0.0	4.4	4.3	0.0	6.3	1.8	0.0	35.5
Prop In Lane	1.00		0.26	1.00		0.31	1.00		0.12	1.00		0.18
Lane Grp Cap(c), veh/h	259	0	344	258	0	306	317	0	1203	620	0	915
V/C Ratio(X)	0.59	0.00	0.25	0.18	0.00	0.31	0.65	0.00	0.26	0.07	0.00	0.85
Avail Cap(c_a), veh/h	259	0	344	258	0	306	317	0	1203	620	0	915
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	38.0	0.0	31.9	34.1	0.0	32.4	17.5	0.0	5.4	9.7	0.0	17.5
Incr Delay (d2), s/veh	3.4	0.0	0.4	0.3	0.0	0.6	4.5	0.0	0.5	0.2	0.0	9.9
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	6.2	0.0	2.9	1.7	0.0	3.4	4.6	0.0	3.6	0.7	0.0	20.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	41.4	0.0	32.3	34.5	0.0	32.9	22.0	0.0	5.9	9.9	0.0	27.4
LnGrp LOS	D		C	C		C	C		A	A		C
Approach Vol, veh/h		237			142			523			821	
Approach Delay, s/veh		38.2			33.4			12.2			26.5	
Approach LOS		D			C			B			C	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		68.0		22.0	13.0	55.0		22.0				
Change Period (Y+Rc), s		7.0		7.0	7.0	7.0		7.0				
Max Green Setting (Gmax), s		61.0		15.0	6.0	48.0		15.0				
Max Q Clear Time (g_c+I1), s		8.3		16.4	6.8	37.5		8.4				
Green Ext Time (p_c), s		2.0		0.0	0.0	4.1		0.3				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh			24.3									
HCM 6th LOS			C									

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions

Timing Plan: Saturday MIDDAY

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	52	46	113	31	9	63	499	127	14	273	14
Future Volume (vph)	10	52	46	113	31	9	63	499	127	14	273	14
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	12	14	14	12	14	14	12	12	14	11	12	12
Grade (%)		0%			1%			-2%			2%	
Storage Length (ft)	100		0	210		0	95		95	90		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		30			25			40			40	
Link Distance (ft)		623			192			486			211	
Travel Time (s)		14.2			5.2			8.3			3.6	
Confl. Peds. (#/hr)									4	4		
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Heavy Vehicles (%)	0%	0%	3%	0%	0%	0%	0%	1%	2%	7%	1%	0%
Shared Lane Traffic (%)												
Turn Type	Perm	NA		Perm	NA		Perm	NA	Perm	Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	3.0	3.0		3.0	3.0		10.0	10.0	10.0	10.0	10.0	
Minimum Split (s)	22.0	22.0		22.0	22.0		22.0	22.0	22.0	22.0	22.0	
Total Split (s)	22.0	22.0		22.0	22.0		41.0	41.0	41.0	41.0	41.0	
Total Split (%)	34.9%	34.9%		34.9%	34.9%		65.1%	65.1%	65.1%	65.1%	65.1%	
Yellow Time (s)	3.5	3.5		3.5	3.5		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.5	2.5		2.5	2.5		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	-1.0	-1.0	-1.0	
Total Lost Time (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		C-Min	C-Min	C-Min	C-Min	C-Min	

Intersection Summary

Area Type: Other

Cycle Length: 63

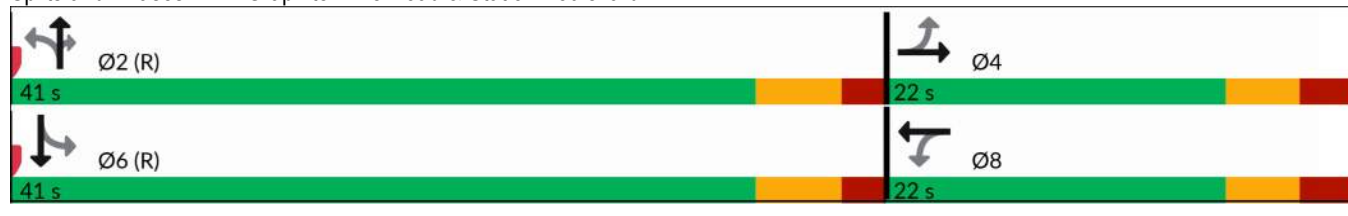
Actuated Cycle Length: 63

Offset: 14 (22%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 50

Control Type: Actuated-Coordinated

Splits and Phases: 2: Graphite Mine Road & Station Boulevard



Lanes, Volumes, Timings

C:\Box\Active\CELE\00001 - Byers Station\Planning\TIA\2025-05-19 Modified Trip Gen\CapAnal\2025-05 PSAT.syn























Synchro 12 Report

05/27/2025

2: Graphite Mine Road & Station Boulevard

2026 Projected Conditions











Timing Plan: Saturday MIDDAY

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	10	52	46	113	31	9	63	499	127	14	273	14
Future Volume (veh/h)	10	52	46	113	31	9	63	499	127	14	273	14
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1800	1872	1828	1794	1866	1866	1875	1860	1920	1679	1764	1778
Adj Flow Rate, veh/h	12	61	54	133	36	11	74	587	149	16	321	16
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Percent Heavy Veh, %	0	0	3	0	0	0	0	1	2	7	1	0
Cap, veh/h	370	192	170	311	287	88	690	1176	1025	414	1052	52
Arrive On Green	0.21	0.21	0.19	0.21	0.21	0.19	0.63	0.63	0.63	0.63	0.63	0.62
Sat Flow, veh/h	1307	916	811	1226	1372	419	1044	1860	1622	648	1665	83
Grp Volume(v), veh/h	12	0	115	133	0	47	74	587	149	16	0	337
Grp Sat Flow(s),veh/h/ln	1307	0	1726	1226	0	1791	1044	1860	1622	648	0	1748
Q Serve(g_s), s	0.5	0.0	3.6	6.4	0.0	1.3	2.2	10.7	2.3	0.9	0.0	5.5
Cycle Q Clear(g_c), s	1.3	0.0	3.6	9.5	0.0	1.3	7.2	10.7	2.3	11.5	0.0	5.5
Prop In Lane	1.00		0.47	1.00		0.23	1.00		1.00	1.00		0.05
Lane Grp Cap(c), veh/h	370	0	361	311	0	375	690	1176	1025	414	0	1105
V/C Ratio(X)	0.03	0.00	0.32	0.43	0.00	0.13	0.11	0.50	0.15	0.04	0.00	0.31
Avail Cap(c_a), veh/h	449	0	466	385	0	483	690	1176	1025	414	0	1105
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	20.6	0.0	21.3	24.9	0.0	20.3	6.8	6.2	4.7	9.3	0.0	5.3
Incr Delay (d2), s/veh	0.0	0.0	0.5	0.9	0.0	0.1	0.3	1.5	0.3	0.2	0.0	0.7
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	0.0	2.5	3.4	0.0	1.0	0.8	5.7	1.1	0.2	0.0	2.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	20.6	0.0	21.8	25.8	0.0	20.5	7.1	7.8	5.0	9.5	0.0	6.0
LnGrp LOS	C		C	C		C	A	A	A	A		A
Approach Vol, veh/h		127			180			810			353	
Approach Delay, s/veh		21.7			24.4			7.2			6.2	
Approach LOS		C			C			A			A	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		44.8		18.2		44.8		18.2				
Change Period (Y+Rc), s		6.0		6.0		6.0		6.0				
Max Green Setting (Gmax), s		35.0		16.0		35.0		16.0				
Max Q Clear Time (g_c+I1), s		13.2		5.6		14.0		12.0				
Green Ext Time (p_c), s		4.5		0.4		1.9		0.2				
Intersection Summary												
HCM 6th Ctrl Delay, s/veh				10.3								
HCM 6th LOS				B								

3: Station Boulevard & Driveway

2026 Projected Conditions






Timing Plan: Saturday MIDDAY

						
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	77	63	69	39	42	71
Future Volume (vph)	77	63	69	39	42	71
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Grade (%)		0%	3%		0%	
Link Speed (mph)		30	25		30	
Link Distance (ft)		383	623		277	
Travel Time (s)		8.7	17.0		6.3	
Confl. Peds. (#/hr)						6
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	0%	0%	0%
Shared Lane Traffic (%)						
Sign Control		Free	Free		Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

3: Station Boulevard & Driveway

2026 Projected Conditions

Timing Plan: Saturday MIDDAY

Intersection						
Int Delay, s/veh	4.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	77	63	69	39	42	71
Future Vol, veh/h	77	63	69	39	42	71
Conflicting Peds, #/hr	0	0	0	0	0	6
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	3	-	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	2	0	0	0	0
Mvmt Flow	80	66	72	41	44	74
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	113	0	-	0	319	99
Stage 1	-	-	-	-	93	-
Stage 2	-	-	-	-	226	-
Critical Hdwy	4.1	-	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	-	3.5	3.3
Pot Cap-1 Maneuver	1489	-	-	-	678	962
Stage 1	-	-	-	-	936	-
Stage 2	-	-	-	-	816	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1489	-	-	-	641	957
Mov Cap-2 Maneuver	-	-	-	-	641	-
Stage 1	-	-	-	-	885	-
Stage 2	-	-	-	-	816	-
Approach	EB	WB		SB		
HCM Control Delay, s/v	4.2	0		10.2		
HCM LOS				B		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1489	-	-	-	809	
HCM Lane V/C Ratio	0.054	-	-	-	0.145	
HCM Control Delay (s/veh)	7.6	-	-	-	10.2	
HCM Lane LOS	A	-	-	-	B	
HCM 95th %tile Q (veh)	0.2	-	-	-	0.5	

APPENDIX E:

Traffic Signal Plans



DETECTOR NOTES

- VIDEO STOP BAR DETECTION ON ALL APPROACHES FOR ALL SIGNALS
- WAVETRONIX ADVANCE DETECTION FOR MAIN APPROACHES, EXCEPT INT. #9

1. PROGRAM TO BE SELECTED BY ADAPTIVE SIGNAL CONTROL OR TRAFFIC RESPONSIVE BACKUP.
2. OFFSETS ARE REFERENCED TO THE START OF NEMA TS2 FIRST GREEN.
3. SYSTEM LIMITS : 11 INTERSECTIONS
MONITOR : PENNDOT DISTRICT 6-0
4. PRIMARY COORDINATION: FIBER OPTIC CABLE
SECONDARY COORDINATION: TBC (DEFAULT TO BACKUP TBC).
5. SYSTEM DESIGNED FOR ECONOLITE'S CENTRACS ATMS

REFER TO TRAFFIC SIGNAL PERMIT DRAWING FOR INDIVIDUAL
INTERSECTION OPERATION, GEOMETRY, PHASING AND CRITICAL TIMES.

TEST THE SYSTEM AT LOCAL INTERSECTION LEVEL, SUBSYSTEM LEVEL, MASTER CONTROLLER LEVEL, AND PERSONAL COMPUTER REMOTE DIAL UP LEVEL.

SET UP PENNDOT DISTRICT 6-0 COMPUTER WITH THE SYSTEM DATABASE AND GRAPHICS. MODIFY THE DATABASE AND GRAPHICS FOR SYSTEMS REVISIONS.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO
INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

MAINTAIN MASTER CONTROLLER COMMUNICATION SUCH AS PHONE DROPS.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING
UNLESS THE PERMITEE COMPLIES WITH THE PROVISIONS OF THE
LATEST AMENDMENT TO ACT 287, PREVENTION OF DAMAGE TO
UNDERGROUND UTILITIES DATE DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT FOR REVIEW PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BORED OR JACKED UNDER THE ROADWAY. INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS TC-8800 SERIES.

EVENT	DAY	TIME	PROGRAM	REMARKS
1	1-5	06:00	5*	AM PEAK
2	1-5	07:00	5	AM PEAK
3	1-5	09:00	6*	MIDDAY
4	1-5	13:30	7	PM PEAK
5	1-5	19:00	6*	EVENINGS
6	1-7	22:00	MAX 1	OVERNIGHTS
7	6-7	09:00	6*	WEEKENDS

OFFSETS IN SECONDS

* DURING EVENTS 1, 3, 5, AND 7 INTERSECTIONS 7 AND 10 TO RUN MAX I
DURING EVENT 1 INTERSECTIONS 6 AND 8 TO RUN MAX I

[illegible][illegible][illegible]

SCALE: NOT TO SCALE

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION ENGINEERING DISTRICT 6-0

COUNTY: CHESTER

MUNICIPALITY: UPPER UWCHLAN TOWNSHIPINTERSECTION: UPPER UWCHLAN SYSTEM PLAN

REVIEWED:

ATE

MUNICIPAL OFFICIAL

DATE

DISTRICT TRAFFIC ENGINEER

DATE _____

NO.	REVISION	DES./ REVW.	DATE	REVW.	DATE	RECOM.	DATE
1	ADAPTIVE SIGNAL SYSTEM	MCB	1/27	Z/25/12		4/8	3/9/12
2							
3							
4							
5							
6							
7							
8							

INTERCONNECT PERMIT #1-0014

SHEET 1 OF 1

GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.

REFER TO TRAFFIC SIGNAL PERMIT DRAWING FOR INDIVIDUAL INTERSECTION OPERATION, GEOMETRY, PHASING AND CRITICAL TIMES.

FOR CONSTRUCTION AND INSPECTION THE SYSTEM PERMIT SHOULD ALWAYS BE ACCOMPANIED WITH TRAFFIC SIGNAL PERMIT DRAWING.

TEST THE SYSTEM AT LOCAL INTERSECTION LEVEL, SUBSYSTEM LEVEL, MASTER CONTROLLER LEVEL AND PERSONAL COMPUTER REMOTE DIAL UP LEVEL.

GATHER THE SYSTEM FAILURE CRITICAL ALARMS REPORT AND ARCHIVE THEM WHERE APPLICABLE.

SET UP PENNDOT DISTRICT 6-0 COMPUTER WITH THE SYSTEM DATABASE AND GRAPHICS. MODIFY THE DATABASE AND GRAPHICS FOR SYSTEMS REVISIONS.

ASSIGN LOOP DETECTORS AND PROGRAM THE CONTROLLERS TO GATHER TRAFFIC VOLUMES IN 15 MINUTE INTERVAL, WHERE APPLICABLE.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

OBTAIN POLE ATTACHMENT PERMIT FOR AERIAL FIBER OPTIC INSTALLATION.

MAINTAIN MASTER CONTROLLER COMMUNICATION SUCH AS PHONE DROPS.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLETES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 287, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT FOR REVIEW PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD OR CONCRETE ROADWAY REGARDLESS OF AGE MUST BE BORED OR JACKED UNDER THE ROADWAY. INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS TC-7800 SERIES.

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION
ENGINEERING DISTRICT 6-0

COUNTY: CHESTER

MUNICIPALITY: UPPER UWCHLAN AND
UWCHLAN TOWNSHIPSINTERSECTION: POTTSTOWN PIKE (SR 0100) FROM
EAST TOWNSHIP LINE ROAD
TO EAGLEVIEW BOULEVARD

REVIEWED:

DATE

MUNICIPAL OFFICIAL

DATE

RECOMMENDED:

MARK L. KRAY

4-27-99

DOUGLAS MAY

4-28-99

DISTRICT TRAFFIC ENGINEER

DATE

NO	REVISION	DES/REVW	DATE	REVW	DATE	RECOM	DATE
1	RETIMING	TPD	3/10/09	LUTZ	3/10/09	LRB	3/10/09
2	RETIME INTERSECTION 2	TPD	4/27/09	LUTZ	4/27/09		
3							
4							
5							
6							
7							
8							

SYSTEM PERMIT # I-0014

CYCLE / SPLIT / OFFSET

PROGRAM 1 =		PHASE								CYCLE		OFFSET#1	OFFSET#2	OFFSET#3
INTERSECTIONS		FILE #	1	2	3	4	5	6	7	8				
11														
10														
9														
8														
7														
6														
5														
4														
3	POTTSTOWN PIKE / EAGLEVIEW BOULEVARD	2527		70		22		70		28	120	24		
2	POTTSTOWN PIKE / PENNSYLVANIA DRIVE	2931	32	64				96	24	120	0			
1	POTTSTOWN PIKE / EAST TOWNSHIP LINE ROAD	3040	17(LEAD)	72				88	31	120	14			
PROGRAM 2 =		PHASE								CYCLE		OFFSET#1	OFFSET#2	OFFSET#3
INTERSECTIONS		FILE #	1	2	3	4	5	6	7	8				
11														
10														
9														
8														
7														
6														
5														
4														
3	POTTSTOWN PIKE / EAGLEVIEW BOULEVARD	2527		65		33		65		22	120	1		
2	POTTSTOWN PIKE / PENNSYLVANIA DRIVE	2931	28	62				80	30	120	0			
1	POTTSTOWN PIKE / EAST TOWNSHIP LINE ROAD	3040	13(LEAD)	83				96	24	120	115			
PROGRAM 3 =		PHASE								CYCLE		OFFSET#1	OFFSET#2	OFFSET#3
INTERSECTIONS		FILE #	1	2	3	4	5	6	7	8				
11														
10														
9														
8														
7														
6														
5														
4														
3	POTTSTOWN PIKE / EAGLEVIEW BOULEVARD	2527		65		33		65		22	120	3		
2	POTTSTOWN PIKE / PENNSYLVANIA DRIVE	2931	30	58				89	31	120	0			
1	POTTSTOWN PIKE / EAST TOWNSHIP LINE ROAD	3040	17(LEAD)	72				89	31	120	6			
PROGRAM 4 =		PHASE								CYCLE		OFFSET#1	OFFSET#2	OFFSET#3
INTERSECTIONS		FILE #	1	2	3	4	5	6	7	8				
11														
10														
9														
8														
7														
6														
5														
4														
3	POTTSTOWN PIKE / EAGLEVIEW BOULEVARD	2527		65		33		65		22	120	3		
2	POTTSTOWN PIKE / PENNSYLVANIA DRIVE	2931	30	58				89	31	120	0			
1	POTTSTOWN PIKE / EAST TOWNSHIP LINE ROAD	3040	17(LEAD)	72				89	31	120	6			

NOTES:
- ALL SPLIT TIMES INCLUDE YELLOW AND RED TIMES FOR A GIVEN PHASE.
- REFER TO SIGNAL PERMIT PLAN FOR MAX 1, MAX 2 AND CLEARANCE AND PED TIMES.

CLOSED LOOP SYSTEM NOTES
TRAFFIC RESPONSIVE OPERATION

- PROGRAMS TO BE SELECTED BY CLOSED LOOP SYSTEM OR TBC BACKUP.
- SYSTEM 1: POTTSTOWN PIKE (3 INTERSECTIONS).
- SYSTEM 1 LIMITS: EAGLEVIEW BOULEVARD AND EAST TOWNSHIP LINE ROAD.
- MASTER CONTROLLER LOCATION: POTTSTOWN PIKE AND PENNSYLVANIA DRIVE.
- PRIMARY COORDINATION: CLOSED LOOP SYSTEM/HARD-WIRE COMMUNICATION CABLE (FIBER OPTIC)
- SECONDARY COORDINATION: TIME BASED COORDINATION (DEFAULT TO BACKUP TBC PROGRAM CHART)
- OFFSETS REFERENCED TO THE BEGINNING OF TS-2 1ST GREEN

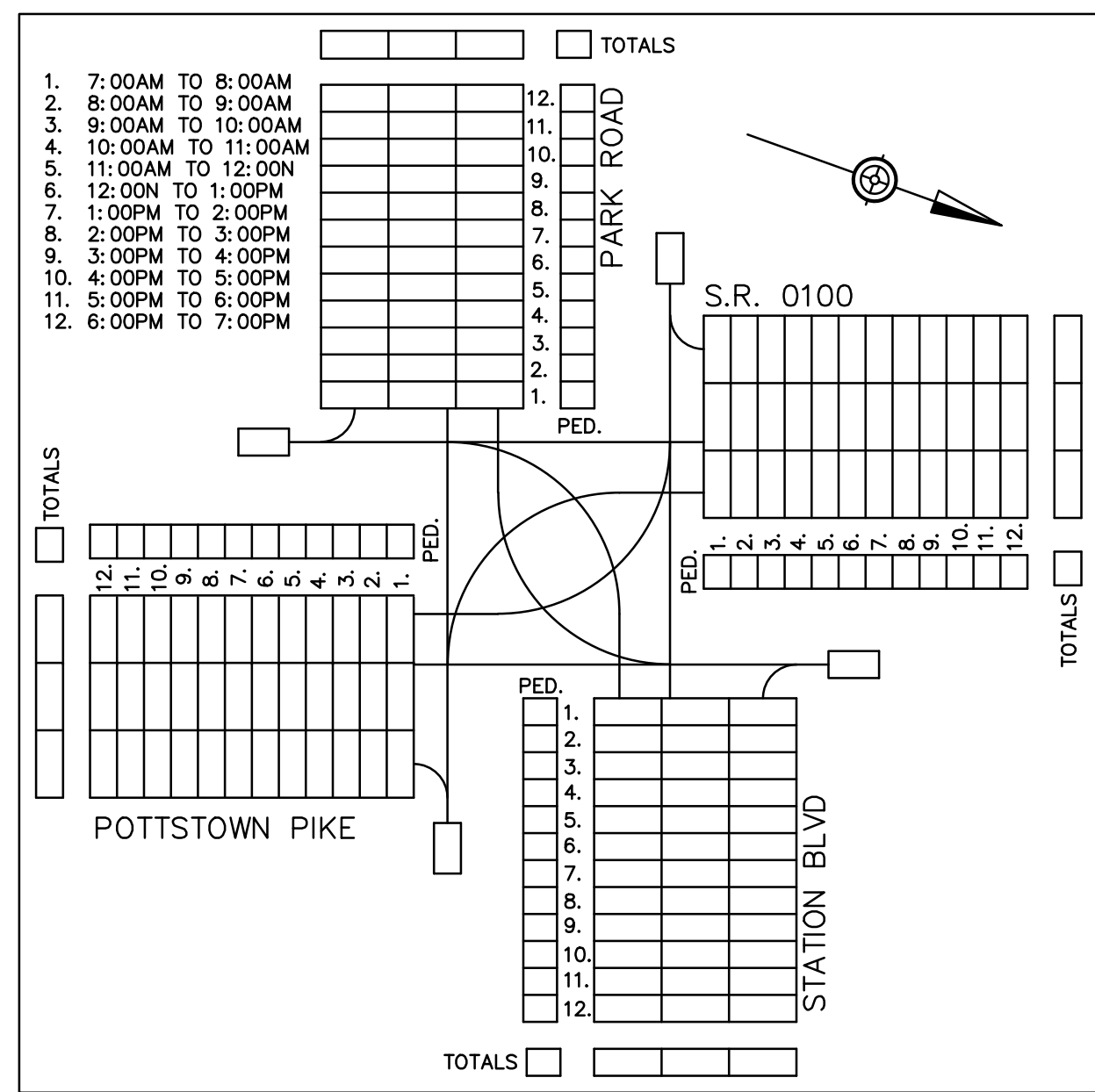
WEEKLY/BACKUP PROGRAM CHART

EVENT	DAY	TIME	PROGRAM*	REMARKS
1	1-7	0000	FREE	VOLUME DENSITY
2	1-5	0600	1	AM PEAK
3	1-5	0900	2	OFF PEAK
4	1-5	1500	3	PM PEAK
5	1-7	1900	2	OFF PEAK
6	6,7	0600	2	OFF PEAK

* DAY 1 = MONDAY
* MAX / FREE WHERE NOTED IN CYCLE / SPLIT / OFFSET MATRIX.

LEGEND

- ① INTERSECTION ADDRESS
- ② SYSTEM LOOP/IDENTIFYING NUMBER
- ③ LOOP SENSOR / INTERSECTION X - LOOP NUMBER Y
- ④ MICROWAVE DETECTION AREA
- ⑤ PHASE NUMBER
- NOT TO SCALE

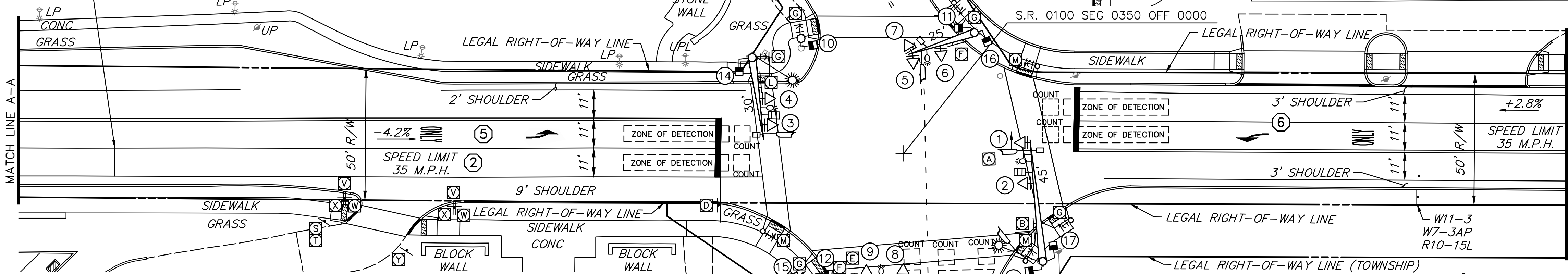
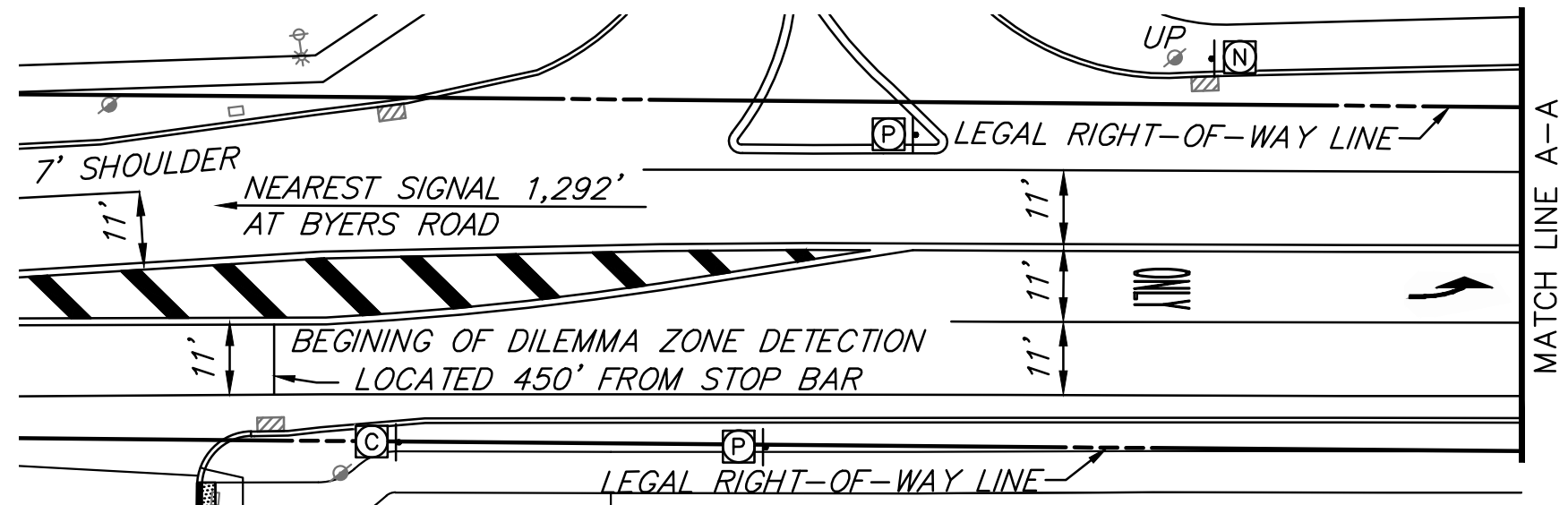


SEC/ACT RADAR DETECTION
LOCATED 230' FROM STOP BAR

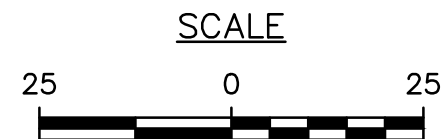
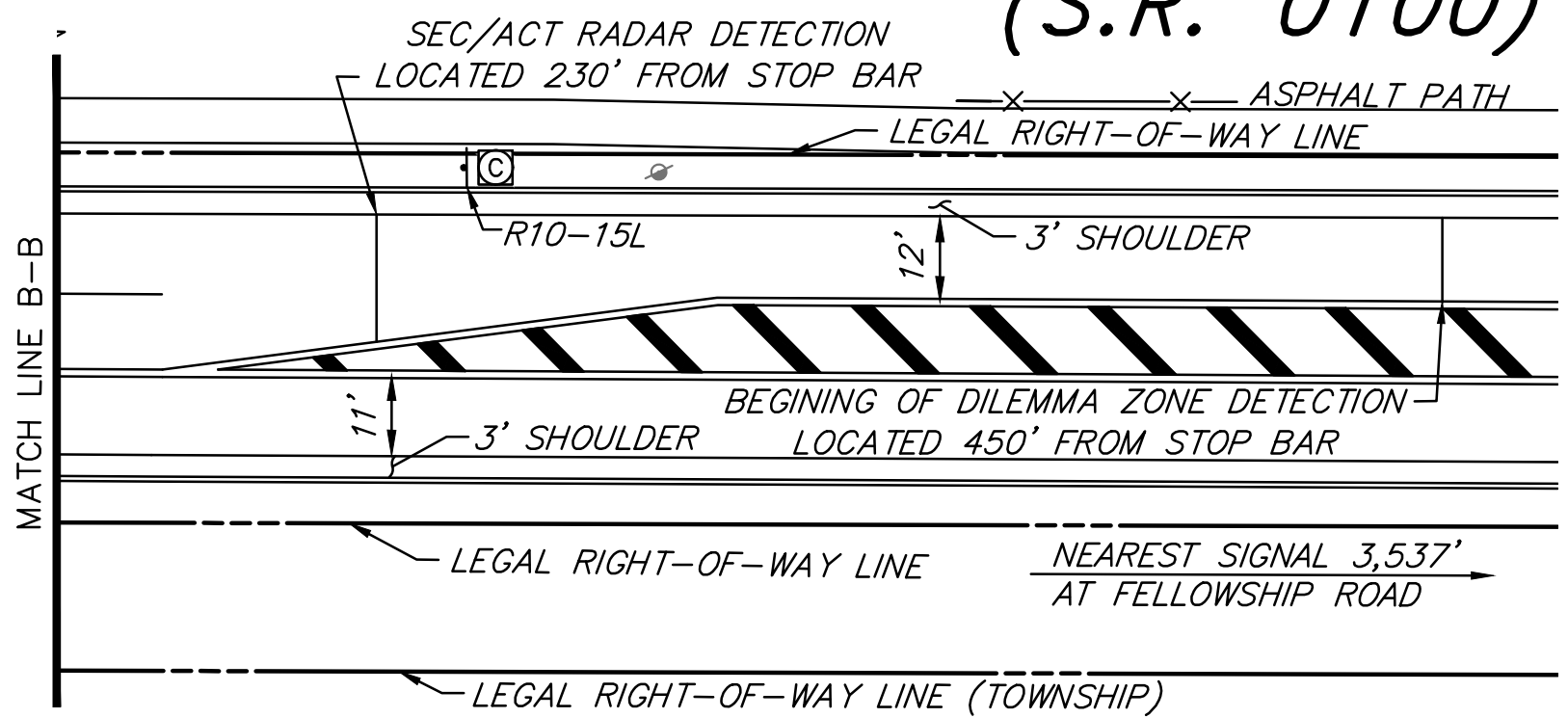
POTTSTOWN PIKE

PARK ROAD

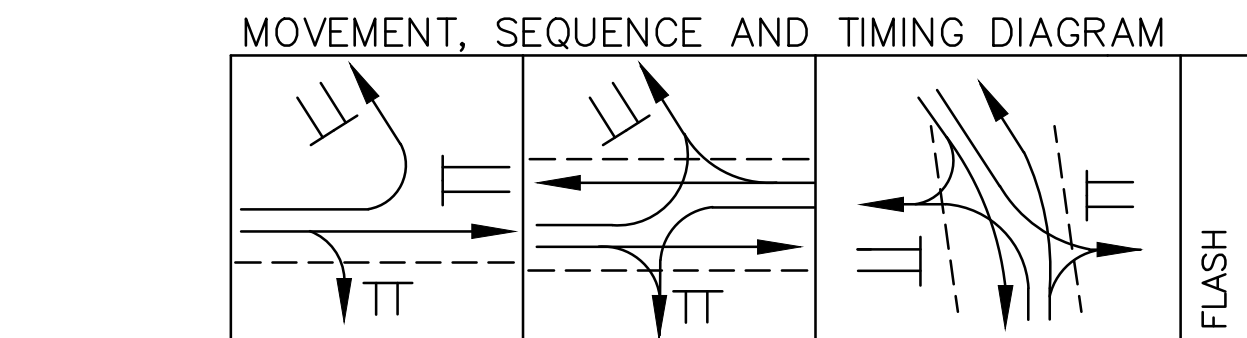
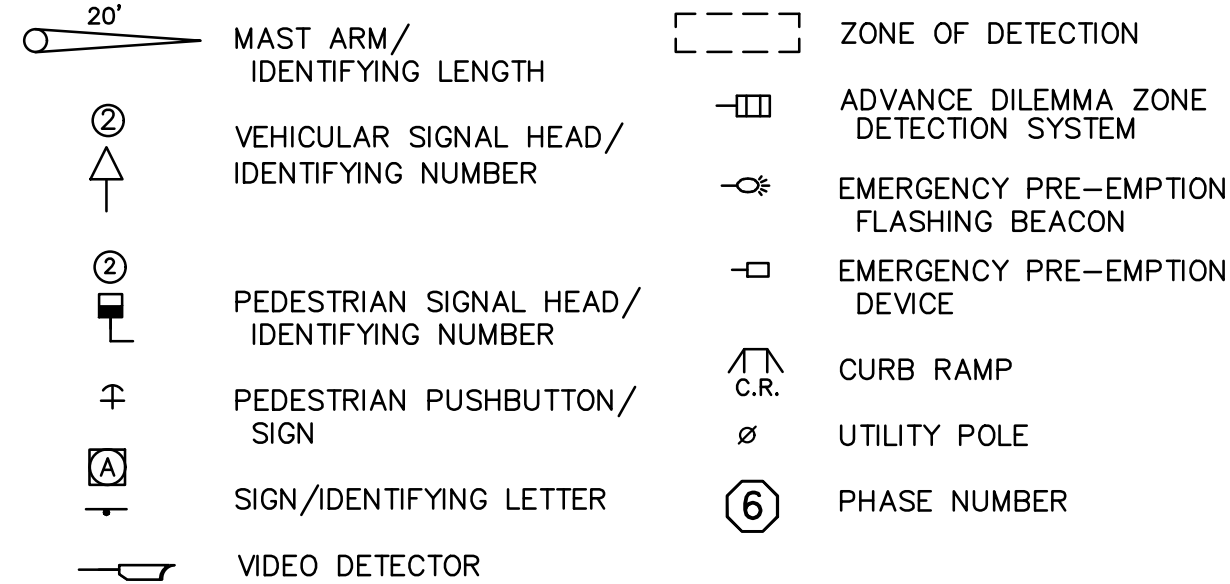
SIGN TABULATION			
PLAN SYMBOL	SERIES NUMBER	SIZE	REMARKS
A	R10-12	30X36	LEFT TURN YIELD ON GREEN
B	D3-5	96X28	OVERHEAD STREET NAME SIGN (SEE DETAIL)
C	R3-7L	30X30	LEFT LANE MUST TURN LEFT
D	R10-6L	24X30	STOP HERE ON RED, (LEFT)
E	R10-11	30X36	NO TURN ON RED
F	D3-4	96X16	OVERHEAD STREET NAME SIGN (SEE DETAIL)
G	R10-3E(L)	9X15	EDUCATIONAL PUSH BUTTON FOR WALK SIGNAL WITH COUNTDOWN TIMER
H	R10-3E(R)	9X15	EDUCATIONAL PUSH BUTTON FOR WALK SIGNAL WITH COUNTDOWN TIMER
I	R10-11	24X30	NO TURN ON RED
J	D3-5	96X28	OVERHEAD STREET NAME SIGN (SEE DETAIL)
K	R10-15R	30X30	TURNING TRAFFIC MUST YIELD TO PEDESTRIANS (RIGHT) SIGN
L	R3-7R	30X30	RIGHT LANE MUST TURN RIGHT
M	R3-2	24X24	NO LEFT TURN
N	W11-2	30X30	PEDESTRIAN SIGN
O	W16-7P	24X12	DIAGONAL DOWNWARD POINT ARROW
P	R5-1	30X30	DO NOT ENTER
Q	R6-1R	36X12	HORIZONTAL RIGHT ONE-WAY SIGN
R	R6-1L	36X12	HORIZONTAL LEFT ONE-WAY SIGN
S	R1-1	30X30	STOP SIGN



(S.R. 0100)

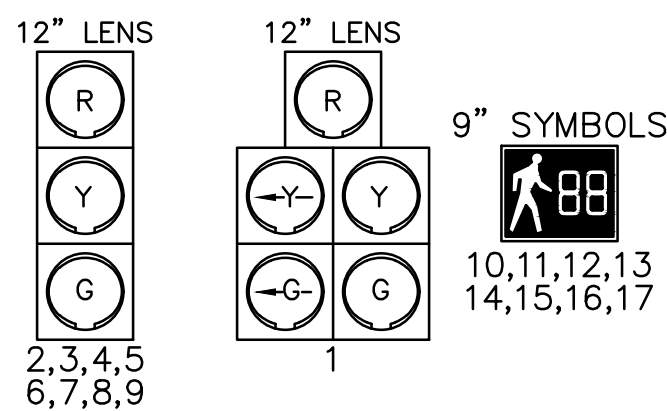


LEGEND



PHASE	2+5	2+6	4+8	FLASH
INTERVAL	1 2 3 4 5 6 7 8 9 10 11 12 13			
1	C/G	C/G	Y	Y
2	G	G	Y	Y
3,4	R	R	R	Y
5,6	R	R	R	R
7,8,9	R	R	R	R
10,11	H	H	H	OFF
12,13	M	FH	H	OFF
14,15	H	H	H	OFF
16,17	H	H	H	OFF

SIGNAL INDICATIONS



OPERATION NOTES

- ① C/G/≠ WHEN FOLLOWED BY PHASE 2+6
- ② G IF FOLLOWED BY 2+6
- ③ TIMING TO BE AS SHOWN IN PHASE 2+6. IT MAY TIME OUT IN THIS PHASE OR IN PHASE 2+6.
- ④ M IF FOLLOWED BY 2+6

++ADVANCED DILEMMA ZONE NOTES

EST. TIME OF ARRIVAL: MIN. 2.5 - MAX. 5.5 SEC.
RANGE OF PROTECTION : MIN. 0 - MAX. 450 FT.
SPEED BOUNDARY: MIN. 27 MPH - MAX. 100 MPH

++ DENSITY ZONE NOTES

RANGE OF DETECTION: STOP BAR TO 450 FT
SPEED BOUNDARY: MIN. 5 MPH - MAX. 45 MPH

* MAN SYMBOL UPON PEDESTRIAN ACTUATION ONLY, OTHERWISE HAND SYMBOL AT ALL TIMES.

- REFER TO SYSTEM PLAN #-0014 FOR ADAPTIVE SIGNAL CONTROL OPERATION.
- SIGNAL TO DWELL IN PHASE 2+6 UNTIL ACTUATED BY PHASE 4+8.

FIXED	4	3	4	3	4	3
MINIMUM	3		15		3	
SEC/ACT			2			
MAX INT.			24			
PASSAGE	5		1++		3	
MAX 1	7		41		41	
MAX 2	7		46		46	
PED (RING 1)*	③		7 22		5 2 16	
PED (RING 2)*			7 16		5 2 16	
MEMORY	NL		MN		L	

GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.

ALL MAINTENANCE WORK INCLUDING TRIMMING OF TREES, NECESSARY FOR PROPER VISIBILITY OF THE SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH PUBLICATION NO. 212.

POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET BEHIND THE FACE OF CURB OR THE EDGE OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM CLEARANCE HORIZONTALLY OF 2 FEET.

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16 FT. ABOVE THE ROADWAY. POST MOUNTED SIGNALS SHALL BE A MINIMUM OF 8 FT. ABOVE THE SIDEWALK OR PAVEMENT.

ALL OVERHEAD SIGNALS MUST BE RIGIDLY MOUNTED, TOP AND BOTTOM, AND EQUIPPED WITH BACKPLATES.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS MEASURED AT RIGHT ANGLES TO THE APPROACH SHALL BE 8 FEET.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

CURBING TO BE INSTALLED BY MUNICIPALITY AND WHERE NOTED, SHALL BE PLAIN CEMENT CONCRETE CURB OR GRANITE CURB, INSTALLED IN ACCORDANCE WITH DEPARTMENT SPECIFICATIONS FORM 408.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 287, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT, FOR REVIEW, PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BORED OR JACKED UNDER THE ROADWAY. INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS TC-8800 SERIES.

SYSTEM PERMIT #-0014

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION
ENGINEERING DISTRICT 6-0

COUNTY: CHESTER

MUNICIPALITY: UPPER UWCHLAN TOWNSHIP

INTERSECTION: POTTSTOWN PIKE (S.R. 0100) AND
PARK ROAD/STATION BLVD

REVIEWED:

DATE

MUNICIPAL OFFICIAL

RECOMMENDED:

LOUIS R. BELMONTE 10/27/04
DISTRICT TRAFFIC ENGINEER DATE

NO.	REVISION	DES./ REVW.	DATE	REVW.	DATE	RECOM.	DATE
1	AS-BUILT, REV. PED TIMES	MCM	6/8/09	LUTZ	6/8/09	LRB	6/8/09
2	ADAPTIVE SIGNAL SYSTEM	MCM	2/28/12	LUTZ	2/28/12	LRB	7/10/13
3	ADD LEFT TURN LANE TO EB PARK RD	MCM		ABP	7/10/13	LRB	7/10/13
4	AS-BUILTS	MCM	8/12/14	LUTZ	9/8/14	LRB	9/22/14
5	PROPOSED TRAIL AT NORTH WEST CORNER	MCM	4/28/16	LUTZ	5/4/16	ABP	5/10/16
6	ADAPTIVE SIGNAL SYSTEM REVISION	MCM	2/22/17	LUTZ	3/23/17	ABP	3/31/17
7	ADA CURB RAMP UPGRADES, RADAR DET. PARAMETERS UPDATED	MCM	4/19/19	LUTZ	5/15/19	ABP	5/25/19
8	ADD LEAD PED INTERVAL 9, REV. PED TIMES	MCM	6/23/20				

SHEET 2 OF 3 PERMIT # 62-2680 FILE # 2680

GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.

ALL MAINTENANCE WORK INCLUDING TRIMMING OF TREES, NECESSARY FOR PROPER VISIBILITY OF THE SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH PUBLICATION NO. 212.

POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET BEHIND THE FACE OF CURB OR THE EDGE OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM CLEARANCE HORIZONTALLY OF 2 FEET.

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16 FT. ABOVE THE ROADWAY. POST MOUNTED SIGNALS SHALL BE A MINIMUM OF 8 FT. ABOVE THE SIDEWALK OR PAVEMENT.

ALL OVERHEAD SIGNALS MUST BE RIGIDLY MOUNTED, TOP AND BOTTOM, AND EQUIPPED WITH BACKPLATES.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS MEASURED AT RIGHT ANGLES TO THE APPROACH SHALL BE 8 FEET.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

CURBING TO BE INSTALLED BY MUNICIPALITY AND WHERE NOTED, SHALL BE PLAIN CEMENT CONCRETE CURB OR GRANITE CURB, INSTALLED IN ACCORDANCE WITH DEPARTMENT SPECIFICATIONS FORM 408.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 287, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT, FOR REVIEW, PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BORED OR JACKED UNDER THE ROADWAY. INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS TC-8800 SERIES.

SYSTEM PERMIT #-0014

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION
ENGINEERING DISTRICT 6-0

COUNTY: CHESTER

MUNICIPALITY: UPPER UWCHLAN TOWNSHIP

INTERSECTION: POTTSTOWN PIKE (S.R. 0100) AND
PARK ROAD/STATION BLVD

REVIEWED:

DATE

MUNICIPAL OFFICIAL DATE

RECOMMENDED:

LOUIS R. BELMONTE 10/27/04
DISTRICT TRAFFIC ENGINEER DATE

NO.	REVISION	DES./REVW.	DATE	REVW.	DATE	RECOM.	DATE
1	AS-BUILT, REV. PED TIMES	MCM	6/8/09	LUTZ	6/8/09	LRB	6/8/09
2	ADAPTIVE SIGNAL SYSTEM	MCM	2/28/12	LUTZ	2/28/12	LRB	7/10/13
3	ADD LEFT TURN LANE TO EB PARK RD	MCM		ABP	7/10/13	LRB	7/10/13
4	AS-BUILTS	MCM	8/12/14	LUTZ	9/8/14	LRB	9/22/14
5	PROPOSED TRAIL AT NORTH WEST CORNER	MCM	4/28/16	LUTZ	5/4/16	ABP	5/10/16
6	ADAPTIVE SIGNAL SYSTEM REVISION	MCM	2/22/17	LUTZ	3/23/17	ABP	3/31/17
7	ADA CURB RAMP UPGRADES, RADAR DET. PARAMETERS UPDATED	MCM	4/19/19	LUTZ	5/15/19	ABP	5/25/19
8	ADD LEAD PED INTERVAL 9, REV. PED TIMES	MCM	6/23/20				

SHEET 3 OF 3 PERMIT # 62-2680 FILE # 2680

EMERGENCY PRE-EMPTION PHASING
MOVEMENT, SEQUENCE AND TIMING DIAGRAM

PHASE	6	2	4	8
INTERVAL	14 15 16	17 18 19	20 21 22	23 24 25
SIGNAL	1 R R R	2 -Y/G Y 2 R 1	4 R R R	8 R R R
2	R R R	G Y 1 R 1	R R R	R R R
3,4	G Y 1 R 1	R R R	R R R	R R R
7,8,9	R R R	R R R	G Y R	R R R
5,6	R R R	R R R	R R R	G Y R
10,11	H H H	H H H	H H H	H H H
12,13	H H H	H H H	H H H	H H H
14,15	H H H	H H H	H H H	H H H
16,17	H H H	H H H	H H H	H H H
FIXED TIME	* 4 3	* 4 3	* 4 3	* 4 3

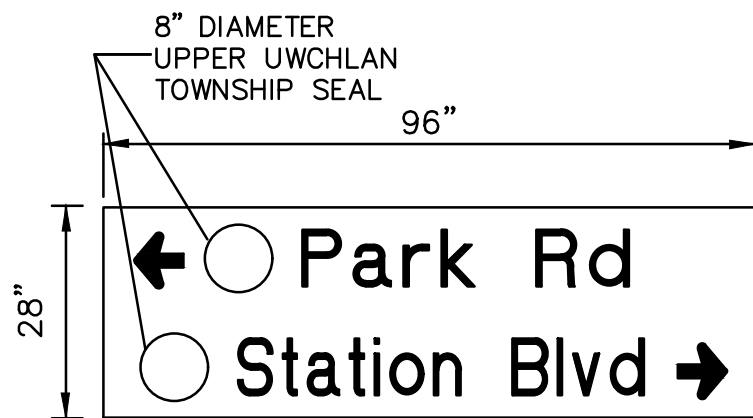
* FOR DURATION OF PRE-EMPTION

* NOTE: IF PRE-EMPTION EQUIPMENT HAS ENCODING CAPABILITIES FOR VEHICLE IDENTIFICATION, IT IS RECOMMENDED TO HAVE THE ZERO "00" FEATURE ON, TO GIVE UNCODED EMITTERS THE ABILITY TO ACTIVATE THE EMERGENCY PRE-EMPTION.

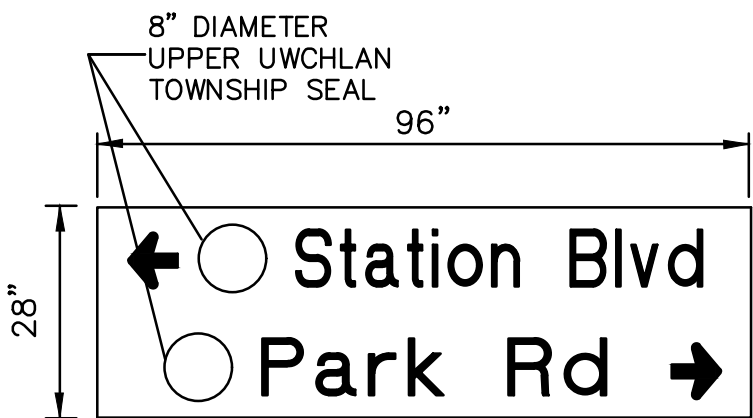
NOTES:

- 1 G WHEN RETURNING TO NORMAL OPERATION.
2 -Y/G WHEN RETURNING TO NORMAL OPERATION.

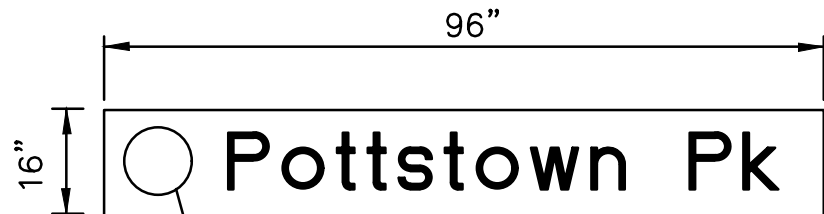
SIGN DETAILS
NOT TO SCALE



SIGN B
FONT: CLEARVIEW ONE
U.C. LET. 8"
L.C. LET. 6"
LEGEND: WHITE
BACKGROUND: GREEN



SIGN L
FONT: CLEARVIEW ONE
U.C. LET. 8"
L.C. LET. 6"
LEGEND: WHITE
BACKGROUND: GREEN



SIGN F
FONT: CLEARVIEW ONE
10% REDUCTION
U.C. LET. 8"
L.C. LET. 6"
LEGEND: WHITE
BACKGROUND: GREEN

EMERGENCY PRE-EMPTION NOTES:

- CONTROLLER TO BE EQUIPPED WITH EMERGENCY PRE-EMPTION FOR THE NORTHBOUND & SOUTHBOUND APPROACHES OF S.R. 0100 AND THE EASTBOUND APPROACH OF PARK ROAD & EASTBOUND APPROACH OF STATION BLVD. WITH A FAIL SAFE DEVICE FOR EACH DIRECTION OF OPERATION. THIS EMERGENCY BEACON SHALL CONSIST OF A FLASHING WHITE FLOOD LIGHT, AND SHALL FLASH WHEN THE EMERGENCY VEHICLE HAS CONTROL OF THE INTERSECTION FOR THE APPROPRIATE APPROACH. LOCATION OF EMERGENCY VEHICLE DETECTORS ARE TO BE FIELD ADJUSTED TO ACHIEVE MAXIMUM OPERATION.
- THE SIGNALS SHALL TERMINATE ALL GREEN INDICATIONS IMMEDIATELY, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, FOLLOWED BY THE COMPLETE YELLOW AND RED CLEARANCE INTERVALS, ACCORDINGLY. THEN THE GREEN INTERVAL FOR THE PRE-EMPTED PHASE SHALL FOLLOW. ONLY THOSE PHASES NOT POSING A YELLOW TRAP CONDITION MAY REMAIN GREEN (2+5) WHEN GOVERNED BY THE APPROACHING EMERGENCY VEHICLE.
- THE SIGNALS SHALL TIME OUT ALL YELLOW AND RED INDICATIONS, WHEN ACTIVATED BY EMERGENCY VEHICLE, FOLLOWED BY THE GREEN INTERVAL OF THE PRE-EMPTION PHASE GOVERNED BY THE APPROACHING EMERGENCY VEHICLE.
- IF SIGNALS HAS BEEN ACTUATED BY PEDESTRIAN PUSH BUTTON AND THE SIGNAL IS PRE-EMPTED DURING THE MAN INTERVAL, THE "MAN" INTERVAL SHALL TERMINATE IMMEDIATELY FOLLOWED BY THE "FLASHING HAND" INDICATION IN ITS ENTIRETY, FOLLOWED BY THE APPROPRIATE SELECTIVE CLEARANCES BEFORE PROCEEDING INTO THE PRE-EMPTION PHASE.
- IF THE SIGNALS ARE FLASHING WHEN ACTIVATED BY AN EMERGENCY VEHICLE, ALL SIGNALS SHALL REMAIN FLASHING.
- IF ADDITIONAL PRE-EMPTION PHASES ARE ACTIVATED WHILE IN PRE-EMPTION, THE ORIGINAL PRE-EMPTION PHASE SHALL TIME OUT BEFORE PROCEEDING TO THE NEXT PRE-EMPTION PHASE.
- UPON COMPLETION OF PRE-EMPTION, PHASE 2,4,6 OR 8 IN RETURNING TO NORMAL OPERATION, PHASE 2+6 INTERVAL 5 SHALL FOLLOW.
- IN EMERGENCY PRE-EMPTION, NO PRIORITY SHALL BE ESTABLISHED, PRE-EMPTION SHALL BE A "FIRST COME, FIRST SERVE" OPERATION.

APPENDIX F:

Turn Lane Warrant Analysis



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: PG Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: AM Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 2px solid red; padding: 2px; display: inline-block;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Left Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement	Include?	Volume	% Trucks	PCEV	
Advancing	Left	75	44.0%	92	Advancing Volume: 201 Opposing Volume: 122 Left Turn Volume: 92
	Through	-	5.0%	109	
	Right	Yes	0		
Opposing	Left	0		0	% Left Turns in Advancing Volume: 45.77%
	Through	-	4.0%	78	
	Right	Yes	42	7.0%	
Right Turn Lane Volume Calculations					
Movement	Include?	Volume	% Trucks	PCEV	
Advancing	Left	0		N/A	Advancing Volume: N/A Right Turn Volume: N/A
	Through	-	4.0%	N/A	
	Right	-	42	7.0%	

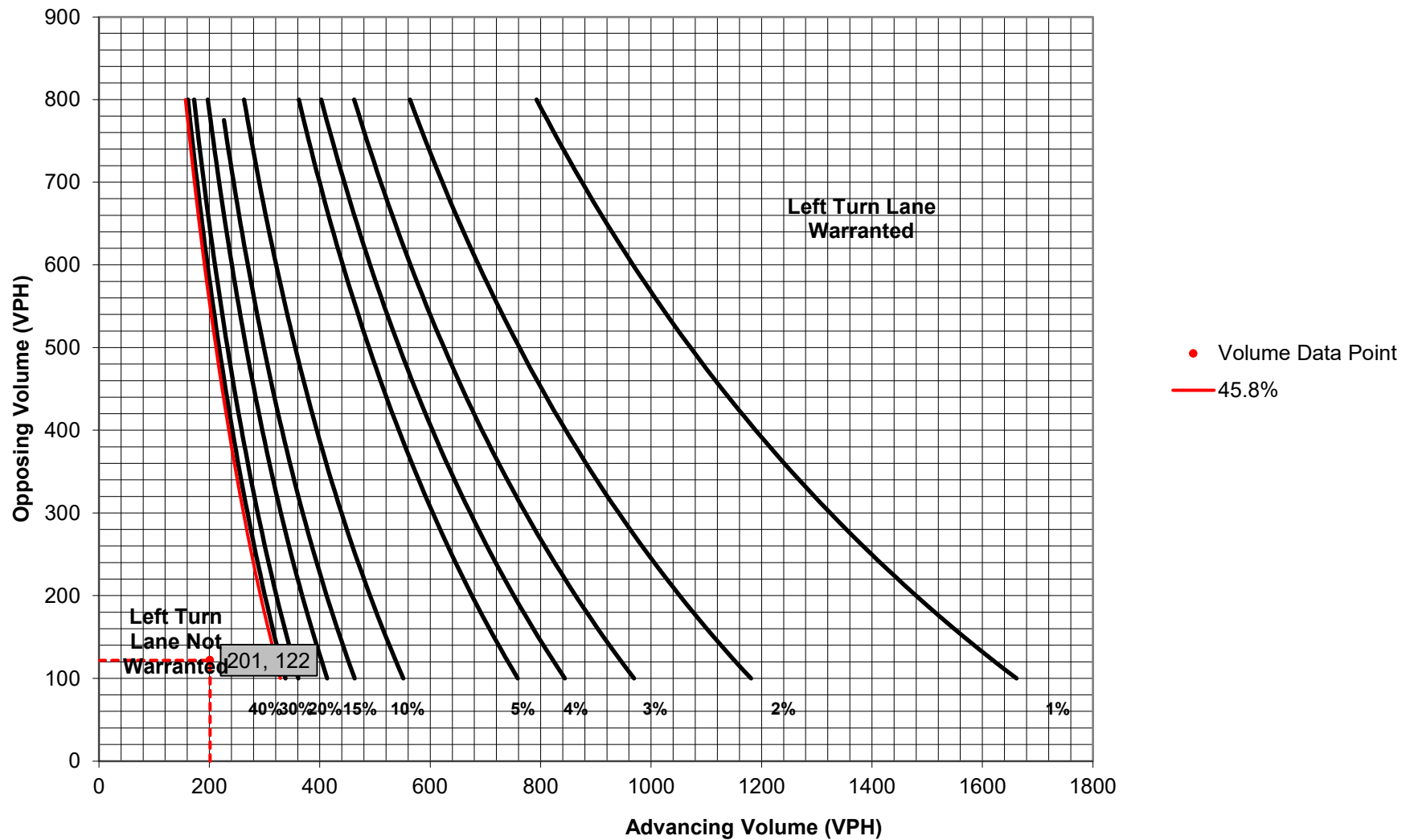
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: Figure 1 Warrant Met?: No	Applicable Warrant Figure: N/A Warrant Met?: N/A

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 92 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known): 	Average # of Vehicles/Cycle: N/A																																								
PennDOT Publication 46, Exhibit 11-6																																									
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th rowspan="3" style="text-align: center;">Type of Traffic Control</th> <th colspan="6" style="text-align: center;">Speed (MPH)</th> </tr> <tr> <th colspan="2" style="text-align: center;">25-35</th> <th colspan="2" style="text-align: center;">40-45</th> <th colspan="2" style="text-align: center;">50-60</th> </tr> <tr> <th colspan="6" style="text-align: center;">Turn Demand Volume</th> </tr> <tr> <th></th> <th style="text-align: center;">High</th> <th style="text-align: center;">Low</th> <th style="text-align: center;">High</th> <th style="text-align: center;">Low</th> <th style="text-align: center;">High</th> <th style="text-align: center;">Low</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">Signalized</td> <td style="text-align: center;">A</td> <td style="text-align: center;">A</td> <td style="text-align: center;">B or C</td> <td style="text-align: center;">B or C</td> <td style="text-align: center;">B or C</td> <td style="text-align: center;">B or C</td> </tr> <tr> <td style="text-align: center;">Unsignalized</td> <td style="text-align: center;">A</td> <td style="text-align: center;">A</td> <td style="text-align: center;">C</td> <td style="text-align: center;">B</td> <td style="text-align: center;">B or C</td> <td style="text-align: center;">B</td> </tr> </tbody> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume							High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																								
	25-35		40-45		50-60																																				
	Turn Demand Volume																																								
	High	Low	High	Low	High	Low																																			
Signalized	A	A	B or C	B or C	B or C	B or C																																			
Unsignalized	A	A	C	B	B or C	B																																			
Left Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Left Turn Lane Storage Length: N/A Feet																																									
Additional Findings: N/A																																									
Additional Comments / Justifications:																																									

Figure 1. Warrant for left turn lanes on two-lane roadways
 (speeds to 35 mph, unsignalized and signalized intersections)
 (L = % Left Turns in Advancing Volume)



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: PG Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: AM Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 1px solid red; padding: 2px; display: inline-block; margin-top: 5px;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Right Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations						
Movement	Include?	Volume	% Trucks	PCEV		
Advancing	Left	75	44.0%	N/A	Advancing Volume: N/A Opposing Volume: N/A Left Turn Volume: N/A	
	Through	-	106	5.0%		N/A
	Right	Yes	0			N/A
Opposing	Left	0		N/A	% Left Turns in Advancing Volume: N/A	
	Through	-	76	4.0%		N/A
	Right	Yes	42	7.0%		N/A
Right Turn Lane Volume Calculations						
Movement	Include?	Volume	% Trucks	PCEV		
Advancing	Left	0		0	Advancing Volume: 122 Right Turn Volume: 44	
	Through	-	76	4.0%		78
	Right	-	42	7.0%		44

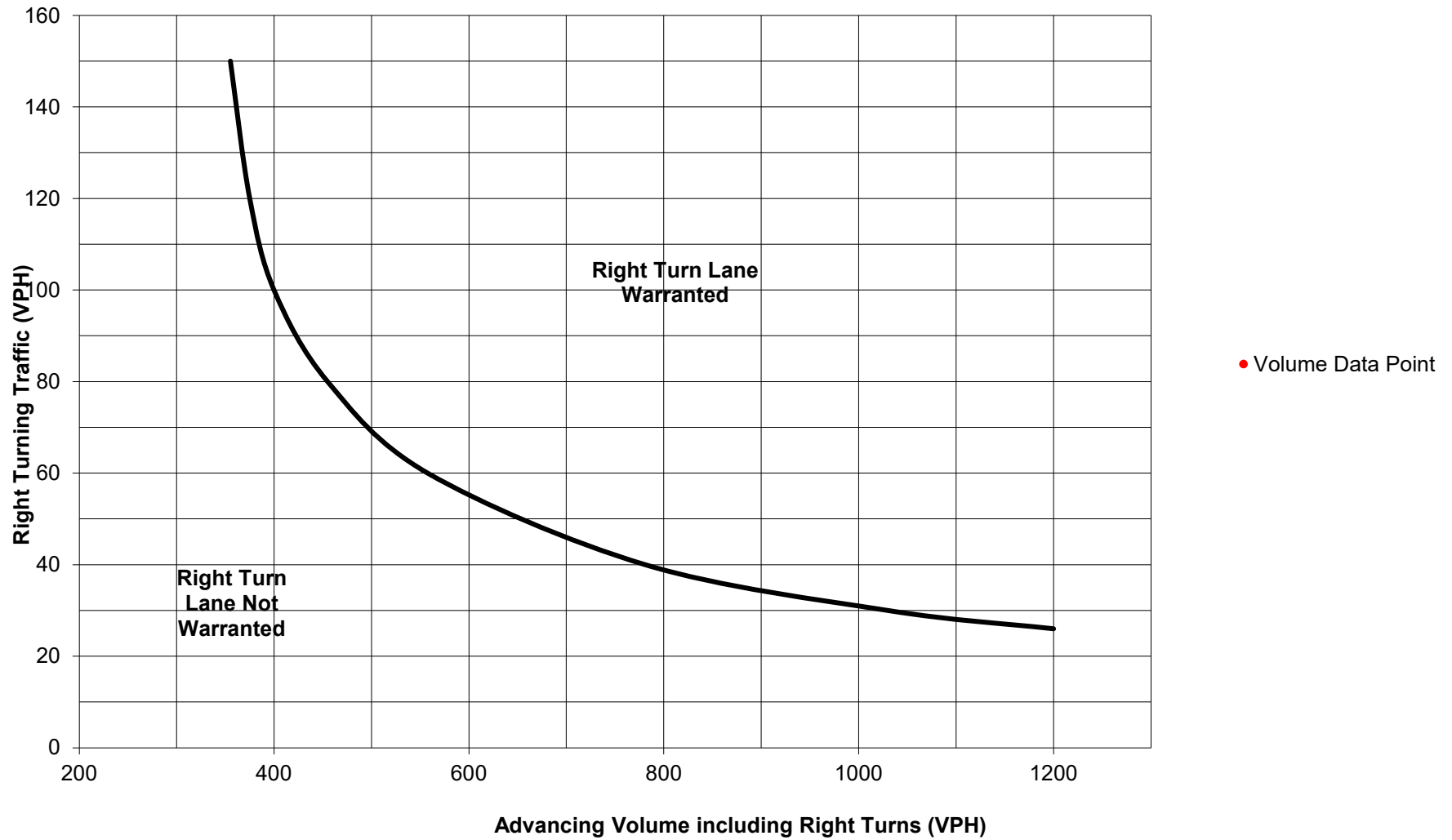
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A Warrant Met?: N/A	Applicable Warrant Figure: Figure 9 Warrant Met?: No

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 44 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known): 	Average # of Vehicles/Cycle: N/A																																							
PennDOT Publication 46, Exhibit 11-6																																								
<table style="width: 100%; border-collapse: collapse; border: 1px solid black;"> <tr style="background-color: #f2f2f2;"> <th rowspan="4" style="text-align: center; padding: 5px;">Type of Traffic Control</th> <th colspan="6" style="text-align: center; padding: 5px;">Speed (MPH)</th> </tr> <tr style="background-color: #f2f2f2;"> <th colspan="2" style="text-align: center; padding: 5px;">25-35</th> <th colspan="2" style="text-align: center; padding: 5px;">40-45</th> <th colspan="2" style="text-align: center; padding: 5px;">50-60</th> </tr> <tr style="background-color: #f2f2f2;"> <th colspan="6" style="text-align: center; padding: 5px;">Turn Demand Volume</th> </tr> <tr style="background-color: #f2f2f2;"> <th style="text-align: center; padding: 5px;">High</th> <th style="text-align: center; padding: 5px;">Low</th> <th style="text-align: center; padding: 5px;">High</th> <th style="text-align: center; padding: 5px;">Low</th> <th style="text-align: center; padding: 5px;">High</th> <th style="text-align: center; padding: 5px;">Low</th> </tr> <tr> <td style="text-align: center; padding: 5px;">Signalized</td> <td style="text-align: center; padding: 5px;">A</td> <td style="text-align: center; padding: 5px;">A</td> <td style="text-align: center; padding: 5px;">B or C</td> <td style="text-align: center; padding: 5px;">B or C</td> <td style="text-align: center; padding: 5px;">B or C</td> <td style="text-align: center; padding: 5px;">B or C</td> </tr> <tr> <td style="text-align: center; padding: 5px;">Unsignalized</td> <td style="text-align: center; padding: 5px;">A</td> <td style="text-align: center; padding: 5px;">A</td> <td style="text-align: center; padding: 5px;">C</td> <td style="text-align: center; padding: 5px;">B</td> <td style="text-align: center; padding: 5px;">B or C</td> <td style="text-align: center; padding: 5px;">B</td> </tr> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume						High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																							
	25-35		40-45		50-60																																			
	Turn Demand Volume																																							
	High	Low	High	Low	High	Low																																		
Signalized	A	A	B or C	B or C	B or C	B or C																																		
Unsignalized	A	A	C	B	B or C	B																																		
Right Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Right Turn Lane Storage Length: N/A Feet																																								
Additional Findings: N/A																																								
Additional Comments / Justifications: <div style="border: 1px solid black; height: 40px; width: 100%;"></div>																																								

**Figure 9. Warrant for right turn lanes on two-lane roadways
(40 mph or lower speeds, unsignalized and signalized intersections)**



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: SL Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: PM Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 2px solid red; padding: 2px; display: inline-block;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Left Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	98	0.0%	98
	Through	-	65	0.0%	65
	Right	Yes	0		0
Opposing	Left	Yes	0		0
	Through	-	119	0.0%	119
	Right	Yes	54	0.0%	54
Advancing Volume: 163 Opposing Volume: 173 Left Turn Volume: 98					
% Left Turns in Advancing Volume: 60.12%					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0		N/A
	Through	-	119	0.0%	N/A
	Right	-	54	0.0%	N/A
Advancing Volume: N/A Right Turn Volume: N/A					

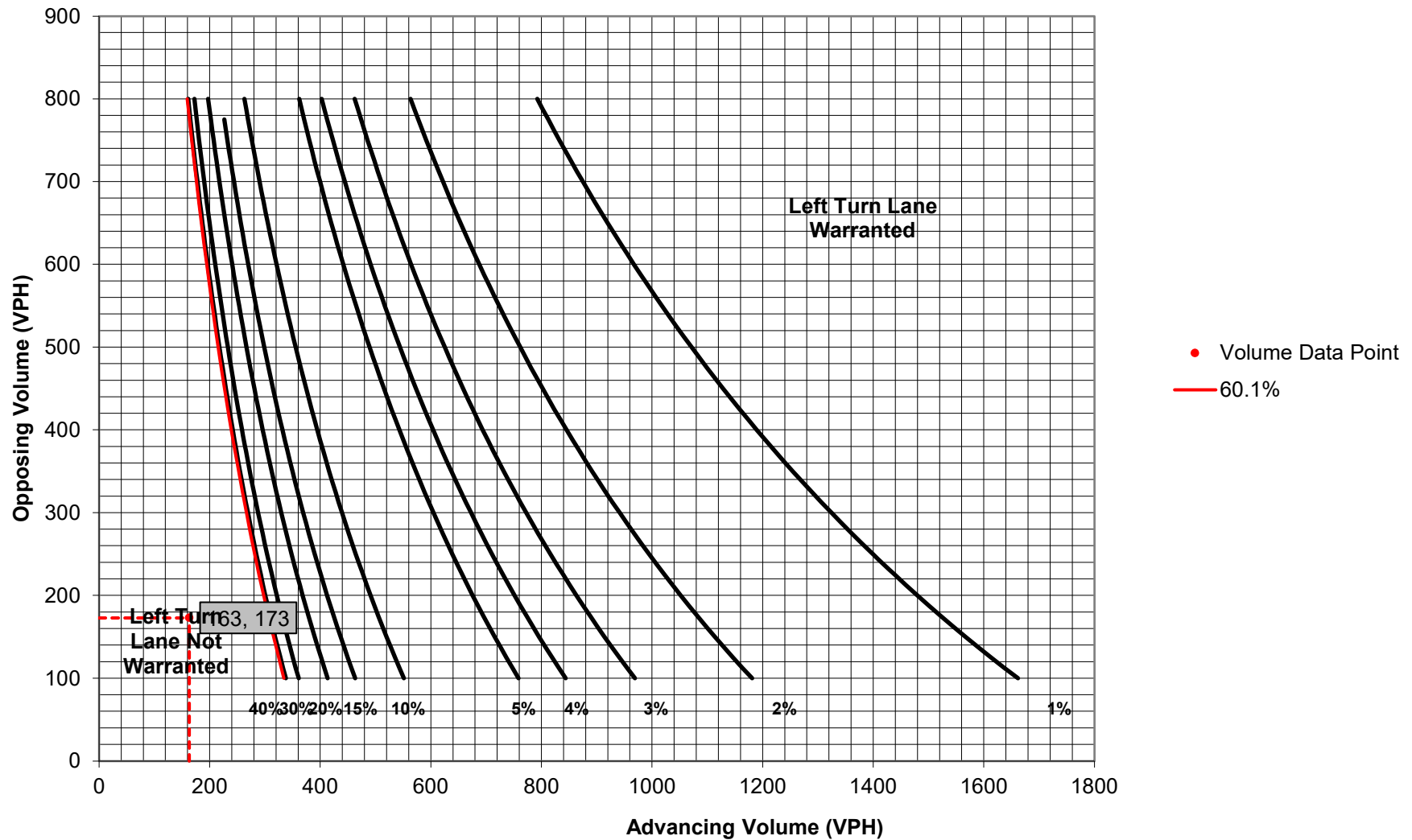
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: Figure 1 Warrant Met?: No	Applicable Warrant Figure: N/A Warrant Met?: N/A

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 98 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known): 	Average # of Vehicles/Cycle: N/A																																							
PennDOT Publication 46, Exhibit 11-6																																								
<table border="1" style="width: 100%; border-collapse: collapse; text-align: center;"> <tr> <th rowspan="4">Type of Traffic Control</th> <th colspan="6">Speed (MPH)</th> </tr> <tr> <th colspan="2">25-35</th> <th colspan="2" rowspan="2">40-45</th> <th colspan="2" rowspan="2">50-60</th> </tr> <tr> <th colspan="6">Turn Demand Volume</th> </tr> <tr> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> </tr> <tr> <td>Signalized</td> <td>A</td> <td>A</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> </tr> <tr> <td>Unsignalized</td> <td>A</td> <td>A</td> <td>C</td> <td>B</td> <td>B or C</td> <td>B</td> </tr> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume						High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																							
	25-35		40-45		50-60																																			
	Turn Demand Volume																																							
	High	Low	High	Low	High	Low																																		
Signalized	A	A	B or C	B or C	B or C	B or C																																		
Unsignalized	A	A	C	B	B or C	B																																		
Left Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Left Turn Lane Storage Length: N/A Feet																																								
Additional Findings: N/A																																								
Additional Comments / Justifications:																																								

Figure 1. Warrant for left turn lanes on two-lane roadways
 (speeds to 35 mph, unsignalized and signalized intersections)
 (L = % Left Turns in Advancing Volume)



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: SL Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: PM Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 1px solid red; padding: 2px; display: inline-block;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Right Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	98	0.0%	N/A
	Through	-	65	0.0%	N/A
	Right	Yes	0		N/A
Opposing	Left	Yes	0		N/A
	Through	-	119	0.0%	N/A
	Right	Yes	54	0.0%	N/A
Advancing Volume: N/A Opposing Volume: N/A Left Turn Volume: N/A % Left Turns in Advancing Volume: N/A					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0		0
	Through	-	119	0.0%	119
	Right	-	54	0.0%	54
Advancing Volume: 173 Right Turn Volume: 54					

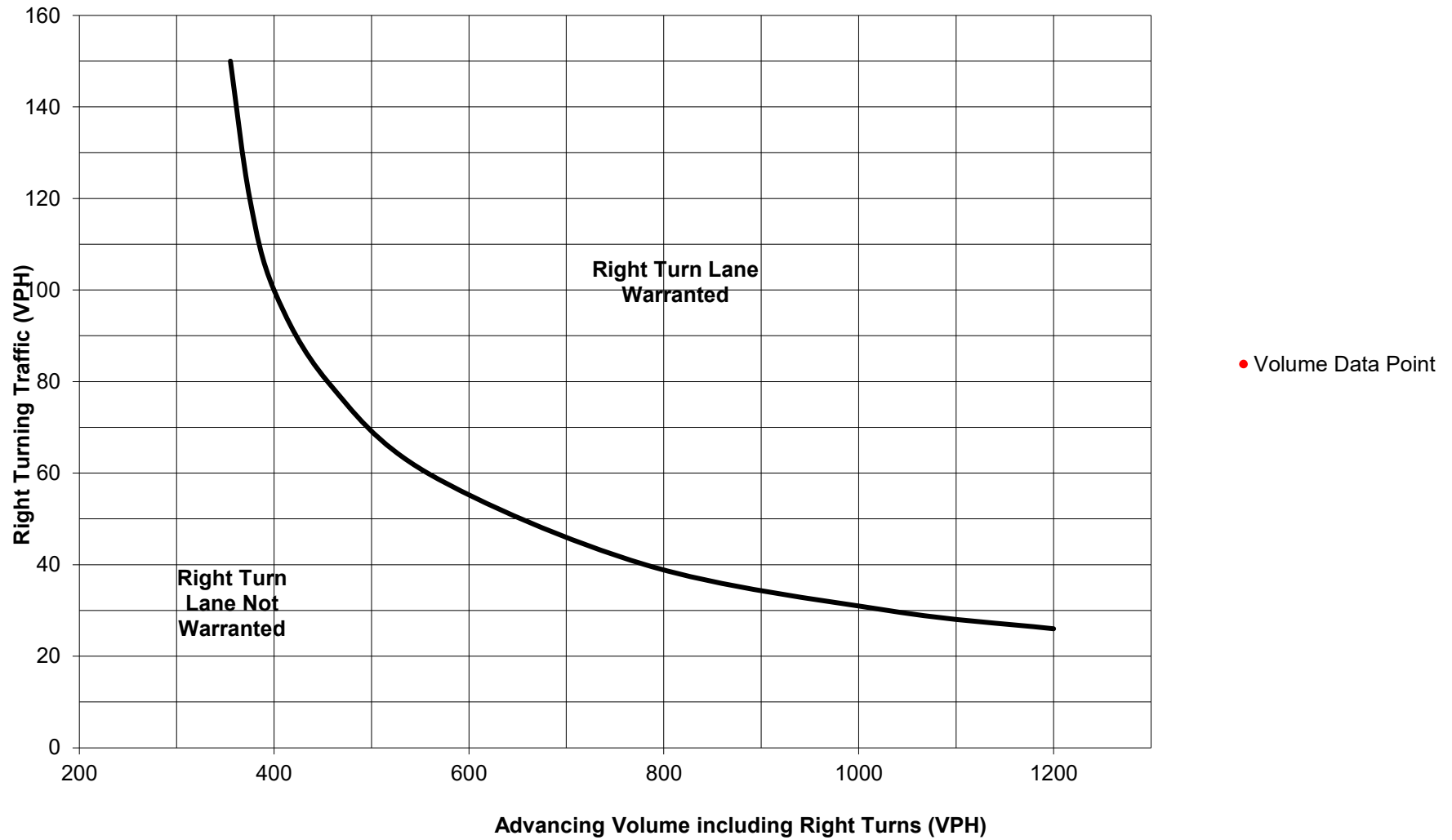
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A Warrant Met?: N/A	Applicable Warrant Figure: Figure 9 Warrant Met?: No

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 54 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known): 	Average # of Vehicles/Cycle: N/A																																								
Penndot Publication 46, Exhibit 11-6																																									
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th rowspan="3">Type of Traffic Control</th> <th colspan="6">Speed (MPH)</th> </tr> <tr> <th colspan="2">25-35</th> <th colspan="2">40-45</th> <th colspan="2">50-60</th> </tr> <tr> <th colspan="6">Turn Demand Volume</th> </tr> <tr> <th></th> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> </tr> </thead> <tbody> <tr> <td>Signalized</td> <td>A</td> <td>A</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> </tr> <tr> <td>Unsignalized</td> <td>A</td> <td>A</td> <td>C</td> <td>B</td> <td>B or C</td> <td>B</td> </tr> </tbody> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume							High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																								
	25-35		40-45		50-60																																				
	Turn Demand Volume																																								
	High	Low	High	Low	High	Low																																			
Signalized	A	A	B or C	B or C	B or C	B or C																																			
Unsignalized	A	A	C	B	B or C	B																																			
Right Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Right Turn Lane Storage Length: N/A Feet																																									
Additional Findings: N/A																																									
Additional Comments / Justifications: <div style="border: 1px solid black; height: 40px; margin-top: 5px;"></div>																																									

**Figure 9. Warrant for right turn lanes on two-lane roadways
(40 mph or lower speeds, unsignalized and signalized intersections)**



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: SL Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: Sat Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 2px solid red; padding: 2px; display: inline-block;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Left Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	77	0.0%	77
	Through	-	63	2.0%	64
	Right	Yes	0		0
Opposing	Left	Yes	0		0
	Through	-	69	0.0%	69
	Right	Yes	39	0.0%	39
Advancing Volume: 141 Opposing Volume: 108 Left Turn Volume: 77 % Left Turns in Advancing Volume: 54.61%					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0		N/A
	Through	-	69	0.0%	N/A
	Right	-	31	0.0%	N/A
Advancing Volume: N/A Right Turn Volume: N/A					

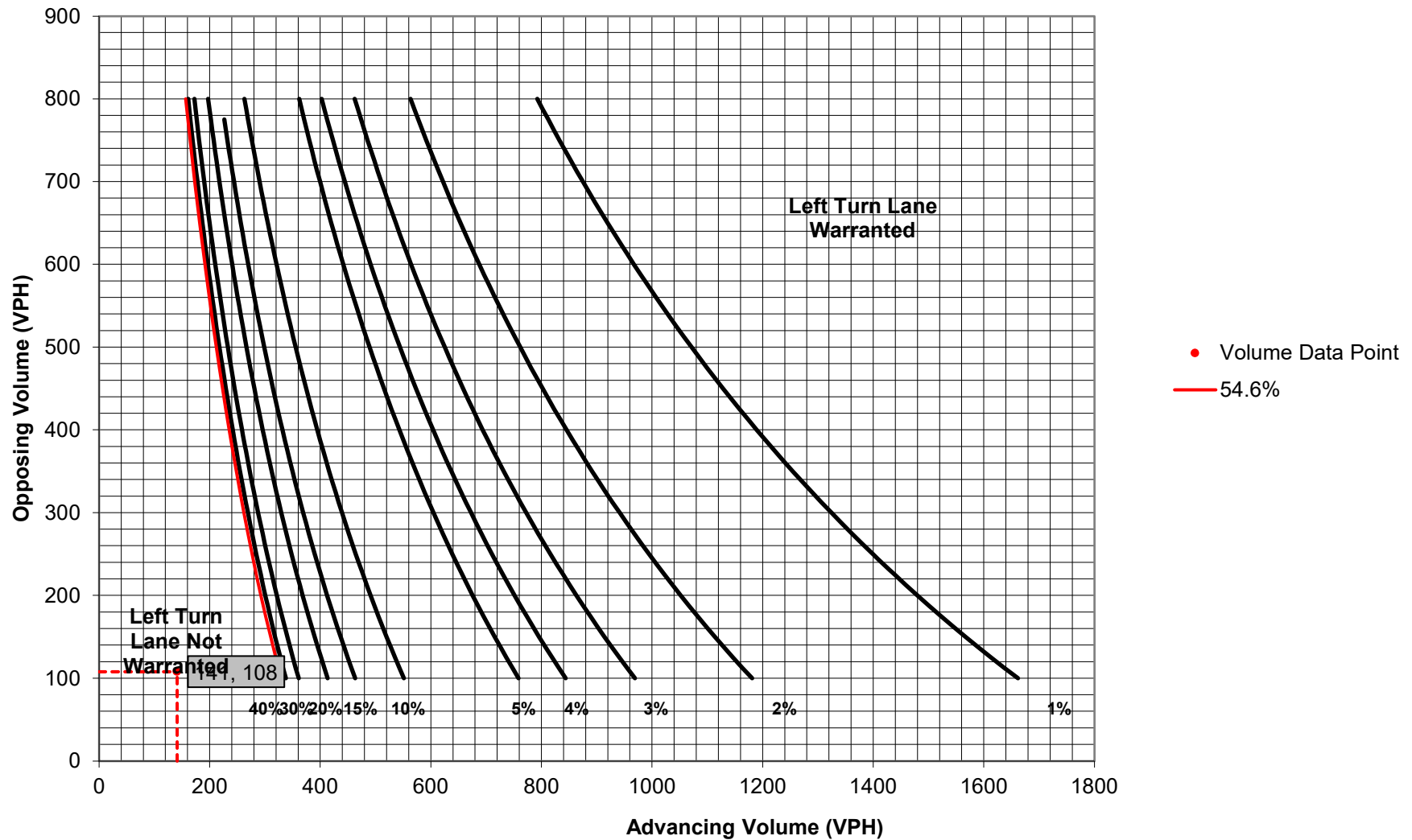
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: Figure 1 Warrant Met?: No	Applicable Warrant Figure: N/A Warrant Met?: N/A

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 77 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known):	Average # of Vehicles/Cycle: N/A																																							
PennDOT Publication 46, Exhibit 11-6																																								
<table border="1" style="width: 100%; border-collapse: collapse; text-align: center;"> <tr> <th rowspan="4">Type of Traffic Control</th> <th colspan="6">Speed (MPH)</th> </tr> <tr> <th colspan="2">25-35</th> <th colspan="2">40-45</th> <th colspan="2">50-60</th> </tr> <tr> <th colspan="6">Turn Demand Volume</th> </tr> <tr> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> </tr> <tr> <td>Signalized</td> <td>A</td> <td>A</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> </tr> <tr> <td>Unsignalized</td> <td>A</td> <td>A</td> <td>C</td> <td>B</td> <td>B or C</td> <td>B</td> </tr> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume						High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																							
	25-35		40-45		50-60																																			
	Turn Demand Volume																																							
	High	Low	High	Low	High	Low																																		
Signalized	A	A	B or C	B or C	B or C	B or C																																		
Unsignalized	A	A	C	B	B or C	B																																		
Left Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Left Turn Lane Storage Length: N/A Feet																																								
Additional Findings: <div style="border: 1px solid black; padding: 2px; display: inline-block;">N/A</div>																																								
Additional Comments / Justifications: <div style="border: 1px solid black; height: 40px; margin-top: 5px;"></div>																																								

Figure 1. Warrant for left turn lanes on two-lane roadways
 (speeds to 35 mph, unsignalized and signalized intersections)
 (L = % Left Turns in Advancing Volume)



Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality: Upper Uwchlan Township County: Chester County PennDOT Engineering District: 6	Analysis Date: 5/28/2025 Conducted By: SL Checked By: Agency/Company Name: Traffic Planning and Design, Inc.
Intersection & Approach Description: Station Boulevard & Site Driveway	
Analysis Period: 2026 Projected (Build) Design Hour: Sat Peak Hour Intersection Control: Unsignalized Posted Speed Limit (MPH): 30 Type of Terrain: Level	Number of Approach Lanes: 1 Undivided or Divided Highway: Undivided <div style="border: 2px solid red; padding: 2px; display: inline-block;">Type of Analysis</div> Left or Right-Turn Lane Analysis?: Right Turn Lane

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	77	0.0%	N/A
	Through	-	63	2.0%	N/A
	Right	Yes	0		N/A
Opposing	Left	Yes	0		N/A
	Through	-	69	0.0%	N/A
	Right	Yes	39	0.0%	N/A
Advancing Volume: N/A Opposing Volume: N/A Left Turn Volume: N/A % Left Turns in Advancing Volume: N/A					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0		0
	Through	-	69	0.0%	69
	Right	-	31	0.0%	31
Advancing Volume: 100 Right Turn Volume: 31					

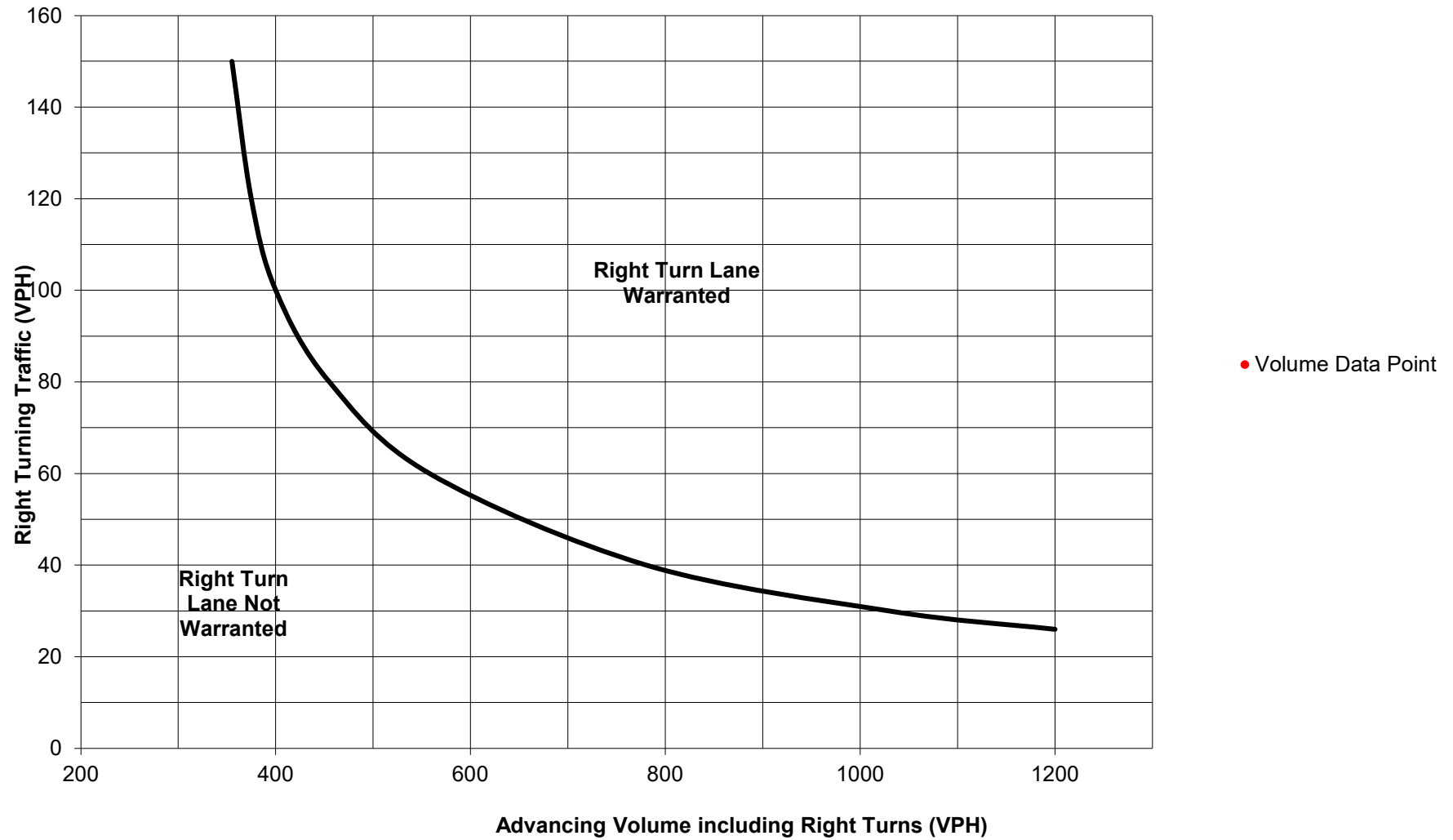
TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A Warrant Met?: N/A	Applicable Warrant Figure: Figure 9 Warrant Met?: No

TURN LANE LENGTH CALCULATIONS

Intersection Control: Unsignalized Design Hour Volume of Turning Lane: 31 Cycles Per Hour (Assumed): 60 Cycles Per Hour (If Known): 	Average # of Vehicles/Cycle: N/A																																							
PennDOT Publication 46, Exhibit 11-6																																								
<table border="1" style="width: 100%; border-collapse: collapse; text-align: center;"> <tr> <th rowspan="4">Type of Traffic Control</th> <th colspan="6">Speed (MPH)</th> </tr> <tr> <th colspan="2">25-35</th> <th colspan="2" rowspan="2">40-45</th> <th colspan="2" rowspan="2">50-60</th> </tr> <tr> <th colspan="6">Turn Demand Volume</th> </tr> <tr> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> <th>High</th> <th>Low</th> </tr> <tr> <td>Signalized</td> <td>A</td> <td>A</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> <td>B or C</td> </tr> <tr> <td>Unsignalized</td> <td>A</td> <td>A</td> <td>C</td> <td>B</td> <td>B or C</td> <td>B</td> </tr> </table>		Type of Traffic Control	Speed (MPH)						25-35		40-45		50-60		Turn Demand Volume						High	Low	High	Low	High	Low	Signalized	A	A	B or C	B or C	B or C	B or C	Unsignalized	A	A	C	B	B or C	B
Type of Traffic Control	Speed (MPH)																																							
	25-35		40-45		50-60																																			
	Turn Demand Volume																																							
	High	Low	High	Low	High	Low																																		
Signalized	A	A	B or C	B or C	B or C	B or C																																		
Unsignalized	A	A	C	B	B or C	B																																		
Right Turn Lane Storage Length, Condition A: N/A Feet Condition B: N/A Feet Condition C: N/A Feet Required Right Turn Lane Storage Length: N/A Feet																																								
Additional Findings: N/A																																								
Additional Comments / Justifications:																																								

**Figure 9. Warrant for right turn lanes on two-lane roadways
(40 mph or lower speeds, unsignalized and signalized intersections)**





1600 Manor Drive, Suite 200
Chalfont, PA 18914

General Project Description & Stormwater Management Calculations

Project: **Final Plan for Lot 2B of Parcel 5C**
Graphite Mine Road & Station Boulevard
Upper Uwchlan Township
Chester County, PA

Client: **Prosperity Property Investments, LLC**
1306 Bellona Avenue
Lutherville-Timonium, MD 21093

Project
Number: **PC211160**

Date: **March 25, 2019**

Last
Revised: **May 30, 2025**

Plans Referenced:

- "Preliminary / Final PRD Plans (Final Plan for Lot 2B of Parcel 5C) for Prosperity Property Investments, LLC" by Bohler, dated 03/15/2022, Revision 8, dated 05/30/25.

Professional
Engineer: Chris Puzinas
PA License #PE083200



Table of Contents

General Project Description/ Stormwater Management.....	1
General Project Description	1
Stormwater Management.....	1
Storm Drainage	2
Conclusions.....	2
USGS Location Map	3
Storm Drainage	4
NOAA Atlas 14 Rainfall Data (used in StormCAD).....	5
Inlet Runoff Calculations C Worksheets	6
100-Year Design Reports.....	7

General Project Description/Stormwater Management

GENERAL PROJECT DESCRIPTION

Byers Retail Acquisition, L.P. proposes to develop Lot #2B of Parcel 5C of the existing 29.79-acre tract at Graphite Mine Road & Station Boulevard known as parcel 5C. The existing 29.79-acre tract was subdivided into two lots – Lot #1 and Lot #2. Lot #1, which consists of 16.35 acres, contains 121 unit townhomes and was previously approved by the township and currently under construction.

Lot #2, the subject of this application, consists of 13.44 acres, and will be subdivided further into Lots #2A and #2B. Lot #2A will contain 55 Townhome units, bringing the total number of residential units for Parcel 5C to 176 residential units.

Lot #2B will be developed into 31,500 square feet of commercial space consisting of a daycare and retail space. The proposal includes the installation of parking, utilities, landscaping and stormwater management controls necessary to support the development. Pertinent data characterizing the existing and future site conditions are shown on the accompanying plans.

STORMWATER MANAGEMENT

Pre-Development

The pre-development condition of Lot #2 consists of one Point of Interest (POI#2) which is indicated on the Pre-Development Drainage Area Plan. POI #2 is located across Graphite Mine Road near the southeast corner of the site at endwalls which discharge into Pickering Creek, which is part of the Delaware River Basin. The on-site area tributary to the point of interest has been delineated. The pre-development conditions were analyzed based on existing conditions for all areas tributary to the points of interest. There are currently two (2) existing sediment basins located on the site. Since these sediment basins were meant to only be on the site temporarily, they were disregarded in the pre-development stormwater analysis.

The development of Lot #2B does not have any impact to the pre-development conditions.

Post-Development

The post-development condition of the site maintains the same Point of Interest and relative drainage patterns. There is an increase in the total impervious surface coverage with a subsequent increase in runoff rates and volume. Therefore, above ground stormwater management basins (AGBs) have been designed to reduce peak rates of runoff and runoff volume. The area tributary to each basin has been delineated and hydrographs have been generated for the 1, 2, 5, 10, 25, 50 and 100 year storms. The inflow hydrographs to each basin have been routed and peak flow rate from each routed basin has been added to the hydrographs generated for any bypass areas to determine the total post-development discharge rate.

The development of Lot #2B does not alter any of the stormwater design for POI #1. The previously approved plan for Lot #2B had allocated 103,038 square feet of impervious coverage, while the current plan proposes only 101,100 square feet of impervious coverage (difference of 1,938 square feet). Since Bioretention Basin 2 was previously designed to manage the entire drainage area, including Lot #2B, the reduction of impervious area results in a reduced volume of runoff getting to the basin. As such, no adjustments are necessary for Bioretention Basin 2. As such, calculations are only provided for conveyance of stormwater to ensure the proposed stormwater conveyance pipes and inlets have the required design capacity.

Requirements

The following requirements are from Chapter 152 "Stormwater Management" of the Upper Uwchlan Ordinances.

§152-311. Conveyance and System Design Standards

- §152-311.F(1) Storm drain system(s), where provided, shall be designed to carry a 25-year peak flow rate, and a 50-year peak flow rate at the sump area.
- §152-311.F(3)(a) All inlets must be designed to accommodate the 25-year peak flow rate.
- §152-311.G(1) All pipe within the paved cartway shall be reinforced concrete or high-density polyethylene pipe.
- §152-311.G(2) All pipe outside of the paved cartway can consist of interior polyethylene pipe.
- §152-311.G(4) Minimum pipe size is 15 inches and have a minimum pipe slope of 0.50%.
- §152-311.G(9) Minimum pipe cover of 18 inches.
- §152-311.J(2) A fence or suitable vegetation shall be provided around all detention basins upon recommendation of the Township Engineer. All fencing shall be at least 3'-6" in height.

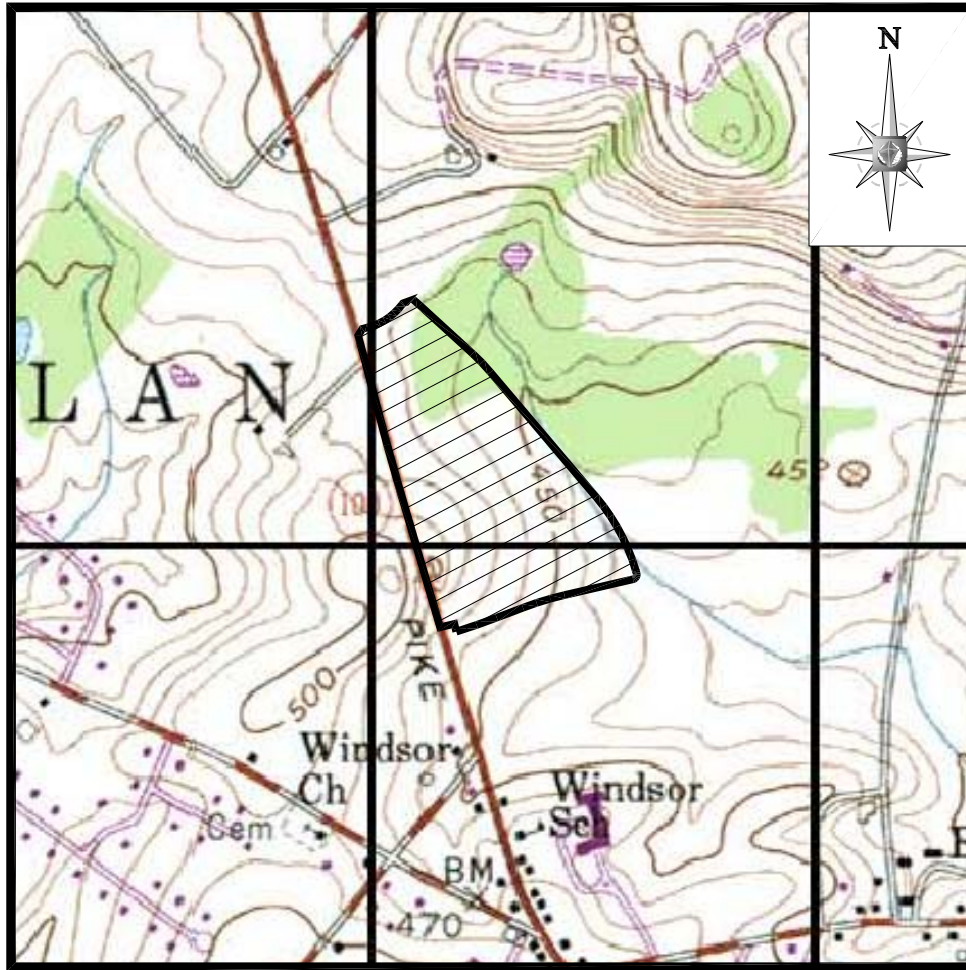
STORM DRAINAGE

The storm drainage system has been designed to intercept runoff at topographic low points and areas of significant runoff quantities and convey stormwater to the stormwater management basins. StormCAD Version V8i has been utilized for designing the storm conveyance system. Conveyance design precipitation amounts are based on NOAA Atlas 14, Volume 2, Version 3 obtained on April 28, 2016 for Chester Springs, PA for the 100-year storm event.

CONCLUSIONS

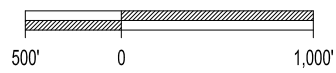
The proposed stormwater management program described above has been designed to comply with the Upper Uwchlan Township Subdivision and Land Development and Stormwater Management Ordinances, Chapter 102 CG-1 Guidelines, and the Stormwater Management Act of 1978.

R:\21\PC211160\Technical\Reports\SWM\Rev-1\PC211160_SWM Report-1.docx



USGS MAP
(DOWNINGTOWN QUAD)

1"=1,000'



Storm Drainage

NOAA Atlas 14, Volume 2, Version 3
Location name: Chester Springs, Pennsylvania,
USA*

Latitude: 40.0831°, Longitude: -75.6887°

Elevation: m/ft**

* source: ESRI Maps

** source: USGS



POINT PRECIPITATION FREQUENCY ESTIMATES

G.M. Bonnin, D. Martin, B. Lin, T. Parzybok, M. Yekta, and D. Riley

NOAA, National Weather Service, Silver Spring, Maryland

[PF tabular](#) | [PF graphical](#) | [Maps & aerals](#)

PF tabular

PDS-based point precipitation frequency estimates with 90% confidence intervals (in inches)¹										
Duration	Average recurrence interval (years)									
	1	2	5	10	25	50	100	200	500	1000
5-min	0.347 (0.317-0.381)	0.413 (0.377-0.454)	0.482 (0.439-0.529)	0.531 (0.483-0.581)	0.585 (0.530-0.641)	0.622 (0.560-0.681)	0.657 (0.589-0.720)	0.687 (0.614-0.754)	0.720 (0.638-0.790)	0.743 (0.654-0.819)
10-min	0.555 (0.506-0.608)	0.661 (0.604-0.726)	0.772 (0.704-0.847)	0.848 (0.772-0.929)	0.933 (0.845-1.02)	0.990 (0.892-1.08)	1.04 (0.936-1.14)	1.09 (0.973-1.20)	1.14 (1.01-1.25)	1.17 (1.03-1.29)
15-min	0.693 (0.633-0.761)	0.830 (0.759-0.912)	0.976 (0.890-1.07)	1.07 (0.977-1.18)	1.18 (1.07-1.30)	1.25 (1.13-1.37)	1.32 (1.18-1.45)	1.37 (1.23-1.51)	1.43 (1.27-1.57)	1.47 (1.29-1.62)
30-min	0.950 (0.868-1.04)	1.15 (1.05-1.26)	1.39 (1.26-1.52)	1.56 (1.42-1.70)	1.75 (1.59-1.92)	1.89 (1.70-2.07)	2.02 (1.81-2.22)	2.14 (1.91-2.35)	2.28 (2.02-2.50)	2.38 (2.10-2.62)
60-min	1.18 (1.08-1.30)	1.44 (1.32-1.58)	1.78 (1.62-1.95)	2.02 (1.84-2.22)	2.33 (2.11-2.55)	2.56 (2.30-2.80)	2.79 (2.50-3.05)	3.00 (2.68-3.29)	3.27 (2.90-3.59)	3.47 (3.06-3.82)
2-hr	1.41 (1.28-1.57)	1.72 (1.55-1.90)	2.13 (1.92-2.36)	2.45 (2.20-2.72)	2.87 (2.56-3.17)	3.20 (2.84-3.54)	3.52 (3.12-3.90)	3.86 (3.38-4.26)	4.30 (3.73-4.76)	4.64 (3.99-5.14)
3-hr	1.54 (1.39-1.71)	1.86 (1.68-2.07)	2.32 (2.09-2.58)	2.67 (2.40-2.96)	3.13 (2.80-3.46)	3.48 (3.10-3.85)	3.85 (3.40-4.25)	4.21 (3.69-4.66)	4.69 (4.07-5.20)	5.06 (4.35-5.62)
6-hr	1.90 (1.72-2.12)	2.30 (2.08-2.56)	2.86 (2.58-3.18)	3.30 (2.97-3.67)	3.92 (3.50-4.34)	4.42 (3.92-4.88)	4.94 (4.34-5.45)	5.48 (4.77-6.05)	6.22 (5.35-6.88)	6.82 (5.78-7.55)
12-hr	2.31 (2.08-2.59)	2.78 (2.52-3.13)	3.48 (3.14-3.89)	4.05 (3.63-4.53)	4.87 (4.33-5.42)	5.56 (4.89-6.17)	6.30 (5.48-6.99)	7.09 (6.10-7.86)	8.23 (6.94-9.13)	9.16 (7.61-10.2)
24-hr	2.69 (2.47-2.96)	3.24 (2.97-3.56)	4.06 (3.71-4.45)	4.74 (4.32-5.19)	5.72 (5.19-6.25)	6.54 (5.91-7.14)	7.42 (6.67-8.09)	8.37 (7.48-9.12)	9.75 (8.61-10.6)	10.9 (9.54-11.9)
2-day	3.12 (2.85-3.45)	3.77 (3.44-4.16)	4.73 (4.30-5.21)	5.50 (5.00-6.07)	6.60 (5.97-7.26)	7.51 (6.76-8.26)	8.47 (7.58-9.31)	9.50 (8.45-10.4)	11.0 (9.66-12.0)	12.1 (10.6-13.4)
3-day	3.29 (3.00-3.64)	3.97 (3.62-4.39)	4.97 (4.52-5.49)	5.77 (5.24-6.37)	6.92 (6.25-7.62)	7.86 (7.07-8.65)	8.85 (7.93-9.74)	9.91 (8.82-10.9)	11.4 (10.1-12.6)	12.6 (11.1-13.9)
4-day	3.46 (3.16-3.83)	4.17 (3.80-4.62)	5.21 (4.74-5.76)	6.05 (5.49-6.68)	7.23 (6.54-7.98)	8.21 (7.38-9.04)	9.24 (8.27-10.2)	10.3 (9.20-11.4)	11.9 (10.5-13.1)	13.1 (11.5-14.5)
7-day	4.06 (3.72-4.44)	4.86 (4.46-5.32)	6.00 (5.50-6.57)	6.94 (6.34-7.59)	8.28 (7.54-9.04)	9.38 (8.50-10.2)	10.5 (9.51-11.5)	11.8 (10.6-12.8)	13.5 (12.1-14.8)	15.0 (13.2-16.4)
10-day	4.62 (4.27-5.03)	5.52 (5.09-6.01)	6.73 (6.20-7.31)	7.70 (7.07-8.35)	9.04 (8.28-9.80)	10.1 (9.24-11.0)	11.2 (10.2-12.2)	12.4 (11.2-13.4)	14.0 (12.6-15.2)	15.3 (13.7-16.6)
20-day	6.25 (5.81-6.73)	7.41 (6.90-7.99)	8.84 (8.21-9.52)	9.96 (9.24-10.7)	11.5 (10.6-12.3)	12.6 (11.7-13.6)	13.8 (12.7-14.8)	15.0 (13.8-16.1)	16.6 (15.2-17.9)	17.8 (16.2-19.2)
30-day	7.77 (7.30-8.28)	9.16 (8.61-9.76)	10.7 (10.0-11.4)	11.8 (11.1-12.6)	13.4 (12.5-14.2)	14.5 (13.6-15.5)	15.7 (14.6-16.7)	16.8 (15.6-17.9)	18.3 (16.9-19.5)	19.3 (17.8-20.7)
45-day	9.86 (9.33-10.5)	11.6 (11.0-12.3)	13.3 (12.6-14.1)	14.6 (13.8-15.5)	16.2 (15.3-17.2)	17.5 (16.5-18.5)	18.6 (17.5-19.7)	19.7 (18.5-20.8)	21.0 (19.7-22.3)	22.0 (20.5-23.3)
60-day	11.8 (11.2-12.5)	13.8 (13.1-14.6)	15.8 (15.0-16.7)	17.2 (16.4-18.2)	19.1 (18.1-20.1)	20.4 (19.3-21.5)	21.6 (20.4-22.8)	22.7 (21.4-24.0)	24.1 (22.7-25.5)	25.1 (23.6-26.6)

¹ Precipitation frequency (PF) estimates in this table are based on frequency analysis of partial duration series (PDS). Numbers in parenthesis are PF estimates at lower and upper bounds of the 90% confidence interval. The probability that precipitation frequency estimates (for a given duration and average recurrence interval) will be greater than the upper bound (or less than the lower bound) is 5%. Estimates at upper bounds are not checked against probable maximum precipitation (PMP) estimates and may be higher than currently valid PMP values. Please refer to NOAA Atlas 14 document for more information.

[Back to Top](#)

PF graphical

Peak Discharge Calculations

Project: Byers Station

Description: Conveyance

Runoff Coefficients

Impervious	0.99
Lawn	0.25

PennDOT Region 5

Conversion Factor, k^* 1.0083

* $Q = k \text{ CIA}$

Catchment Areas

Name	Tc (min.)	Groundcover Areas (Ac.)					Total Area (Ac)	C	Q (cfs)
		Impervious	Lawn						100-year
RD01	5.00	0.05	0.00				0.05	0.99	0.44
RD02	5.00	0.05	0.00				0.05	0.99	0.41
RD03	5.00	0.07	0.00				0.07	0.99	0.55
RD04	5.00	0.07	0.00				0.07	0.99	0.58
RD05	5.00	0.03	0.00				0.03	0.99	0.21
RD06	5.00	0.01	0.00				0.01	0.99	0.06
RD07	6.00	0.00	0.00				0.00	0.99	0.03
RD08	7.00	0.01	0.00				0.01	0.99	0.07
RD09	8.00	0.02	0.00				0.02	0.99	0.12
RD10	9.00	0.02	0.00				0.02	0.99	0.16
RD11	10.00	0.02	0.00				0.02	0.99	0.16
RD12	11.00	0.03	0.00				0.03	0.99	0.21
RD13	12.00	0.01	0.00				0.01	0.99	0.08
RD14	13.00	0.01	0.00				0.01	0.99	0.09
RD15	14.00	0.02	0.00				0.02	0.99	0.10
RD16	15.00	0.03	0.00				0.03	0.99	0.19
RD17	16.00	0.03	0.00				0.03	0.99	0.18
RD18	17.00	0.05	0.00				0.05	0.99	0.31
RD19	18.00	0.05	0.00				0.05	0.99	0.29
RD20	19.00	0.07	0.00				0.07	0.99	0.38
RD21	20.00	0.07	0.00				0.07	0.99	0.39
CB01	5.00	0.11	0.01				0.11	0.99	0.95
IN40A	5.00	0.12	0.02				0.15	0.87	1.07
IN43	5.00	0.08	0.01				0.09	0.87	0.66
IN204	5.00	0.00	0.16				0.16	0.25	0.33
IN215	5.00	0.03	0.00				0.04	0.95	0.29
IN216	5.00	0.01	0.12				0.13	0.29	0.31
IN304	5.00	0.00	0.01				0.01	0.25	0.02
IN304A	5.00	0.00	0.01				0.01	0.25	0.02
IN504	5.00	0.15	0.05				0.20	0.82	1.36
IN505	5.00	0.00	0.05				0.05	0.25	0.11
IN506	5.00	0.09	0.01				0.10	0.91	0.74
IN507	5.00	0.08	0.01				0.10	0.89	0.73
IN508	5.00	0.10	0.00				0.11	0.96	0.87
IN509	5.00	0.07	0.00				0.07	0.97	0.59
IN510	5.00	0.10	0.01				0.11	0.94	0.86
IN511	5.00	0.00	0.07				0.07	0.27	0.16
IN512	5.00	0.19	0.05				0.23	0.84	1.62
IN513	5.00	0.09	0.02				0.11	0.88	0.79
IN514	5.00	0.22	0.01				0.23	0.97	1.87
YD515	5.00	0.00	0.01				0.01	0.55	0.06
YD514	5.00	0.01	0.01				0.02	0.55	0.11
YD513	5.00	0.01	0.02				0.03	0.56	0.13
YD512	5.00	0.01	0.01				0.02	0.65	0.12
IN516	5.00	0.00	0.65				0.65	0.25	1.35
IN517	5.00	0.02	0.31				0.33	0.30	0.83
IN518	5.00	0.00	0.20				0.20	0.26	0.44
IN519	5.00	0.01	0.12				0.13	0.31	0.34
IN520	5.00	0.01	0.07				0.07	0.33	0.21
IN521	5.00	0.02	0.11				0.13	0.34	0.37
IN522	5.00	0.05	0.02				0.07	0.79	0.43
IN520+(RD01+RD02+RD03+RD04)	5.00	0.25	0.07				0.32	0.83	2.18
IN519+(RD05+RD06+RD07+RD08+RD17)	5.00	0.09	0.12				0.21	0.56	0.97
IN518+(RD09+RD10+RD11+RD12)	5.00	0.09	0.20				0.29	0.49	1.19
IN517+(RD13+RD14+RD15+RD16)	5.00	0.09	0.31				0.41	0.42	1.42
YD515+(RD18)	6.00	0.06	0.01				0.07	0.91	0.48
YD514+(RD19+RD20)	7.00	0.13	0.01				0.14	0.92	1.02
YD513+(RD21)	5.00	0.08	0.01				0.08	0.93	0.64

The provided calculations demonstrate the inlets have capacity to handle the flow draining to them. None of the inlets receive a flow that exceeds 4 cfs in the 100 year storm event. Please refer to plans entitled "Final Plan for Lot 2B of Parcel 5C", dated 03/15/2022, last revised, 05/30/2025, for the associated inlet drainage areas.

Scenario: 100-Year
Current Time Step: 0.000Hr
Catch Basin FlexTable: Gutter Spread

Label	Inlet	Inlet Location	Ground Elevation (ft)	Rim Elevation (ft)	Local Surface Flow (ft ³ /s)	Carryover Rational Flow (ft ³ /s)	Total Surface Flow to Inlet (ft ³ /s)	Capture Efficiency (Calculated) (%)	Intercepted Rational Flow (ft ³ /s)	Bypassed Rational Flow (ft ³ /s)	Bypass Target	Gutter Depth (in)	Gutter Spread (ft)	Cross Slope (ft/ft)	Longitudinal Slope (ft/ft)
EX.IN216	PADOT Type 'M'	In Sag	437.00	437.00	0.30	0.00	0.30	100.0	0.30	0.00	(N/A)	1.0	4.2	0.0200	(N/A)
HW01	PADOT Type 'C'	In Sag	437.00	437.00	1.99	0.00	1.99	100.0	1.99	0.00	(N/A)	2.2	9.0	0.0200	(N/A)
HW02	PADOT Type 'C'	In Sag	437.00	437.00	2.44	0.00	2.44	100.0	2.44	0.00	(N/A)	2.5	10.3	0.0200	(N/A)
IN35	PADOT Type 'C'	On Grade	445.11	445.11	0.72	0.00	0.72	82.5	0.59	0.13	IN38	1.2	5.2	0.0200	0.0200
IN36	PADOT Type 'C'	On Grade	445.08	445.08	0.39	0.00	0.39	90.0	0.35	0.04	IN37	1.0	4.1	0.0200	0.0200
IN37	PADOT Type 'C'	On Grade	443.13	443.13	0.22	0.04	0.26	94.3	0.24	0.01	IN39	0.8	3.5	0.0200	0.0200
IN38	PADOT Type 'C'	On Grade	443.50	443.50	0.27	0.13	0.40	89.9	0.36	0.04	IN40	1.0	4.2	0.0200	0.0200
IN39	PADOT Type 'C'	On Grade	439.56	439.56	1.02	0.80	1.82	68.5	1.25	0.57	IN41	1.8	7.3	0.0200	0.0200
IN40	PADOT Type 'C'	On Grade	441.27	441.27	0.51	0.04	0.56	85.9	0.48	0.08	IN40A	1.1	4.7	0.0200	0.0200
IN40A	PADOT Type 'C'	On Grade	439.52	439.52	1.04	0.08	1.11	76.1	0.85	0.27	IN42	1.5	6.1	0.0200	0.0200
IN41	PADOT Type 'C'	On Grade	436.41	436.41	0.39	0.57	0.97	78.2	0.76	0.21	IN44A	1.4	5.8	0.0200	0.0200
IN42	PADOT Type 'C'	On Grade	436.40	436.40	0.57	0.27	0.84	80.3	0.67	0.17	IN43	1.3	5.5	0.0200	0.0200
IN43	PADOT Type 'C'	On Grade	434.78	434.78	0.62	0.17	0.79	81.2	0.64	0.15	IN46	1.3	5.4	0.0200	0.0200
IN44	PADOT Type 'C'	On Grade	433.57	433.57	0.17	0.70	0.87	79.7	0.69	0.18	IN45	1.3	5.6	0.0200	0.0200
IN44A	PADOT Type 'C'	On Grade	434.47	434.47	1.15	0.93	2.08	66.5	1.38	0.70	IN44	1.8	7.7	0.0200	0.0200
IN45	PADOT Type 'C'	On Grade	432.24	432.24	0.16	0.18	0.33	91.8	0.31	0.03	(N/A)	0.9	3.9	0.0200	0.0200
IN46	PADOT Type 'C'	On Grade	432.40	432.40	0.39	0.15	0.54	86.2	0.47	0.07	(N/A)	1.1	4.7	0.0200	0.0200
IN49	PADOT Type 'M'	In Sag	451.25	451.25	0.89	0.00	0.89	100.0	0.89	0.00	(N/A)	1.7	7.1	0.0200	(N/A)
IN204	PADOT Type 'M'	In Sag	443.50	443.50	0.32	0.00	0.32	100.0	0.32	0.00	(N/A)	1.0	4.3	0.0200	(N/A)
IN215	PADOT Type 'C'	In Sag	435.97	435.97	0.30	0.00	0.30	100.0	0.30	0.00	(N/A)	0.5	2.1	0.0200	(N/A)
IN304	PADOT Type 'C'	On Grade	442.69	442.69	0.02	0.00	0.02	100.0	0.02	0.00	IN40	0.3	1.3	0.0200	0.0200
IN304A	PADOT Type 'C'	In Sag	442.25	442.25	0.02	0.00	0.02	100.0	0.02	0.00	(N/A)	0.0	0.1	0.0200	(N/A)
IN305	PADOT Type 'C'	On Grade	442.50	442.50	0.28	0.27	0.55	86.0	0.47	0.08	IN39	1.1	4.7	0.0200	0.0200
IN306	PADOT Type 'C'	On Grade	443.09	443.09	2.09	0.00	2.09	66.4	1.39	0.70	IN39	1.9	7.7	0.0200	0.0200
IN307	PADOT Type 'C'	On Grade	443.26	443.26	1.02	0.11	1.13	75.9	0.86	0.27	IN305	1.5	6.1	0.0200	0.0200
IN308	PADOT Type 'C'	On Grade	444.58	444.58	0.65	0.00	0.65	83.8	0.55	0.11	IN307	1.2	5.0	0.0200	0.0200
IN309	PADOT Type 'C'	On Grade	444.18	444.18	1.24	0.00	1.24	74.4	0.93	0.32	IN314	1.5	6.4	0.0200	0.0200
IN310	PADOT Type 'C'	On Grade	443.90	443.90	1.09	0.00	1.09	76.4	0.83	0.26	IN311	1.5	6.1	0.0200	0.0200
IN311	PADOT Type 'C'	On Grade	442.53	442.53	1.05	0.26	1.31	73.7	0.96	0.34	IN313	1.6	6.5	0.0200	0.0200
IN313	PADOT Type 'C'	In Sag	440.56	440.56	0.73	0.34	1.08	100.0	1.08	0.00	(N/A)	1.4	5.9	0.0200	(N/A)
IN314	PADOT Type 'C'	In Sag	440.56	440.56	3.54	0.32	3.85	100.0	3.85	0.00	(N/A)	3.4	14.0	0.0200	(N/A)
IN315	PADOT Type 'C'	In Sag	442.32	442.32	1.42	0.00	1.42	100.0	1.42	0.00	(N/A)	1.7	7.2	0.0200	(N/A)
IN316	PADOT Type 'C'	On Grade	439.22	439.22	1.54	0.00	1.54	71.2	1.09	0.44	IN318	1.7	6.9	0.0200	0.0200
IN317	PADOT Type 'C'	On Grade	439.12	439.12	1.56	0.00	1.56	70.9	1.11	0.45	IN319	1.7	6.9	0.0200	0.0200
IN318	PADOT Type 'C'	On Grade	436.94	436.94	0.76	0.44	1.21	74.9	0.90	0.30	IN44A	1.5	6.3	0.0200	0.0200
IN319	PADOT Type 'C'	On Grade	436.83	436.83	1.29	0.45	1.74	69.2	1.21	0.54	IN320	1.7	7.2	0.0200	0.0200
IN320	PADOT Type 'C'	On Grade	436.23	436.23	0.94	0.54	1.47	71.8	1.06	0.41	IN44A	1.6	6.8	0.0200	0.0200
IN322	PADOT Type 'C'	In Sag	439.00	439.00	1.47	0.00	1.47	100.0	1.47	0.00	(N/A)	1.8	7.3	0.0200	(N/A)
IN504	PADOT Type 'C'	In Sag	444.16	444.16	1.30	0.00	1.30	100.0	1.30	0.00	(N/A)	1.6	6.8	0.0200	(N/A)
IN505	PADOT Type 'C'	In Sag	442.08	442.08	0.10	0.00	0.10	100.0	0.10	0.00	(N/A)	0.0	0.1	0.0200	(N/A)
IN506	PADOT Type 'C'	In Sag	443.04	443.04	0.72	0.00	0.72	100.0	0.72	0.00	(N/A)	1.1	4.4	0.0200	(N/A)
IN507	PADOT Type 'C'	In Sag	444.96	444.96	0.71	0.00	0.71	100.0	0.71	0.00	(N/A)	1.0	4.4	0.0200	(N/A)
IN508	PADOT Type 'C'	In Sag	444.86	444.86	0.84	0.00	0.84	100.0	0.84	0.00	(N/A)	1.2	5.0	0.0200	(N/A)
IN509	PADOT Type 'C'	In Sag	444.00	444.00	0.54	0.00	0.54	100.0	0.54	0.00	(N/A)	0.9	3.6	0.0200	(N/A)
IN510	PADOT Type 'C'	In Sag	442.33	442.33	0.82	0.00	0.82	100.0	0.82	0.00	(N/A)	1.2	4.9	0.0200	(N/A)
IN511	PADOT Type 'M'	In Sag	440.81	440.81	1.02	0.00	1.02	100.0	1.02	0.00	(N/A)	1.8	7.6	0.0200	(N/A)
IN512	PADOT Type 'M'	In Sag	441.50	441.50	1.53	0.00	1.53	100.0	1.53	0.00	(N/A)	2.3	9.5	0.0200	(N/A)
IN513	PADOT Type 'C'	On Grade	441.00	441.00	0.77	0.00	0.77	81.5	0.63	0.14	IN514	1.3	5.3	0.0200	0.0200
IN514	PADOT Type 'C'	In Sag	437.85	437.85	1.77	0.14	1.91	100.0	1.91	0.00	(N/A)	2.1	8.8	0.0200	(N/A)
IN516	PADOT Type 'C'	In Sag	445.60	445.60	1.29	0.00	1.29	100.0	1.29	0.00	(N/A)	1.6	6.7	0.0200	(N/A)
IN517	PADOT Type 'M'	In Sag	443.00	443.00	0.79	0.00	0.79	100.0	0.79	0.00	(N/A)	1.6	6.6	0.0200	(N/A)
IN518	PADOT Type 'M'	In Sag	441.85	441.85	0.41	0.00	0.41	100.0	0.41	0.00	(N/A)	1.2	4.8	0.0200	(N/A)
IN519	PADOT Type 'M'	In Sag	441.50	441.50	0.32	0.00	0.32	100.0	0.32	0.00	(N/A)	1.0	4.3	0.0200	(N/A)
IN520	PADOT Type 'M'	In Sag	441.00	441.00	0.18	0.00	0.18	100.0	0.18	0.00	(N/A)	0.8	3.4	0.0200	(N/A)
IN521	PADOT Type 'M'	In Sag	435.60	435.60	0.35	0.00	0.35	100.0	0.35	0.00	(N/A)	1.1	4.5	0.0200	(N/A)
IN522	PADOT Type 'C'	In Sag	437.32	437.32	0.44	0.00	0.44	100.0	0.44	0.00	(N/A)	0.7	3.0	0.0200	(N/A)
YD512	Grate 18 Nyloplast	In Sag	445.70	445.70	0.10	0.00	0.10	100.0	0.10	0.00	(N/A)	46.5	193.6	0.0200	(N/A)
YD513	PADOT Type 'C'	In Sag	446.20	446.20	0.13	0.00	0.13	100.0	0.13	0.00	(N/A)	0.0	0.1	0.0200	(N/A)
YD514	PADOT Type 'C'	In Sag	446.20	446.20	0.09	0.00	0.09	100.0	0.09	0.00	(N/A)	0.0	0.1	0.0200	(N/A)
YD515	PADOT Type 'M'	In Sag	446.20	446.20	0.04	0.00	0.04	100.0	0.04	0.00	(N/A)	0.5	2.1	0.0200	(N/A)

R:\21\PC211160\Technical\Stormwater & E&S\Rev 5 - Retail Addition\StormCAD\Byers Station_Storm-6-new.stc

Scenario: 100-Year
Current Time Step: 0.000Hr
Catch Basin FlexTable: Inlet Report

Label	Inlet	Inlet Location	Ground Elevation (ft)	Rim Elevation (ft)	Invert Out (ft)	Inlet DA (acres)	Inlet C	Inlet Intensity (in/h)	Inlet Tc (min)
EX.IN216	PADOT Type 'M'	In Sag	437.00	437.00	433.75	0.130	0.290	7.880	5.000
HW01	PADOT Type 'C'	In Sag	437.00	437.00	432.47	0.610	0.410	7.880	5.000
HW02	PADOT Type 'C'	In Sag	437.00	437.00	431.76	0.640	0.480	7.880	5.000
IN35	PADOT Type 'C'	On Grade	445.11	445.11	441.46	0.210	0.430	7.880	5.000
IN36	PADOT Type 'C'	On Grade	445.08	445.08	440.84	0.080	0.620	7.880	5.000
IN37	PADOT Type 'C'	On Grade	443.13	443.13	438.83	0.050	0.550	7.880	5.000
IN38	PADOT Type 'C'	On Grade	443.50	443.50	439.87	0.050	0.690	7.880	5.000
IN39	PADOT Type 'C'	On Grade	439.56	439.56	431.31	0.150	0.860	7.880	5.000
IN40	PADOT Type 'C'	On Grade	441.27	441.27	437.07	0.080	0.810	7.880	5.000
IN40A	PADOT Type 'C'	On Grade	439.52	439.52	436.92	0.150	0.870	7.880	5.000
IN41	PADOT Type 'C'	On Grade	436.41	436.41	430.53	0.050	0.990	7.880	5.000
IN42	PADOT Type 'C'	On Grade	436.40	436.40	432.00	0.080	0.900	7.880	5.000
IN43	PADOT Type 'C'	On Grade	434.78	434.78	431.66	0.090	0.870	7.880	5.000
IN44	PADOT Type 'C'	On Grade	433.57	433.57	428.90	0.050	0.440	7.880	5.000
IN44A	PADOT Type 'C'	On Grade	434.47	434.47	429.32	0.210	0.690	7.880	5.000
IN45	PADOT Type 'C'	On Grade	432.24	432.24	427.39	0.020	0.990	7.880	5.000
IN46	PADOT Type 'C'	On Grade	432.40	432.40	429.00	0.080	0.620	7.880	5.000
IN49	PADOT Type 'M'	In Sag	451.25	451.25	447.03	0.450	0.250	7.880	5.000
IN204	PADOT Type 'M'	In Sag	443.50	443.50	440.54	0.160	0.250	7.880	5.000
IN215	PADOT Type 'C'	In Sag	435.97	435.97	433.10	0.040	0.950	7.880	5.000
IN304	PADOT Type 'C'	On Grade	442.69	442.69	439.72	0.010	0.250	7.880	5.000
IN304A	PADOT Type 'C'	In Sag	442.25	442.25	440.25	0.010	0.250	7.880	5.000
IN305	PADOT Type 'C'	On Grade	442.50	442.50	439.33	0.040	0.880	7.880	5.000
IN306	PADOT Type 'C'	On Grade	443.09	443.09	438.44	0.310	0.850	7.880	5.000
IN307	PADOT Type 'C'	On Grade	443.26	443.26	438.19	0.150	0.860	7.880	5.000
IN308	PADOT Type 'C'	On Grade	444.58	444.58	437.59	0.090	0.910	7.880	5.000
IN309	PADOT Type 'C'	On Grade	444.18	444.18	441.43	0.180	0.870	7.880	5.000
IN310	PADOT Type 'C'	On Grade	443.90	443.90	436.32	0.160	0.860	7.880	5.000
IN311	PADOT Type 'C'	On Grade	442.53	442.53	435.70	0.150	0.880	7.880	5.000
IN313	PADOT Type 'C'	In Sag	440.56	440.56	425.22	0.110	0.840	7.880	5.000
IN314	PADOT Type 'C'	In Sag	440.56	440.56	424.94	0.530	0.840	7.880	5.000
IN315	PADOT Type 'C'	In Sag	442.32	442.32	439.28	0.210	0.850	7.880	5.000
IN316	PADOT Type 'C'	On Grade	439.22	439.22	436.47	0.220	0.880	7.880	5.000
IN317	PADOT Type 'C'	On Grade	439.12	439.12	436.18	0.240	0.820	7.880	5.000
IN318	PADOT Type 'C'	On Grade	436.94	436.94	434.19	0.120	0.800	7.880	5.000
IN319	PADOT Type 'C'	On Grade	436.83	436.83	433.90	0.180	0.900	7.880	5.000
IN320	PADOT Type 'C'	On Grade	436.23	436.23	433.28	0.190	0.620	7.880	5.000
IN322	PADOT Type 'C'	In Sag	439.00	439.00	435.50	0.450	0.410	7.880	5.000
IN504	PADOT Type 'C'	In Sag	444.16	444.16	439.68	0.200	0.820	7.880	5.000
IN505	PADOT Type 'C'	In Sag	442.08	442.08	439.33	0.050	0.250	7.880	5.000
IN506	PADOT Type 'C'	In Sag	443.04	443.04	439.05	0.100	0.910	7.880	5.000
IN507	PADOT Type 'C'	In Sag	444.96	444.96	441.04	0.100	0.890	7.880	5.000
IN508	PADOT Type 'C'	In Sag	444.86	444.86	439.85	0.110	0.960	7.880	5.000
IN509	PADOT Type 'C'	In Sag	444.00	444.00	438.64	0.070	0.970	7.880	5.000
IN510	PADOT Type 'C'	In Sag	442.33	442.33	438.23	0.110	0.940	7.880	5.000
IN511	PADOT Type 'M'	In Sag	440.81	440.81	437.75	0.180	0.710	7.880	5.000
IN512	PADOT Type 'M'	In Sag	441.50	441.50	436.94	0.230	0.840	7.880	5.000
IN513	PADOT Type 'C'	On Grade	441.00	441.00	436.23	0.110	0.880	7.880	5.000
IN514	PADOT Type 'C'	In Sag	437.85	437.85	434.93	0.230	0.970	7.880	5.000

IN516	PADOT Type 'C'	In Sag	445.60	445.60	441.70	0.650	0.250	7.880	5.000
IN517	PADOT Type 'M'	In Sag	443.00	443.00	436.54	0.330	0.300	7.880	5.000
IN518	PADOT Type 'M'	In Sag	441.85	441.85	435.73	0.200	0.260	7.880	5.000
IN519	PADOT Type 'M'	In Sag	441.50	441.50	434.79	0.130	0.310	7.880	5.000
IN520	PADOT Type 'M'	In Sag	441.00	441.00	433.91	0.070	0.330	7.880	5.000
IN521	PADOT Type 'M'	In Sag	435.60	435.60	432.68	0.130	0.340	7.880	5.000
IN522	PADOT Type 'C'	In Sag	437.32	437.32	432.06	0.070	0.790	7.880	5.000
YD512	Grate 18 Nyloplast	In Sag	445.70	445.70	443.82	0.020	0.650	7.880	5.000
YD513	PADOT Type 'C'	In Sag	446.20	446.20	442.90	0.030	0.560	7.880	5.000
YD514	PADOT Type 'C'	In Sag	446.20	446.20	442.43	0.020	0.550	7.880	5.000
YD515	PADOT Type 'M'	In Sag	446.20	446.20	441.96	0.010	0.550	7.880	5.000

R:\21\PC211160\Technical\Stormwater & E&S\Rev 5 - Retail Addition\StormCAD\Byers Station_Storm-6-new.stc

Scenario: 100-Year
Current Time Step: 0.000Hr
Conduit FlexTable: Pipe Report

Start Node	Stop Node	Section Size (Catalog Conduit)	Material	Length (User Defined) (ft)	Slope (ft/ft)	Capacity (Full Flow) (ft ³ /s)	Flow (Link) (ft ³ /s)	Velocity (Average) (ft/s)	Elevation Ground (Start) (ft)	Elevation Ground (Stop) (ft)	Invert (Upstream) (ft)	Invert (Downstream) (ft)	Cover (Start) (ft)	Cover (Stop) (ft)	Hydraulic Grade Line (In) (ft)	Hydraulic Grade Line (Out) (ft)	Energy Grade Line (In Link) (ft)
DB500	DB501	15 inch	HDPE	16.0	0.0050	4.95	0.10	1.61	446.81	446.55	443.51	443.43	2.05	1.87	443.63	443.55	443.67
DB501	YD513	15 inch	HDPE	36.0	0.0053	5.08	0.10	1.64	446.55	446.20	443.26	443.07	2.04	1.88	443.38	443.27	443.42
YD512	DB500	15 inch	HDPE	27.0	0.0052	5.04	0.10	1.64	445.70	446.81	443.82	443.68	0.63	1.88	443.94	443.80	443.99
IN504	IN506	15 inch	HDPE	91.4	0.0050	4.96	1.30	3.41	444.16	443.04	439.68	439.22	3.23	2.57	440.47	440.45	440.51
IN505	IN506	15 inch	HDPE	21.9	0.0050	4.96	0.10	1.60	442.08	443.04	439.33	439.22	1.50	2.57	440.45	440.45	440.45
IN507	IN508	15 inch	HDPE	67.6	0.0151	8.60	0.71	4.23	444.96	444.86	441.04	440.02	2.67	3.59	441.37	440.37	441.49
IN506	IN509	15 inch	HDPE	49.4	0.0049	4.88	2.09	1.70	443.04	444.00	439.05	438.81	2.74	3.94	440.43	440.38	440.47
IN508	IN509	15 inch	HDPE	79.9	0.0130	7.98	1.53	5.02	444.86	444.00	439.85	438.81	3.76	3.94	440.34	440.36	440.52
IN509	IN510	15 inch	HDPE	47.3	0.0051	5.01	4.05	3.30	444.00	442.33	438.64	438.40	4.11	2.68	440.29	440.13	440.46
IN510	IN511	15 inch	HDPE	61.6	0.0050	4.96	4.79	3.90	442.33	440.81	438.23	437.92	2.85	1.64	440.03	439.74	440.26
YD513	YD514	15 inch	HDPE	60.0	0.0050	4.95	0.81	2.97	446.20	446.20	442.90	442.60	2.05	2.35	443.25	442.94	443.38
YD514	YD515	15 inch	HDPE	60.0	0.0050	4.95	1.27	3.38	446.20	446.20	442.43	442.13	2.52	2.82	442.87	442.56	443.04
IN522	IN43	15 inch	HDPE	45.8	0.0083	6.37	5.47	5.84	437.32	434.78	432.06	431.68	4.01	1.85	433.09	432.93	433.49
IN521	IN522	15 inch	HDPE	54.1	0.0050	4.94	5.13	4.58	435.60	437.32	432.68	432.41	1.67	3.66	433.72	433.33	434.07
IN520	IN521	15 inch	HDPE	166.7	0.0064	5.58	4.91	5.13	441.00	435.60	433.91	432.85	5.84	1.50	434.82	433.90	435.23
MH503	IN520	15 inch	HDPE	72.2	0.0050	4.94	3.42	4.35	443.15	441.00	434.44	434.08	7.46	5.67	435.21	434.91	435.50
IN519	MH503	15 inch	HDPE	36.3	0.0050	4.93	3.44	4.34	441.50	443.15	434.79	434.61	5.46	7.29	435.56	435.36	435.85
MH502	IN519	15 inch	HDPE	58.2	0.0050	4.94	3.31	4.31	442.97	441.50	435.25	434.96	6.47	5.29	436.00	435.70	436.29
IN518	MH502	15 inch	HDPE	62.0	0.0050	4.95	3.33	4.33	441.85	442.97	435.73	435.42	4.87	6.30	436.48	436.17	436.77
IN517	IN518	15 inch	HDPE	129.1	0.0050	4.93	2.86	4.17	443.00	441.85	436.54	435.90	5.21	4.70	437.22	436.58	437.49
EX.IN216	IN42	15 inch	HDPE	27.5	0.0564	16.62	0.30	5.21	437.00	436.40	433.75	432.20	2.00	2.95	433.96	432.44	434.04
IN514	IN215	15 inch	HDPE	42.6	0.0390	13.81	9.24	12.06	437.85	435.97	434.93	433.27	1.67	1.45	436.09	435.01	437.03
IN513	IN514	15 inch	HDPE	66.6	0.0170	9.12	7.53	8.30	441.00	437.85	436.23	435.10	3.52	1.50	437.32	436.47	438.00
IN512	IN513	15 inch	HDPE	107.5	0.0050	4.96	7.06	5.75	441.50	441.00	436.94	436.40	3.31	3.35	438.65	437.50	439.16
IN511	IN512	15 inch	HDPE	72.7	0.0051	4.99	5.69	4.64	440.81	441.50	437.75	437.38	1.81	2.87	439.66	439.17	439.99
IN516	IN517	15 inch	HDPE	97.0	0.0149	8.55	1.99	5.68	445.60	443.00	441.70	440.25	2.65	1.50	442.26	440.66	442.48
YD515	IN516	15 inch	HDPE	19.3	0.0047	4.78	0.79	2.88	446.20	445.60	441.96	441.87	2.99	2.48	442.31	442.29	442.43
IN304A	IN304	15 inch	HDPE	24.0	0.0108	7.28	0.02	1.28	442.25	442.69	440.25	439.99	0.75	1.45	440.30	440.04	440.32
IN215	MH305	15 inch	HDPE	48.0	0.0429	14.50	9.49	12.59	435.97	435.42	433.10	431.04	1.62	3.13	434.27	432.48	435.25
IN204	IN38	15 inch	HDPE	21.0	0.0186	9.54	0.32	3.59	443.50	443.50	440.54	440.15	1.71	2.10	440.76	440.31	440.83
MH305	IN44A	24 inch	HDPE	29.0	0.0328	44.35	20.07	13.77	435.42	434.47	430.29	429.34	3.13	3.13	431.90	431.44	432.75
MH304	IN313	24 inch	HDPE	116.0	0.0100	24.51	9.26	7.26	442.29	440.56	435.32	434.16	4.97	4.40	436.41	435.01	436.84
MH303	EW303	30 inch	HDPE	15.0	0.0067	36.28	16.68	3.40	436.75	434.71	422.60	422.50	11.65	9.71	426.27	426.25	426.45
EXMH63	EXMH62	24 inch	HDPE	188.0	0.0106	25.28	1.42	4.34	442.59	440.40	433.13	431.13	7.46	7.27	433.54	431.45	433.69
EXMH62	EW301	24 inch	HDPE	28.0	0.0300	42.45	1.38	6.18	440.40	434.00	430.34	429.50	8.06	2.50	430.75	429.75	430.89
IN309	IN310	15 inch	HDPE	29.0	0.0097	6.88	0.93	3.91	444.18	443.90	441.43	441.15	1.50	1.50	441.81	441.46	441.94
IN308	IN310	24 inch	HDPE	221.0	0.0050	17.29	5.75	4.95	444.58	443.90	437.59	436.49	4.99	5.41	438.44	437.28	438.76
IN305	IN307	15 inch	HDPE	35.0	0.0111	7.39	0.47	3.38	442.50	443.26	439.33	438.94	1.92	3.07	439.60	439.15	439.69
IN307	IN308	24 inch	HDPE	85.0	0.0051	17.43	5.30	4.87	443.26	444.58	438.19	437.76	3.07	4.82	439.00	438.52	439.31
IN310	IN311	24 inch	HDPE	89.0	0.0051	17.43	7.20	5.28	443.90	442.53	436.32	435.87	5.58	4.66	437.27	436.77	437.64
HW02	IN313	15 inch	HDPE	21.0	0.0100	7.00	2.44	5.19	437.00	440.56	431.76	431.55	3.99	7.76	432.39	432.07	432.63
IN319	IN320	15 inch	HDPE	24.0	0.0217	10.30	4.24	7.98	436.83	436.23	433.90	433.38	1.68	1.60	434.73	434.30	435.10
IN320	MH305	15 inch	HDPE	25.0	0.0268	11.46	5.26	9.14	436.23	435.42	433.28	432.61	1.70	1.56	434.21	433.26	434.66
IN44A	IN44	24 inch	HDPE	30.0	0.0133	28.30	21.30	9.89	434.47	433.57	429.32	428.92	3.15	2.65	431.26	431.05	431.99
MH08	IN35	18 inch	HDPE	66.0	0.0259	18.32	0.89	5.36	450.13	445.11	443.68	441.97	4.95	1.64	444.03	442.20	444.16
MH302	EW304	24 inch	HDPE	21.0	0.0052	17.74	26.00	8.28	430.22	426.00	422.61	422.50	5.61	1.50	424.64	424.29	425.70
MH301	MH302	24 inch	HDPE	207.0	0.0140	28.96	26.40	10.45	433.00	430.22	425.67	422.78	5.33	5.44	427.47	424.76	428.69
IN322	EXMH63	24 inch	HDPE	206.0	0.0107	25.33	1.47	4.39	439.00	442.59	435.50	433.30	1.50	7.29	435.92	433.63	436.07
IN315	MH304	15 inch	HDPE	22.0	0.0050	4.95	1.42	3.48	442.32	442.29	439.28	439.17	1.79	1.87	439.75	439.63	439.93
IN311	MH304	24 inch	HDPE	21.0	0.0100	24.51	7.99	6.97	442.53	442.29	435.70	435.49	4.83	4.80	436.71	436.70	437.10
IN313	IN314	30 inch	HDPE	22.0	0.0050	31.42	12.31	6.01	440.56	440.56	425.22	425.11	12.84	12.95	426.40	426.37	426.85
IN314	MH303	30 inch	HDPE	83.0	0.0065	35.84	16.82	7.18	440.56	436.75	424.94	424.40	13.12	9.85	426.33	426.30	426.89
IN316	IN317	15 inch	HDPE	24.0	0.0050	4.95	1.09	3.24	439.22	439.12	436.47	436.35	1.50	1.52	436.88	436.89	437.03
IN317	IN319	15 inch	HDPE	115.0	0.0183	9.46	2.19	6.27	439.12	436.83	436.18	434.08	1.69	1.50	436.77	434.88	437.00
IN306	IN307	24 inch	HDPE	29.0	0.0028	12.87	4.03	3.62	443.09	443.26	438.44	438.36	2.65	2.90	439.21	439.11	439.41
HW01	IN39	15 inch	HDPE	21.0	0.0386	13.74	1.99	7.97	437.00	439.56	432.47	431.66	3.28	6.65	433.03	432.24	433.25
IN318	IN319	15 inch	HDPE	24.0	0.0050	4.95	0.90	3.07	436.94	436.83	434.19	434.07	1.50	1.51	434.80	434.80	434.84
IN304	IN40	15 inch	HDPE	51.0	0.0490	15.49	0.04	2.66	442.69	441.27	439.72	437.22	1.72	2.80	439.80	437.36	439.82
IN40A	IN39	15 inch	HDPE	22.0	0.0314	12.39	0.85	5.78	439.52	439.56	436.92	436.23	1.35	2.08	437.28	436.45	437.41
IN49	MH08	15 inch	HDPE	20.0	0.0140	8.28	0.89	4.41	451.25	450.13	447.03	446.75	2.97	2.13	447.40	447.03	447.53
IN45	MH301	24 inch	HDPE	116.0	0.0134	28.33	26.63	10.25	432.24	433.00	427.39	425.84	2.85	5.16	429.19	427.78	430.43
IN46	IN45	15 inch	HDPE	34.0	0.0400	14.00	0.47	5.28	432.40	432.24	429.00	427.64	2.15	3.35	429.59	429.60	429.60
IN44	IN45	24 inch	HDPE	38.0	0.0384	48.04	26.04	15.60	433.57	432.24	428.90	427.44	2.67	2.80	430.69	430.05	431.89
IN41	MH305	24 inch	HDPE	40.0	0.0017	10.25	5.94	3.38	436.41	435.42	430.53	430.46	3.88	2.96	432.01	431.99	432.10
IN42	IN41	15 inch	HDPE	22.0	0.0041	4.48	0.97	2.91	436.40	436.41	432.00	431.91	3.15	3.25	432.40	432.30	432.53
IN43	IN44	15 inch	HDPE	50.0	0.0482	15.36	5.98	11.73	434.78	433.57	431.66	429.25	1.87	3.07	432.65	431.00	433.16
IN39	IN41	24 inch	HDPE	151.0	0.0045	1											

IN35	IN36	24 inch	HDPE	22.0	0.0195	34.26	1.47	5.43	445.11	445.08	441.46	441.03	1.65	2.05	441.88	441.32	442.03
------	------	---------	------	------	--------	-------	------	------	--------	--------	--------	--------	------	------	--------	--------	--------

R:\21\PC211160\Technical\Stormwater & E&S\Rev 5 - Retail Addition\StormCAD\Byers Station_Storm-6-new.stc



July 3, 2025

File No. 03-0434.07

E-MAIL ONLY

Mr. Tony Scheivert
Upper Uwchlan Township Manager
140 Pottstown Pike
Chester Springs, PA 19425

Reference: Parcel 5C (Byers Station) – Lot 2B (Phase 2)
Amended Final PRD Plans – First Review
Upper Uwchlan Township, Chester County, PA

Dear Tony:

Gilmore & Associates, Inc. (G&A) is in receipt of the following documents from Bohler Engineering, unless otherwise noted:

- Plan set consisting of Thirty Five (35) sheets titled “Byers Station Amended Final PRD Plans (Final Plan for Lot 2B of Parcel 5C)” dated March 15, 2022, last revised May 30, 2025.
- Byers Station Lot 2B, Parcel 5C – Traffic Impact Assessment, prepared by TPD, dated June 2, 2025.
- Byers Station Lot 2B, Parcel 5C - Parking and Traffic Analysis, prepared by TPD, dated June 2, 2025.
- General Project Description and Stormwater Management Calculations, dated March 25, 2019, last revised May 30, 2025.
- Correspondence from Alyson Zarro, Esq., RRH&C, dated June 4, 2025.

G&A, as well as the Township Traffic Consultant, Bowman, and Township Sewer Consultant, ARRO, have completed our first review of the above referenced Amended Final Plans for compliance with the Conditions of Tentative Approval Byers Station PRD, Township Zoning Ordinance, Subdivision and Land Development Ordinance, as well as the Stormwater Management Ordinance, and wish to submit the following comments for consideration.

I. OVERVIEW

The subject parcel is located at the northeastern corner of the intersection of Station Boulevard and Pottstown Pike. The tract is approximately 29.8 acres in size and is located in the R-4

184 West Main Street | Suite 300 | Trappe, PA 19426 | Phone: 610-489-4949 | Fax: 610-489-8447

File No.: 03-0434.07
July 3, 2025

Residential District and C-1 Village District, being developed under the Planned Residential Development Option as set forth in the Byers Station conditions of tentative approval. The parcel was subdivided into three lots; Lot 1, Lot 2A, and Lot 2B. Lots 1 and 2A consisting of 16.353 acres and 9.468 acres respectively, are now fully constructed. This application is for a "Phase 2" for Lot 2B which is 3.971 acres in size. There is currently a 10,500 SF Daycare Center and 10,500 SF Retail Space under construction on the site. The applicant would now like to propose an additional 10,500 SF Retail Space on the north side of the parcel.

Stormwater management will be handled via two (2) bioretention basins located along the eastern ends of the property which were constructed during previous phases of the project.

II. TOWNSHIP ENGINEERS COMMENTS
GILMORE & ASSOCIATES, INC.

1. We would recommend all plans and documents be revised to reference "Phase 2" to differentiate this application from the previously approved one.
2. ZO §200-74.B., §200-73.B(2)(b) and §200-76.A(1). – Off-street loading and unloading is proposed to be handled as it was in the previous phase with standard parking spaces being identified for loading / unloading during off peak business hours. We have no objection to handling this issue in this manner for this phase as well.
3. There appears to be a large gap in the buffer landscaping at the northwest corner of the site. (Above the retaining wall) Please provide landscaping in this area in accordance with SALDO §162-57.
4. Design plans must be provided for the proposed retaining walls. This may be deferred until construction, however, at a minimum, a letter should be provided from a structural engineer confirming the proposed landscaping above the wall will not conflict with any tie-backs or other elements of the walls.
5. Please revise the reference to the Parking and Traffic Analysis in the Site Design Requirements Table on Sheet 2 to reference the latest version. (Dated June 2, 2025)
6. The existing curb lines for Begonia Drive appear to be missing on several plan sheets. Please review and correct.
7. Please provide plans to the Chester County Conservation District to determine what level of modification will be required to the NPDES Permit. Please copy the Township on all correspondence.

File No.: 03-0434.07
July 3, 2025

III. TOWNSHIP TRAFFIC CONSULTANT COMMENTS
BOWMAN

1. SALDO Section 162-9.H(2) – The traffic study should be revised to include the missing figures.
2. ZO Section 200-73.F – The Byers Station Conditions of Tentative Approval allow for a reduction in the sum total of the required parking if there are efficiencies derived by shared parking for uses with complementary peak demands based on industry parking generation data. This method of calculating the required parking is consistent with the approach previously approved by the Township for this site. Based on the results of the submitted shared parking analysis, the total combined peak parking demand for all of the land uses on Lot 2B is 108 parking spaces. By comparison, the plan proposes 143 parking spaces, and therefore, the proposed parking supply is sufficient to serve the needs of the development.
3. SALDO Section 162-9.H(2) – Previous iterations of the plans and conditions of approval for this Lot 2B development proposed an extension of Iris Lane within a variable width access and utility easement for a driveway to Pottstown Pike. The current plan eliminates this access and proposes to extinguish the easement.. The current plan proposes more development square footage (31,500 sf vs. 25,520 sf) and generates more traffic than contemplated by the previous version of the plan when the Pottstown Pike access had been proposed. Permanent elimination of the access to Pottstown Pike will add more retail traffic to Begonia Drive. Furthermore, elimination of the access to Pottstown Pike precludes any opportunity to reduce development traffic pressure at the intersection of Pottstown Pike and Park Road/Station Boulevard. Because of the inter-relationship of the Pottstown Pike access and the adjacent residential community, we recommend further discussion regarding the plan at a public meeting which provides an opportunity for the neighbors to provide input.
4. It is our understanding that Begonia Drive is maintained by the neighboring HOA. As such, with all of the Lot 2B commercial traffic using Begonia Drive for access, then we question whether the Lot 2B development should participate in the maintenance of this road.
5. With all of the Lot 2B commercial traffic using Begonia Drive for access, we recommend consideration for measures to minimize interactions between residential/school

File No.: 03-0434.07

July 3, 2025

pedestrian traffic and retail traffic. As contemplated by the current plan, residential pedestrians are required to cross Begonia Drive, as well as cross two retail driveways in order to walk to/from Station Boulevard and the Pickering Valley Elementary School. As such, measures should be considered, such as sidewalk on the east side of Begonia Drive and/or traffic calming devices along Begonia Drive. These strategies will help to slow traffic and better integrate the retail and residential traffic and pedestrian activity.

IV. TOWNSHIP SEWER CONSULTANT COMMENTS **ARRO**

1. The plans propose the construction of a second phase with a second 10,500 square foot (SF) commercial building and additional parking on the site. The applicant will need to indicate any change to the proposed estimated sanitary sewer capacity for the project. The capacity will need to be purchased by the applicant.
2. Should there be any increases in sanitary sewer capacity above that previously approved Sewage Facilities Planning Module (SFPM). An amended approval will need to be obtained by Pennsylvania Department of Environmental Protection (PADEP).
3. An appropriately sized oil grease interceptor shall be provided for any restaurants/establishments preparing food. If those proposed locations are known at this time please identify on the drawing, in order to ensure that oil grease interceptors are provided.
4. The necessary financial security shall be posted with the Township, which shall be in a form and amount acceptable to the Township.

This concludes our first review of the above referenced Amended Final Plans. We recommend the plans be revised to address the above referenced comments as well as any of those of the Township Planning Commission and Board of Supervisors. If you have any questions, please do not hesitate to contact me.

Mr. Tony Scheivert
Upper Uwchlan Township Manager
Reference: Parcel 5C (Byers Station) – Lot 2B (Phase 2)
Amended Final PRD Plans – First Review
Upper Uwchlan Township, Chester County, PA

Page – 5 –

File No.: 03-0434.07
July 3, 2025

Sincerely,

David N. Leh

David N. Leh, P.E.
Vice President
Gilmore & Associates, Inc.

cc: Upper Uwchlan Township Planning Commission Members
Upper Uwchlan Township Board of Supervisors
Kristin S. Camp, Esq., BBM&M
Christopher J. Williams, P.E., Bowman
David Schlott, P.E., ARRO Consulting, Inc.
Matt Brown, P.E., DEE – UUTMA Authority Administrator
Alyson Zarro, Esq. – RRHC
Chris Puzinas, P.E., Bohler Engineering
Guy DiMartino, P.E., TPD
Allan Greenberg, Applicant
Gary Large, Applicant
Gwen Jonik, Township Secretary



TOWNSHIP OF UPPER UWCHLAN
BOARD OF SUPERVISORS MEETING,
CONDITIONAL USE HEARING

July 21, 2025

7:00 p.m.

DRAFT

Attending:

Board of Supervisors

Andrew P. Durkin, Vice-Chair
Sandra M. D'Amico, Member

Township Administration

Tony Scheivert, Township Manager
Gwen Jonik, Township Secretary
Jill Bukata, Township Treasurer
Anthony Campbell, Zoning Officer
Tom Jones, Police Chief
Mike Esterlis, Public Works Director
Dave Leh, Township Engineer

Kristin Camp, Esq., Township Solicitor
Melissa Bailey, Court Stenographer

LOCATION: The Barn at Upland Farm, 301 Pottstown Pike, Chester Springs PA 19425

Mr. Durkin called the evening to order at 7:02 p.m., led the Pledge of Allegiance, and offered a moment of silence. No one planned to record the meeting. There were 25 citizens in attendance.

Swearing in Ceremony ~ K9 Officer Dexter

Chief Jones summarized Upper Uwchlan's K9 Officer program over the years. There have been 2 K9 Officers previously. Officer Zmuida and K9 Dexter attended intensive training earlier this year. Tony Scheivert administered the Oath of Office. Sgt. Bright and Corporal Davis presented Dexter and Officer Zmuida with their credentials, which are handed down from both of their K9 programs.

Chief Jones recognized Anthony Gorgone and Family for their donation to the program, funding the purchase of Dexter and for the training program. Chief Jones recognized Eagle Animal Hospital for their donation of veterinary care.

Mr. Durkin announced a 5-minute recess.

Mr. Durkin reconvened the meeting at 7:22 p.m.

Conditional Use Hearing ~ 705 N. Reeds Road

Kristin Camp explained the conditional use approval process and introduced the Application of Michaela Kolbe, seeking approval to use 2 cottages on her property as short-term rentals. One of the cottages is currently being used as a short-term rental and Ms. Kolbe proposes eventually using the second as such, if approved. Short-term rentals are allowed in the R1 zoning district via conditional use approval. Melissa Bailey recorded the Hearing. Her transcript is the official Record of the Hearing. The following is a brief summary.

Ms. Camp entered the following Board exhibits into the Record:

B1 – Conditional Use Application

B2 – Proof of publication of the Hearing in the Daily Local News

B3 – Aerial photos of the property

B4 – Affidavit of property posting

B5 – Draft June 12, 2025 Planning Commission meeting minutes

B6 – Extension of timing to hold the Hearing ~ Granted by the Applicant

Ms. Camp explained becoming a Party to the proceedings. There were none.

Ms. Kolbe was sworn in by Ms. Bailey and provided testimony. The property is 1 acre with 2 separate cottages; she renovated the front cottage and has been renting it out; she's currently renovating the second cottage; the cottages are 650 SF each, 1 bedroom; rentals are for 2 people; safety measures in place; she's beautifying the property; parties are not allowed. Ms. Kolbe is the local responsible 'agent'.

There was discussion with Ms. Camp and the Board regarding age of renters, whether pets were allowed, and fencing for dogs.

Mr. Waltman, 730 N. Reeds Road made comment concerning parking and rule enforcement.

Ms. Costa, 710 N. Reeds Road commented on the property beautification, and is concerned with groups booking both cottages at the same time.

Mr. Sweeney, 735 N. Reeds Road inquired if rentals were permitted before, has no issue with the use, and made comment about rule enforcement if she's not living there.

Ms. Camp advised that Supervisor Baxter wasn't present this evening but may want to take part in the Decision. The Board has 45 days to render their Decision. Ms. Camp closed the Record and the Board will vote at their August meeting.

Mr. Durkin announced a 5-minute recess, and reconvened the Meeting at 7:59 p.m.

Approval of Minutes

Mrs. D'Amico moved to approve the minutes of the June 23, 2025 Board of Supervisors Meeting. Mr. Durkin seconded, and the motion carried.

Approval of Payments

Mrs. D'Amico moved to approve the payments to all vendors as listed July 16, 2025. Mr. Durkin seconded, and the motion carried.

Treasurer's Report

Jill Bukata reported that the balance sheet remains strong; we are halfway through the year; year-to-date revenues are at 62.8% of the budget; year-to-date expenses are at 48.8% of the budget; earned income tax revenue is @\$58,000 higher than this time last year.

Mr. Scheivert reported that we received a check from the "Green Light Go" traffic light upgrade grant; 2 traffic lights were hit by a dump truck – repairs/replacement will be reimbursed by insurance.

Mr. Durkin announced an agenda item would be taken out of order.

ADMINISTRATION. F.

Dan O'Brien, RBC Capital Markets, LLC ~ General Obligation Bond Issue Review

Mrs. Baxter was present via phone. Mr. O'Brien provided a municipal bonds overview. The last time a Bond was issued for the Township was 6 years ago. The Township borrows funds for projects via bonds. Bonds are tax exempt from income taxes on the interest rate. Bonds, following the Local Government Bond Act, are complicated so the Township has a Bond

Counsel. Bonds are paid off over 30-40 years, usually the useful life of what you're borrowing the money for. Interest rates are fixed over the life of the Bond. The Municipal Authority projects are included in the Bond. RBC is the investment bank. Upper Uwchlan has a very high rating – Aa1. The procedure is to enact an ordinance to set bond parameters and authorize the bond purchase agreement with RBC. This bond will be for \$9,000,000; \$1,600,000 to acquire 144/128 Byers Road, \$1,385,000 for Hickory Park improvements, and \$5,500,000 for the Municipal Authority's sewer-related projects – acquire wastewater disposal areas, extend service. Mr. O'Brien walked through the debt structure for 20-year, 25-year, 30-year. The Supervisors and the Authority may meet to decide the term of the bond.

Supervisor's Report

Mr. Durkin read the published calendar: July 27, 2025 1:00-4:00 Farmhouse Museum open at Upland Farm Farmhouse hosted by the Historical Commission; August 10, 2025 4:30 p.m. Live music concert by Starner & Schmidts Acoustic Duo at Upland Farm Park hosted by the Park & Recreation Board; September 1, 2025 Office Closed ~ Labor Day; yard waste collection dates July 23, August 6, August 20.

Mr. Durkin announced that the Supervisors will meet only once in August, August 12, 2025 at 6:00 p.m.

Administration Reports

Township Engineer's Report

Dave Leh reported the Planning Commission recommended approval for Byers Station Parcel 5C Lot 2B Phase 2, proposing another 10,500 SF retail/commercial building to the north of the Celebree Daycare that is currently under construction; revised plans for 500 Pottstown Pike (Porsche Service Center) are under consultant review.

Building and Codes Department

Anthony Campbell reported 62 building permits were issued last month, totaling \$34,038 in permit fees; 146 scheduled inspections performed; 12 resales; 4 new homes settled; decreased number of zoning complaints; helped with storm cleanup.

Police Chief's Report

Chief Jones reported 964 incidents were logged, 110 citations issued, the 2025 Block Party was a big success; prepare emergency kits for your home and cars in case of summer storms.

Public Works Department

Mike Esterlis reported the Department completed 124 work orders, completed road base repairs on Oscar Way and Senn Drive, assisted with Junior Police Academy and the block party and removed artifacts for the Historical Commission from the Windsor Baptist Church.

Sue Quake questioned the timeline for paving of Font Road. Mr. Esterlis noted it would most likely be this Fall for trench restoration and paving on Surrey Lane, Carriage Drive and Font Road, from Black Horse to Route 100.

ADMINISTRATION

Ordinance Amendments. Ms. Camp advised amendments had been made and reviewed by the County Planning Commission to modernize our sign ordinance and to amend the short term rental ordinance – that approval is specific to the applicant, not the land, that pets are limited

and to incorporate into the applicant's rules that quiet hours are 10:00 p.m. – 7:00 a.m. There was more discussion about adding age restrictions for the renters – 21 or 25 years of age -- into the ordinance. The vote was tabled tonight to add an age restriction and vote in Sept.

Struble Trail Traffic Crossing Signal Agreement. Ms. Camp explained an Agreement with the County for the pedestrian crossing signal on Dorlan Mill Road for the Struble Trail extension. The County asked the Township to be the Permittee, own the signal and maintain it. The County will reimburse the Townships for those costs. This Agreement memorializes those details. Mrs. D'Amico moved to approve and execute the Agreement. Mr. Durkin seconded, and the motion carried.

Disposition of Township. Gwen Jonik explained that the Public Works Department had replaced the 2008 John Deere tractor and boom mower and offered it for sale via Municibid, an electronic auction. The high bid was \$33,300.00. Mrs. D'Amico moved to accept the high bid and approve the sale to the high bidder. Mr. Durkin seconded, and the motion carried.

Verizon Cable Franchise Renewal. Mr. Scheivert read a statement that the Township was negotiating renewing the Verizon Cable Franchise and was seeking public comment regarding past performance of Verizon and future cable-related needs. Mr. Scheivert noted that this used to be a revenue stream for the Township, however, with streaming services abounding, this revenue continues to drop. There were no comments from the public.

Proposal for architectural drawings for 144 Byers Road. Mr. Scheivert advised that McHugh Engineering Associates had provided a scope of work for providing consulting engineering services for 144 Byers Road, the building the Township has purchased for administration offices, for additions/alterations. Giresi Design Architecture had provided schematic design drawings which were used in preparing McHugh's quote. Mrs. D'Amico moved to approve McHugh's proposal, not to exceed \$108,500, and subject to the revision of "Terms" the Township Solicitor may find objectionable. Mr. Durkin seconded, and the motion carried.

Open Session

Gerry Stein made comments regarding proposals for development of the Lionville Station Road property and how that would potentially affect East Township Line Road. Mr. Scheivert has been in touch with Uwchlan Township's Manager and there are several proposals – a natatorium (Olympic size pool), warehousing, homes/apartments -- and he knows the Planning Commission is concerned with the traffic. As the proposals are presented to Uwchlan Township, we can have our consultants review them and make comments. Bowman is the traffic engineer in both Townships.

Adjournment

There being no further business to be brought before the Board, Mr. Durkin adjourned the Meeting at 9:32 p.m.

Respectfully submitted,

Gwen A. Jonik
Township Secretary

Range of Checking Accts: GENERAL to GENERAL Range of Check Ids: 56379 to 56441
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void	Ref Num
56379	07/23/25	HOPEUNIF HOPE UNIFORM	313.06	07/31/25	3561
56380	08/12/25	108EMERG 10-8 EMERGENCY VEHICLE SERVICE	840.00		3569
56381	08/12/25	21ST 21st CENTURY MEDIA PHILLY	102.03		3569
56382	08/12/25	911SAFET 911 SAFETY EQUIPMENT	3,872.50		3569
56383	08/12/25	ACAPPELL A CAPPELLA POPS	100.00		3569
56384	08/12/25	ADVAN010 ADVANCED HORTICULTURAL SOLN	2,564.00		3569
56385	08/12/25	ALEXAFLE ALEXANDER FLEMING	112.00		3569
56386	08/12/25	AQUAP010 AQUA PENNSYLVANIA	1,567.15		3569
56387	08/12/25	ARBORCHE ARBORCHEM	1,840.00		3569
56388	08/12/25	BAYDISTR BAY GARAGE EQUIPMENT LLC	560.00		3569
56389	08/12/25	BRANDSPC BRANDYWINE VALLEY SPCA	67.60		3569
56390	08/12/25	BRANDYHD BRANDYWINE HARLEY-DAVIDSON	1,688.99		3569
56391	08/12/25	BRIAN010 BRIAN HOSKINS FORD	365.33		3569
56392	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI	519.50		3569
56393	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI	2,220.00		3569
56394	08/12/25	BURKHOLD BURKHOLDER MFG, INC.	75.00		3569
56395	08/12/25	CAMPBANT ANTHONY CAMPBELL	50.00		3569
56396	08/12/25	CEDAR010 CEDAR HOLLOW RECYCLING	161.10		3569
56397	08/12/25	CHARLHIG CHARLES A HIGGINS & SONS	337.50		3569
56398	08/12/25	COLLIFL COLLIFLOWER, INC	44.85		3569
56399	08/12/25	COMCA010 COMCAST	1,965.34		3569
56400	08/12/25	CRESTLIN CRESTLINE SPECIALTIES, INC	1,542.35		3569
56401	08/12/25	DELA030 DELAWARE VALLEY HEALTH TRUST	56,641.67		3569
56402	08/12/25	DIVISIO9 DIVISION 09 CONTRACT FLOORING	1,865.31		3569
56403	08/12/25	DVWCT DELAWARE VALLEY WORKERS COMP	11,936.00		3569
56404	08/12/25	EAGLEKUB EAGLE POWER KUBOTA	376.76		3569
56405	08/12/25	EAGLHARD EAGLE HARDWARE	94.08		3569
56406	08/12/25	GAPPOWER GAP POWER	500.00		3569
56407	08/12/25	HAWEI010 H.A. WEIGAND, INC.	1,043.00		3569
56408	08/12/25	HEIDELBE HEIDELBERG MATERIALS	956.95		3569
56409	08/12/25	HELPNOW HELP-NOW,LLC	4,193.03		3569
56410	08/12/25	INTER010 INTERCON TRUCK EQUIPMENT	1,610.82		3569
56411	08/12/25	KEENC010 KEEN COMPRESSED GAS COMPANY	55.29		3569
56412	08/12/25	LEVEN010 LEVENGOOD SEPTIC SERVICE	301.50		3569
56413	08/12/25	LUDWI060 LUDWIG'S CORNER SUPPLY CO.	180.56		3569
56414	08/12/25	MIDAT010 MID ATLANTIC CONNECTIONS, INC.	1,494.00		3569
56415	08/12/25	NAPA0010 NAPA AUTO PARTS	115.87		3569
56416	08/12/25	NEWHO010 NEW HOLLAND AUTO GROUP	913.42		3569
56417	08/12/25	PARAD010 ROBERT PARADIS	95.00		3569
56418	08/12/25	PATINOLG PATINO LANDSCAPE GROUP, LLC	3,650.00		3569
56419	08/12/25	PENDERGA PENDERGAST	65.00		3569
56420	08/12/25	PSATS030 PSATS-CDL PROGRAM	50.00		3569
56421	08/12/25	RGSASSOC RGS ASSOCIATES, INC	8,833.47		3569
56422	08/12/25	RHOADS RHOADS ENERGY	1,876.91		3569
56423	08/12/25	ROBLITTL ROBERT E. LITTLE, INC.	615.37		3569
56424	08/12/25	SEMPERON SEMPERON	1,173.18		3569
56425	08/12/25	SHIRTMOR SHIRTS N MORE	667.65		3569
56426	08/12/25	SNAPON01 SNAP-ON TOOLS	303.00		3569
56427	08/12/25	STAPLES STAPLES	838.87		3569
56428	08/12/25	STEPHEQU STEPHENSON EQUIPMENT, INC.	2,056.77		3569
56429	08/12/25	STYER010 STYER PROPANE	125.80		3569

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void	Ref Num
56430	08/12/25	SUSMCKEN SUSAN and PAUL MCKENNA	500.00		3569
56431	08/12/25	SWEETWAT SWEETWATER NATURAL PRODUCTS LL	80.00		3569
56432	08/12/25	THEPR020 PAVION	639.32		3569
56433	08/12/25	TMOBILE T-MOBILE	561.53		3569
56434	08/12/25	TONYSCH TONY SCHEIVERT	100.00		3569
56435	08/12/25	TRANSK9 TRANSK9 USA, INC	163.50		3569
56436	08/12/25	VERIZ010 VERIZON	300.72		3569
56437	08/12/25	VERIZFIO VERIZON	124.99		3569
56438	08/12/25	VERIZOSP VERIZON - SPECIAL PROJECTS	434.28		3569
56439	08/12/25	WESTV010 WEST VINCENT TOWNSHIP	23,861.70		3569
56440	08/12/25	WIGGISHR WIGGINS SHREDDING	137.35		3569
56441	08/12/25	ZMUID005 DANIEL ZMUIDA	68.89		3569
Report Totals					
		<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
	Checks:	63	0	150,509.86	0.00
	Direct Deposit:	0	0	0.00	0.00
	Total:	63	0	150,509.86	0.00

Range of Checking Accts: GENERAL to GENERAL Range of Check Ids: 56379 to 56441
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void	Ref Num
PO #	Item	Description				Contract	Ref Seq Acct
56379	07/23/25	HOPEUNIF HOPE UNIFORM				07/31/25	3561
25-00999	1	gun metal seal button set (72)	313.06	01-410-000-238 Clothing/Uniforms	Expenditure		1 1
56380	08/12/25	108EMERG 10-8 EMERGENCY VEHICLE SERVICE					3569
25-01029	1	replace reemption bracket 3211	840.00	01-410-000-235 Vehicle Maintenance	Expenditure		54 1
56381	08/12/25	21ST 21st CENTURY MEDIA PHILLY					3569
25-01013	1	cable franchise renewal	102.03	01-400-000-341 Advertising	Expenditure		17 1
56382	08/12/25	911SAFET 911 SAFETY EQUIPMENT					3569
25-01075	1	holsters (6)	1,746.00	01-410-000-238 Clothing/Uniforms	Expenditure		122 1
25-01075	2	keepers/handcuffs/multi cases	1,558.00	01-410-000-238 Clothing/Uniforms	Expenditure		123 1
25-01075	3	baton/cap cover/badges	568.50	01-410-000-238 Clothing/Uniforms	Expenditure		124 1
			<u>3,872.50</u>				
56383	08/12/25	ACAPPELL A CAPPELLA POPS					3569
25-01077	1	tree lighting - deposit	100.00	01-454-001-201 Park & Rec Special Events	Expenditure		126 1
56384	08/12/25	ADVAN010 ADVANCED HORTICULTURAL SOLN					3569
25-01001	1	ff-custom biological	2,564.00	01-454-003-450 Contracted Services	Expenditure		1 1
56385	08/12/25	ALEXAFLE ALEXANDER FLEMING					3569
25-01002	1	gasmask pouch reimbursement	112.00	01-410-000-238 Clothing/Uniforms	Expenditure		2 1
56386	08/12/25	AQUAP010 AQUA PENNSYLVANIA					3569
25-01004	1	twp	118.17	01-409-003-360 Utilities	Expenditure		4 1
25-01004	2	twp	221.35	01-409-003-360 Utilities	Expenditure		5 1
25-01004	3	pw	192.62	01-409-001-360 Utilities	Expenditure		6 1
25-01004	4	pw	148.11	01-409-001-360 Utilities	Expenditure		7 1
25-01004	5	upland	260.28	01-454-005-360 Utilities	Expenditure		8 1
25-01004	6	upland	199.38	01-454-005-360 Utilities	Expenditure		9 1
25-01004	7	ff	68.03	01-454-003-360 Utilities	Expenditure		10 1
25-01004	8	hp	207.79	01-454-002-360 Utilities	Expenditure		11 1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	
PO #	Item	Description					Ref Seq	Acct
56386	08/12/25	AQUA PENNSYLVANIA						
25-01004	9	milford	25.62	01-409-004-360	Expenditure		12	1
				Utilities				
25-01004	10	upland	125.80	01-454-005-360	Expenditure		13	1
				Utilities				
			1,567.15					
56387	08/12/25	ARBORCHE ARBORCHEM					3569	
25-01005	1	vastlan, freelexx, clean cut	1,840.00	01-454-001-200	Expenditure		14	1
				Supplies				
56388	08/12/25	BAYDISTR BAY GARAGE EQUIPMENT LLC					3569	
25-01006	1	vehicle lift inspection	340.00	01-409-001-450	Expenditure		15	1
				Contracted Services				
25-01006	2	valve flow regulating 2gpm	220.00	01-409-001-450	Expenditure		16	1
				Contracted Services				
			560.00					
56389	08/12/25	BRANDSPC BRANDYWINE VALLEY SPCA					3569	
25-01016	1	investigate dog negligence	67.60	01-422-000-530	Expenditure		18	1
				Contributions/SPCA				
56390	08/12/25	BRANDYHD BRANDYWINE HARLEY-DAVIDSON					3569	
25-01017	1	10k service unit 3211	1,688.99	01-410-000-235	Expenditure		19	1
				Vehicle Maintenance				
56391	08/12/25	BRIAN010 BRIAN HOSKINS FORD					3569	
25-01030	1	diagnose tire monitor 3201	365.33	01-410-000-235	Expenditure		55	1
				Vehicle Maintenance				
56392	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI					3569	
25-01066	1	705 reeds rd/kolbe cu appl	519.50	01-404-000-305	Expenditure		113	1
				Reimbursable CU Legal				
56393	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI					3569	
25-01067	1	twp-july services	2,220.00	01-404-000-311	Expenditure		114	1
				Non Reimbursable Legal				
56394	08/12/25	BURKHOLD BURKHOLDER MFG, INC.					3569	
25-01062	1	gorilla lift spring/cable	75.00	01-438-001-235	Expenditure		110	1
				Vehicle Maintenance - Facilities				
56395	08/12/25	CAMPBANT ANTHONY CAMPBELL					3569	
25-01003	1	telephone reimbursement	50.00	01-413-000-320	Expenditure		3	1
				Telephone				
56396	08/12/25	CEDAR010 CEDAR HOLLOW RECYCLING					3569	
25-01019	1	oversized rebar	161.10	01-438-000-245	Expenditure		20	1
				Highway Supplies				
56397	08/12/25	CHARLHIG CHARLES A HIGGINS & SONS					3569	
25-01020	1	rt100/ticonderoga in flash	337.50	01-434-000-450	Expenditure		21	1
				Contracted Services				

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	Ref Seq	Num Acct
PO #	Item	Description							
56398	08/12/25	COLLI01 COLLIFLOWER, INC					3569		
25-01021	1	metric tubing	24.86	01-438-000-200 Supplies	Expenditure		22		1
25-01021	2	swivel x-returned on 2719327	65.00	01-438-000-200 Supplies	Expenditure		23		1
25-01021	3	reducer/expander, 1/2" hose	45.01	01-438-000-200 Supplies	Expenditure		24		1
			<u>44.85</u>						
56399	08/12/25	COMCA010 COMCAST					3569		
25-01022	1	hp	381.44	01-454-002-450 Contracted Services	Expenditure		25		1
25-01022	2	twp	794.77	01-409-003-450 Contracted Services	Expenditure		26		1
25-01022	3	pw	373.35	01-409-001-450 Contracted Services	Expenditure		27		1
25-01022	4	upland	415.78	01-454-005-450 Contracted Services	Expenditure		28		1
			<u>1,965.34</u>						
56400	08/12/25	CRESTLIN CRESTLINE SPECIALTIES, INC					3569		
25-01023	1	blockparty giveaway/pen, opener	1,542.35	01-415-000-200 Supplies	Expenditure		29		1
56401	08/12/25	DELA030 DELAWARE VALLEY HEALTH TRUST					3569		
25-01024	1	admin	5,015.12	01-401-000-156 Employee Benefit Expens	Expenditure		30		1
25-01024	2	pd	32,092.95	01-410-000-156 Employee Benefit Expense	Expenditure		31		1
25-01024	3	codes	3,863.01	01-413-000-156 Employee Benefit Expens	Expenditure		32		1
25-01024	4	pw	13,320.12	01-438-000-156 Employee Benefit Expense	Expenditure		33		1
25-01024	5	facilities	2,350.47	01-438-001-156 Employee Benefit Expense	Expenditure		34		1
			<u>56,641.67</u>						
56402	08/12/25	DIVISIO9 DIVISION 09 CONTRACT FLOORING					3569		
25-01026	1	vinyl plank-(rain damage)	1,865.31	01-409-001-250 Maint & Repair	Expenditure		41		1
56403	08/12/25	DVWCT DELAWARE VALLEY WORKERS COMP					3569		
25-01025	1	2024 audit - codes (1%)	119.36	01-413-000-354 Insurance - Workers Comp	Expenditure		35		1
25-01025	2	2024 audit - admin (1%)	119.36	01-401-000-354 Insurance-Workers Comp	Expenditure		36		1
25-01025	3	2024 audit - pw (18%)	2,148.48	01-438-000-354 Insurance Workers Com	Expenditure		37		1
25-01025	4	2024 audit - facilities (12%)	1,432.32	01-438-001-354 Insurance - Workers Comp - Facilities	Expenditure		38		1
25-01025	5	2024 audit - parks (5%)	596.80	01-454-001-354 Insurance - Workers Com	Expenditure		39		1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	
PO #	Item	Description					Ref Seq	Acct
56403	08/12/25	DELAWARE VALLEY WORKERS COMP						
25-01025	6	2024 audit - pd (63%)	7,519.68	01-410-000-354	Expenditure		40	1
			11,936.00	Insurance - Workers Com				
56404	08/12/25	EAGLEKUB EAGLE POWER KUBOTA					3569	
25-01027	1	string trimmer pt104/tap	134.97	01-438-001-235	Expenditure		42	1
				Vehicle Maintenance - Facilities				
25-01027	2	fuel valve	40.04	01-438-001-235	Expenditure		43	1
				Vehicle Maintenance - Facilities				
25-01027	3	bracket/fuel shutoff valve	55.01	01-438-001-235	Expenditure		44	1
				Vehicle Maintenance - Facilities				
25-01027	4	wing nuts	17.58	01-438-001-235	Expenditure		45	1
				Vehicle Maintenance - Facilities				
25-01027	5	11x9 bolts, gas & tank caps	129.16	01-438-001-235	Expenditure		46	1
			376.76	Vehicle Maintenance - Facilities				
56405	08/12/25	EAGLHARD EAGLE HARDWARE					3569	
25-01028	1	5" bolt-144 byers	7.49	01-401-000-200	Expenditure		47	1
				Supplies				
25-01028	2	hardware	1.89	01-438-000-200	Expenditure		48	1
				Supplies				
25-01028	3	dish soap, hardware	10.77	01-454-001-200	Expenditure		49	1
				Supplies				
25-01028	4	levels, screws	37.47	01-454-005-250	Expenditure		50	1
				Repairs & Maint				
25-01028	5	90° elbow	4.99	01-438-000-200	Expenditure		51	1
				Supplies				
25-01028	6	brush cleaner	8.49	01-454-005-250	Expenditure		52	1
				Repairs & Maint				
25-01028	7	painter tape/brush set	22.98	01-454-005-250	Expenditure		53	1
			94.08	Repairs & Maint				
56406	08/12/25	GAPPOWER GAP POWER					3569	
25-01031	1	scissor lift rental	500.00	01-454-005-450	Expenditure		56	1
				Contracted Services				
56407	08/12/25	HAWEI010 H.A. WEIGAND, INC.					3569	
25-01032	1	10'uppers/sheeting/posts/uchan	1,043.00	01-433-000-200	Expenditure		57	1
				Supplies				
56408	08/12/25	HEIDELBE HEIDELBERG MATERIALS					3569	
25-01033	1	highway supplies	491.68	01-438-000-245	Expenditure		58	1
				Highway Supplies				
25-01033	2	magnolia base repair	465.27	01-438-000-245	Expenditure		59	1
			956.95	Highway Supplies				
56409	08/12/25	HELPNOW HELP-NOW,LLC					3569	
25-01064	1	monthly guardian services	4,193.03	01-407-000-450	Expenditure		112	1
				Contracted Services				

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	
PO #	Item	Description					Ref Seq	Acct
56410	08/12/25	INTER010 INTERCON TRUCK EQUIPMENT					3569	
25-01034	1	tri color corner led signal	551.20	01-438-000-235 Vehicle Maintenance	Expenditure		60	1
25-01034	2	striker bolt & bracket	30.96	01-438-000-235 Vehicle Maintenance	Expenditure		61	1
25-01034	3	gresen v20 series valve	1,028.66	01-438-000-200 Supplies	Expenditure		62	1
			<u>1,610.82</u>					
56411	08/12/25	KEENC010 KEEN COMPRESSED GAS COMPANY					3569	
25-01036	1	cylinder rental invoice	55.29	01-438-000-200 Supplies	Expenditure		63	1
56412	08/12/25	LEVEN010 LEVENGOOD SEPTIC SERVICE					3569	
25-01037	1	hp-pumped holding tank	301.50	01-454-002-450 Contracted Services	Expenditure		64	1
56413	08/12/25	LUDWI060 LUDWIG'S CORNER SUPPLY CO.					3569	
25-01038	1	propane tank refills	161.00	01-454-005-200 Supplies	Expenditure		65	1
25-01038	2	100 pk blades	15.99	01-438-000-200 Supplies	Expenditure		66	1
25-01038	3	metric bolts	3.57	01-438-000-200 Supplies	Expenditure		67	1
			<u>180.56</u>					
56414	08/12/25	MIDAT010 MID ATLANTIC CONNECTIONS, INC.					3569	
25-01039	1	install new chandeliers	1,494.00	01-454-005-250 Repairs & Maint	Expenditure		68	1
56415	08/12/25	NAPA0010 NAPA AUTO PARTS					3569	
25-01041	1	core deposit refunds	230.00	01-438-000-235 Vehicle Maintenance	Expenditure		69	1
25-01041	2	junction box	16.21	01-438-000-235 Vehicle Maintenance	Expenditure		70	1
25-01041	3	20 & 30 amp fuses	19.15	01-438-000-235 Vehicle Maintenance	Expenditure		71	1
25-01041	4	teminal	3.53	01-438-000-235 Vehicle Maintenance	Expenditure		72	1
25-01041	5	battery (no core deposit)	126.56	01-438-000-235 Vehicle Maintenance	Expenditure		73	1
25-01041	6	oil & filters	121.98	01-438-000-235 Vehicle Maintenance	Expenditure		74	1
25-01041	7	filters	58.44	01-438-000-235 Vehicle Maintenance	Expenditure		75	1
			<u>115.87</u>					
56416	08/12/25	NEWHO010 NEW HOLLAND AUTO GROUP					3569	
25-01042	1	ad wiring f3	298.50	01-438-000-235 Vehicle Maintenance	Expenditure		76	1
25-01042	2	fuel cap trk 8	19.92	01-438-000-235 Vehicle Maintenance	Expenditure		77	1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	
PO #	Item	Description					Ref Seq	Acct
56416	08/12/25	NEW HOLLAND AUTO GROUP						
		Continued						
25-01042	3	filler/reinforct/covers 3205	212.94	01-410-000-235	Expenditure		78	1
				Vehicle Maintenance				
25-01042	4	injector	314.15	01-438-000-235	Expenditure		79	1
				Vehicle Maintenance				
25-01042	5	bezel 3205	67.91	01-410-000-235	Expenditure		80	1
				Vehicle Maintenance				
			<u>913.42</u>					
56417	08/12/25	PARAD010 ROBERT PARADIS					3569	
25-01047	1	rifle instructor recert	95.00	01-410-000-316	Expenditure		85	1
				Training/Seminar				
56418	08/12/25	PATINOLG PATINO LANDSCAPE GROUP, LLC					3569	
25-01043	1	patricia dr-dead tree removal	3,650.00	01-413-000-450	Expenditure		81	1
				Contra Svs-MAGNET				
56419	08/12/25	PENDERGA PENDERGAST					3569	
25-01044	1	freezer pops	65.00	01-454-001-200	Expenditure		82	1
				Supplies				
56420	08/12/25	PSATS030 PSATS-CDL PROGRAM					3569	
25-01045	1	cdl alcohol/kendall	50.00	01-438-000-450	Expenditure		83	1
				Contracted Services				
56421	08/12/25	RGSASSOC RGS ASSOCIATES, INC					3569	
25-01046	1	5/31-7/05 services-landscape	8,833.47	01-414-002-367	Expenditure		84	1
				General Planning				
56422	08/12/25	RHOADS RHOADS ENERGY					3569	
25-01048	1	generator fuel	1,876.91	01-438-000-230	Expenditure		86	1
				Gasoline & Oil				
56423	08/12/25	ROBLITTL ROBERT E. LITTLE, INC.					3569	
25-01049	1	hose/chain saw service kit	35.53	01-438-000-260	Expenditure		87	1
				Small Tools & Equipment				
25-01049	2	tube/hoses/carburetor	132.70	01-438-000-260	Expenditure		88	1
				Small Tools & Equipment				
25-01049	3	vbelt w112 aramid	161.44	01-438-001-260	Expenditure		89	1
				Small Tools & Equip - Facilities				
25-01049	4	chain loops	95.36	01-438-001-260	Expenditure		90	1
				Small Tools & Equip - Facilities				
25-01049	5	plugs, fuel and air filters	190.34	01-438-001-260	Expenditure		91	1
				Small Tools & Equip - Facilities				
			<u>615.37</u>					
56424	08/12/25	SEMPERON SEMPERON					3569	
25-01052	1	twp	1,068.14	01-409-003-320	Expenditure		93	1
				Telephone				
25-01052	2	pw	105.04	01-409-001-320	Expenditure		94	1
				Telephone				
			<u>1,173.18</u>					

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num
PO #	Item	Description					Ref Seq Acct
56425	08/12/25	SHIRTMOR SHIRTS N MORE					3569
25-01054	1	township apparel - balance due	667.65	01-401-000-200 Supplies	Expenditure		96 1
56426	08/12/25	SNAPON01 SNAP-ON TOOLS					3569
25-01050	1	open finder w/case	303.00	01-438-000-260 Small Tools & Equipment	Expenditure		92 1
56427	08/12/25	STAPLES STAPLES					3569
25-01055	1	twp-office supplies	542.81	01-401-000-200 Supplies	Expenditure		97 1
25-01055	2	pd-office supplies	296.06	01-410-000-200 Supplies	Expenditure		98 1
			838.87				
56428	08/12/25	STEPHEQU STEPHENSON EQUIPMENT, INC.					3569
25-01057	1	display screen	2,056.77	01-438-000-200 Supplies	Expenditure		100 1
56429	08/12/25	STYER010 STYER PROPANE					3569
25-01059	1	milford 90 gal	125.80	01-409-004-231 Propane	Expenditure		102 1
56430	08/12/25	SUSMCKEN SUSAN and PAUL MCKENNA					3569
25-01056	1	summer concert series	500.00	01-454-001-201 Park & Rec Special Events	Expenditure		99 1
56431	08/12/25	SWEETWAT SWEETWATER NATURAL PRODUCTS LL					3569
25-01058	1	playground mulch-4yds	80.00	01-454-005-250 Repairs & Maint	Expenditure		101 1
56432	08/12/25	THEPRO20 PAVION					3569
25-01063	1	control sys test/chg batteries	639.32	01-409-003-450 Contracted Services	Expenditure		111 1
56433	08/12/25	TMOBILE T-MOBILE					3569
25-01060	1	admin	34.83	01-400-000-320 Telephone	Expenditure		103 1
25-01060	2	pd	313.38	01-410-000-320 Telephone	Expenditure		104 1
25-01060	3	codes	34.83	01-413-000-320 Telephone	Expenditure		105 1
25-01060	4	pw	69.63	01-438-000-320 Telephone	Expenditure		106 1
25-01060	5	pw ipad	21.34	01-438-000-322 Ipad Expense	Expenditure		107 1
25-01060	6	router	87.52	01-410-000-320 Telephone	Expenditure		108 1
			561.53				
56434	08/12/25	TONYSCH TONY SCHEIVERT					3569
25-01053	1	telephone reimbursement	100.00	01-400-000-320 Telephone	Expenditure		95 1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num
PO #	Item	Description					Ref Seq Acct
56435	08/12/25	TRANSK9 TRANSK9 USA, INC					3569
25-01061	1	airflow fan	163.50	01-410-000-235 Vehicle Maintenance	Expenditure		109 1
56436	08/12/25	VERIZ010 VERIZON					3569
25-01069	1	milford	145.69	01-409-004-320 Telephone	Expenditure		115 1
25-01069	2	pw	155.03	01-409-001-320 Telephone	Expenditure		116 1
			<u>300.72</u>				
56437	08/12/25	VERIZFIO VERIZON					3569
25-01070	1	ff	124.99	01-454-003-320 Telephone	Expenditure		117 1
56438	08/12/25	VERIZOSP VERIZON - SPECIAL PROJECTS					3569
25-01071	1	july services	217.14	01-434-000-450 Contracted Services	Expenditure		118 1
25-01071	2	august services	217.14	01-434-000-450 Contracted Services	Expenditure		119 1
			<u>434.28</u>				
56439	08/12/25	WESTV010 WEST VINCENT TOWNSHIP					3569
25-01072	1	compensation for 2022 to 2025	23,861.70	01-411-000-354 Insurance - Workers Com	Expenditure		120 1
56440	08/12/25	WIGGISHR WIGGINS SHREDDING					3569
25-01074	1	twp	137.35	01-409-001-450 Contracted Services	Expenditure		121 1
56441	08/12/25	ZMUID005 DANIEL ZMUIDA					3569
25-01076	1	reimburse for dog food	68.89	01-410-000-221 K-9	Expenditure		125 1

Report Totals	Paid	Void	Amount Paid	Amount Void
Checks:	63	0	150,509.86	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	<u>63</u>	<u>0</u>	<u>150,509.86</u>	<u>0.00</u>

Range of Checking Accts: GENERAL EFTS to GENERAL EFTS Range of Check Ids: 1231 to 1234
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void Ref Num
1231	08/04/25	BANKAMER BANK OF AMERICA	9,227.91	3562
1232	08/05/25	WEXBANK WEX BANK	10,880.96	3566
1233	08/25/25	AQUAP010 AQUA PENNSYLVANIA	8,030.24	3567
1234	08/08/25	HARTFORD THE HARTFORD	2,918.45	3574

Report Totals	Paid	Void	Amount Paid	Amount Void
Checks:	4	0	31,057.56	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	4	0	31,057.56	0.00

Range of Checking Accts: GENERAL EFTS to GENERAL EFTS Range of Check Ids: 1231 to 1234
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num
PO #	Item	Description					Ref Seq Acct
1231	08/04/25	BANKAMER BANK OF AMERICA					3562
25-01000	1	amazon prime	14.99	01-410-000-420 Dues/Subscription/Memb	Expenditure		1 1
25-01000	2	acme-jpa-day 1	34.25	01-410-000-340 Public Relations	Expenditure		2 1
25-01000	3	acme-jpa-day 2	18.65	01-410-000-340 Public Relations	Expenditure		3 1
25-01000	4	acme-jpa-day 4	4.99	01-410-000-340 Public Relations	Expenditure		4 1
25-01000	5	amazon-jpa certificate holders	34.99	01-410-000-340 Public Relations	Expenditure		5 1
25-01000	6	amazon-jpa frames	95.91	01-410-000-340 Public Relations	Expenditure		6 1
25-01000	7	amazon-k9 plaque for donors	79.98	01-410-000-340 Public Relations	Expenditure		7 1
25-01000	8	amazon-pa amplifier	259.40	01-410-000-327 Radio Equipment M & R	Expenditure		8 1
25-01000	9	amazon-pelican case w/o foam	106.95	01-410-000-260 Small Tools & Equipment	Expenditure		9 1
25-01000	10	amazon-work stool	169.99	01-410-000-260 Small Tools & Equipment	Expenditure		10 1
25-01000	11	axon-taser training recert	1,790.00	01-410-000-316 Training/Seminar	Expenditure		11 1
25-01000	12	chick-fil-a jpa gift cards	100.00	01-410-000-340 Public Relations	Expenditure		12 1
25-01000	13	dunkin-jpa gift cards	50.00	01-410-000-340 Public Relations	Expenditure		13 1
25-01000	14	dunkin-jpa day 1	41.97	01-410-000-340 Public Relations	Expenditure		14 1
25-01000	15	dunkin-jpa day 2	41.97	01-410-000-340 Public Relations	Expenditure		15 1
25-01000	16	dunkin-jpa day 3	41.97	01-410-000-340 Public Relations	Expenditure		16 1
25-01000	17	dunkin-jpa day 4	41.97	01-410-000-340 Public Relations	Expenditure		17 1
25-01000	18	dunkin-jpa day 5	41.97	01-410-000-340 Public Relations	Expenditure		18 1
25-01000	19	montgo coroner	1.00	01-410-000-316 Training/Seminar	Expenditure		19 1
25-01000	20	montgo coroner	35.00	01-410-000-316 Training/Seminar	Expenditure		20 1
25-01000	21	pa dog license	10.70	01-410-000-221 K-9	Expenditure		21 1
25-01000	22	precision explosive k9 training	288.00	01-410-000-221 K-9	Expenditure		22 1
25-01000	23	rons-jpa gift cards	100.00	01-410-000-340 Public Relations	Expenditure		23 1
25-01000	24	spatolas-jpa day 4	421.83	01-410-000-340 Public Relations	Expenditure		24 1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num	
PO #	Item	Description					Ref Seq	Acct
1231	BANK OF AMERICA	Continued						
25-01000	25	msft	1,254.34	01-407-000-450 Contracted Services	Expenditure		25	1
25-01000	26	adobe	83.56	01-407-000-220 Software	Expenditure		26	1
25-01000	27	ezpass	315.00	01-410-000-317 Parking & travel	Expenditure		27	1
25-01000	28	vista print-lift lever magnets	33.30	01-454-002-200 Supplies-Hickory	Expenditure		28	1
25-01000	29	vista prints-garden tour stop	119.72	01-454-001-201 Park & Rec Special Events	Expenditure		29	1
25-01000	30	zoho annual subscription	248.40	01-407-000-220 Software	Expenditure		30	1
25-01000	31	zip recruiter-open position	316.94	01-438-000-341 Advertisting	Expenditure		31	1
25-01000	32	amazon-barn key tage	3.99	01-454-005-200 Supplies	Expenditure		32	1
25-01000	33	amazon-computer cable strips	17.41	01-401-000-200 Supplies	Expenditure		33	1
25-01000	34	amazon-no smoking sign	28.98	01-454-005-200 Supplies	Expenditure		34	1
25-01000	35	amazon-garden tour stamp/pad	22.95	01-454-001-201 Park & Rec Special Events	Expenditure		35	1
25-01000	36	grandma zooks-barn flowers	119.23	01-454-005-200 Supplies	Expenditure		36	1
25-01000	37	forest bathing	60.00	01-401-000-200 Supplies	Expenditure		37	1
25-01000	38	staples-paper products	80.22	01-409-001-200 Township properties - supplies	Expenditure		38	1
25-01000	39	amazon-c fold towels	80.44	01-409-001-200 Township properties - supplies	Expenditure		39	1
25-01000	40	amazon-radiator cleaning wand	86.98	01-438-000-260 Small Tools & Equipment	Expenditure		40	1
25-01000	41	thorndale carpet-vinyl molding	60.00	01-409-001-250 Maint & Repair	Expenditure		41	1
25-01000	42	worldwide environmental	902.56	01-409-001-250 Maint & Repair	Expenditure		42	1
25-01000	43	pennboc conference-rl	285.00	01-413-000-316 Training/Seminar	Expenditure		43	1
25-01000	44	pennboc conference-ac	230.00	01-413-000-316 Training/Seminar	Expenditure		44	1
25-01000	45	dunkin-block party	41.97	01-454-001-202 Community Day	Expenditure		45	1
25-01000	46	rons-block party	559.68	01-454-001-202 Community Day	Expenditure		46	1
25-01000	47	montesano	109.07	01-401-000-200 Supplies	Expenditure		47	1
25-01000	48	acme-hist closet cleanout	12.99	01-438-000-200 Supplies	Expenditure		48	1
25-01000	49	roccos-traffic light accident	153.94	01-401-000-200 Supplies	Expenditure		49	1
25-01000	50	park road cafe-bridal coffee	115.01	01-401-000-200 Supplies	Expenditure		50	1

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Seq	Num Acct
PO #	Item	Description						
1231	BANK OF AMERICA	Continued						
25-01000	51	montesano-empc lunch	34.94	01-415-000-200 Supplies	Expenditure		51	1
25-01000	52	acme-garden tour waters	31.92	01-454-001-201 Park & Rec Special Events	Expenditure		52	1
25-01000	53	webstaurant store credit	7.11	01-454-001-202 Community Day	Expenditure		53	1
			<u>9,227.91</u>					
1232	08/05/25	WEXBANK WEX BANK						3566
25-01010	1	admin	203.68	01-401-000-230 Gasoline & Oil	Expenditure		1	1
25-01010	2	pd	4,839.13	01-410-000-230 Gasoline & Oil	Expenditure		2	1
25-01010	3	codes	271.03	01-413-000-230 Gasoline & Oil	Expenditure		3	1
25-01010	4	pw	3,602.50	01-438-000-230 Gasoline & Oil	Expenditure		4	1
25-01010	5	facilities	1,598.97	01-438-001-230 Gasoline & Oil - Facilities	Expenditure		5	1
25-01010	6	municipal authority gas	365.65	01-495-000-000 Expense Reclass	Expenditure		6	1
			<u>10,880.96</u>					
1233	08/25/25	AQUAP010 AQUA PENNSYLVANIA						3567
25-01011	1	217 hydrants	7,026.46	01-411-000-451 Hydrant expenses-Aqua	Expenditure		1	1
25-01011	2	31 hydrants	1,003.78	01-411-000-451 Hydrant expenses-Aqua	Expenditure		2	1
			<u>8,030.24</u>					
1234	08/08/25	HARTFORD THE HARTFORD						3574
25-01082	1	admin	381.36	01-401-000-156 Employee Benefit Expens	Expenditure		1	1
25-01082	2	pd	1,736.82	01-410-000-156 Employee Benefit Expense	Expenditure		2	1
25-01082	3	codes	200.35	01-413-000-156 Employee Benefit Expens	Expenditure		3	1
25-01082	4	pw	426.55	01-438-000-156 Employee Benefit Expense	Expenditure		4	1
25-01082	5	facilities	173.37	01-438-001-156 Employee Benefit Expense	Expenditure		5	1
			<u>2,918.45</u>					
<hr/>								
Report Totals			<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>		
	Checks:		4	0	31,057.56	0.00		
	Direct Deposit:		0	0	0.00	0.00		
	Total:		4	0	31,057.56	0.00		

Range of Checking Accts: SOLID WASTE to SOLID WASTE Range of Check Ids: 10759 to 10763
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void	Ref Num
10759	08/12/25	AJBL0010 A.J. BLOENSKI	89,737.08		3570
10760	08/12/25	CCSWA010 CCSWA	23,672.08		3570
10761	08/12/25	REBSTEIN REBECCA & ARTHUR STEINBERGER	55.01		3570
10762	08/12/25	TOTALREC TOTAL RECYCLE	4,796.01		3570
10763	08/12/25	TREAS010 TREASURER OF CHESTER COUNTY	2,576.67		3570

Report Totals	<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
Checks:	5	0	120,836.85	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	5	0	120,836.85	0.00

Range of Checking Accts: SOLID WASTE to SOLID WASTE Range of Check Ids: 10759 to 10763
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void Contract	Ref Num
PO #	Item	Description					Seq Acct
10759	08/12/25	AJBL0010 A.J. BLOSENSKI					3570
25-01014	1	august recycling services	28,973.58	05-427-000-460 Contracted Services - Recycling	Expenditure		1 1
25-01014	2	august trsh services	60,763.50	05-427-000-450 Contracted Services	Expenditure		2 1
			89,737.08				
10760	08/12/25	CCSWA010 CCSWA					3570
25-01018	1	sw-7/8-7/15	9,339.44	05-427-000-700 Tipping Fees	Expenditure		4 1
25-01018	2	sw-7/16-7/22	7,716.53	05-427-000-700 Tipping Fees	Expenditure		5 1
25-01018	3	sw-7/23-7/29	6,616.11	05-427-000-700 Tipping Fees	Expenditure		6 1
			23,672.08				
10761	08/12/25	REBSTEIN REBECCA & ARTHUR STEINBERGER					3570
25-01051	1	refund trash overpayment	55.01	05-495-000-000 Expense Reclass	Expenditure		7 1
10762	08/12/25	TOTALREC TOTAL RECYCLE					3570
25-01015	1	july services	4,796.01	05-427-000-725 Tipping Fees - Recycling	Expenditure		3 1
10763	08/12/25	TREAS010 TREASURER OF CHESTER COUNTY					3570
25-01068	1	hazardous waste event	2,576.67	05-427-000-810 Hazardous Waste Event	Expenditure		8 1

Report Totals		<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
	Checks:	5	0	120,836.85	0.00
	Direct Deposit:	0	0	0.00	0.00
	Total:	5	0	120,836.85	0.00

Range of Checking Accts: STORM WATER EFT to STORM WATER EFT Range of Check Ids: 200 200
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void	Ref Num
2	08/08/25	LOWES020 LOWES BUSINESS ACCOUNT	250.32		3564

Report Totals		<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
	Checks:	1	0	250.32	0.00
	Direct Deposit:	0	0	0.00	0.00
	Total:	1	0	250.32	0.00

Range of Checking Accts: STORM WATER EFT to STORM WATER EFT Range of Check Ids: 2 to 2
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor					Reconciled/Void	Ref Num
PO #	Item	Description	Amount Paid	Charge Account	Account Type	Contract	Ref Seq	Acct
25-01008	1	08/08/25 LOWES020 LOWES BUSINESS ACCOUNT concrete mix	225.96	08-446-000-250 Maintenance & repair	Expenditure		3564 1	1
25-01008	2	1/2" x 10ft steel	24.36	08-446-000-250 Maintenance & repair	Expenditure		2	1
			250.32					

Report Totals	Paid	Void	Amount Paid	Amount Void
Checks:	1	0	250.32	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	1	0	250.32	0.00

Range of Checking Accts: CAPITAL to CAPITAL Range of Check Ids: 2154 to 2157
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void	Ref Num
2154	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI	160.50		3571
2155	08/12/25	INTERPLA INTERIOR PLANNING GROUP	1,233.50	08/12/25 VOID	3571 (Reason: wrong amount)
2156	08/12/25	MIDAT010 MID ATLANTIC CONNECTIONS, INC.	350.00		3571
2157	08/12/25	INTERPLA INTERIOR PLANNING GROUP	1,232.50		3572

Report Totals	<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
Checks:	3	1	1,743.00	1,233.50
Direct Deposit:	0	0	0.00	0.00
Total:	3	1	1,743.00	1,233.50

Range of Checking Accts: CAPITAL to CAPITAL Range of Check Ids: 2154 to 2157
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor				Reconciled/Void	Ref Num
PO #	Item	Description	Amount Paid	Charge Account	Account Type	Contract	Ref Seq Acct
2154	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI					3571
25-01065	1	settlement/deed legal services	160.50	30-409-002-625	Expenditure		3 1
				Capital Construction - 128 Byers Road			
2155	08/12/25	INTERPLA INTERIOR PLANNING GROUP		(Void Reason: wrong amount)		08/12/25 VOID	3571
25-01035	1	interior design services	1,233.50	30-409-002-625	Expenditure		1 1
				Capital Construction - 128 Byers Road			
2156	08/12/25	MIDAT010 MID ATLANTIC CONNECTIONS, INC.					3571
25-01040	1	3 phase outlets & swith boxes	350.00	30-409-002-625	Expenditure		2 1
				Capital Construction - 128 Byers Road			
2157	08/12/25	INTERPLA INTERIOR PLANNING GROUP					3572
25-01081	1	capital construct 144 byers	1,232.50	30-409-002-625	Expenditure		1 1
				Capital Construction - 128 Byers Road			
Report Totals							
			<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>	
		Checks:	3	1	1,743.00	1,233.50	
		Direct Deposit:	0	0	0.00	0.00	
		Total:	3	1	1,743.00	1,233.50	

Range of Checking Accts: DEV ESCROW to DEV ESCROW Range of Check Ids: 839 to 841
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void Ref Num
839	08/12/25	ARROC010 ARRO CONSULTING, INC.	775.50	3573
840	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI	666.50	3573
841	08/12/25	RICHGRUB RICHARD GRUBB & ASSOCIATES	2,182.00	3573

Report Totals	<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
Checks:	3	0	3,624.00	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	3	0	3,624.00	0.00

Range of Checking Accts: DEV ESCROW to DEV ESCROW Range of Check Ids: 839 to 841
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Charge Account	Account Type	Reconciled/Void	Ref Num
PO #	Item	Description				Contract	Ref Seq Acct
839	08/12/25	ARROC010 ARRO CONSULTING, INC.					3573
25-01078	1	preserve phase II-toll	244.50	248-2-035	Project		1 1
				PMC SEWER PHASE III			
25-01078	2	preserve-mckee	86.00	248-035	Project		2 1
				THE PRESERVE @ MARSH CREEK SEW			
25-01078	3	preserve-mckee	71.25	248-035	Project		3 1
				THE PRESERVE @ MARSH CREEK SEW			
25-01078	4	prosperity lot 5c 2b	373.75	248-046	Project		4 1
				Prosperity Byers 5C 2B			
			775.50				
840	08/12/25	BUCKL010 BUCKLEY, BRION, MCGUIRE, MORRI					3573
25-01079	1	prosperity lot 5c 2b	666.50	248-046	Project		5 1
				Prosperity Byers 5C 2B			
841	08/12/25	RICHGRUB RICHARD GRUBB & ASSOCIATES					3573
25-01080	1	windsor parsonage	2,182.00	248-031	Project		6 1
				WINDSOR BAPTIST CHURCH			

Report Totals	Paid	Void	Amount Paid	Amount Void
Checks:	3	0	3,624.00	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	3	0	3,624.00	0.00

Range of Checking Accts: PAYROLL EFTS to PAYROLL EFTS Range of Check Ids: 833 to 833
Report Type: All Checks Report Format: Super Condensed Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor	Amount Paid	Reconciled/Void Ref Num
833	08/15/25	AFLAC010 AFLAC	820.94	3568

Report Totals	<u>Paid</u>	<u>Void</u>	<u>Amount Paid</u>	<u>Amount Void</u>
Checks:	1	0	820.94	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	1	0	820.94	0.00

Range of Checking Accts: PAYROLL EFTS to PAYROLL EFTS Range of Check Ids: 833 to 833
Report Type: All Checks Report Format: Detail Check Type: Computer: Y Manual: Y Dir Deposit: Y

Check #	Check Date	Vendor				Reconciled/Void	Ref Num
PO #	Item	Description	Amount Paid	Charge Account	Account Type	Contract	Ref Seq Acct
833	08/15/25	AFLAC010 AFLAC					3568
25-01012	1	payroll benefit deduction	820.94	01-221-000-000	Expenditure		1 1
				Benefit Deduction- Aflac (AFL)			

Report Totals	Paid	Void	Amount Paid	Amount Void
Checks:	1	0	820.94	0.00
Direct Deposit:	0	0	0.00	0.00
Total:	1	0	820.94	0.00



UPPER UWCHLAN TOWNSHIP

MEMORANDUM

TO: BOARD OF SUPERVISORS
FROM: Jill Bukata, Township Treasurer
RE: Status Update
DATE: August 12, 2025

Finance has worked on the following items during the month

- Received and processed 40 trash and 979 sewer payments (7/16/2025 to 8/5/2025)

Highlights of the July, 2025 financial statements

- The balance sheet remains strong with cash of nearly **\$10.0 million** - of that amount **approximately \$4.4 million** is not available for the routine operations of the Township as it is reserved for specific purposes, ie. Liquid Fuels and Act 209 for highways, Turf Field for replacement of the Turf Field, Capital Projects for capital improvements etc.
- Combined revenue and expense status (General Fund & Solid Waste Fund):
 - Percentage through the year 58.3%
 - YTD revenues \$ 6,735,150 66.0%
 - YTD expenses \$ 5,452,981 54.4%
 - YTD net income \$ 1,282,169 (before transfers)
 - YTD transfers out \$ 2,104,096
 - YTD net income (after transfers) \$ (821,927)**
 - Budgeted 2025 net income/(loss) \$ 175,367 (before transfers)
 - Budgeted 2025 net income (after) \$ (870,633) (after transfers)
- Earned income taxes received as of July 31 were nearly \$88,000 higher than at the same time last year and the Township is on target to exceed the 2025 budget.
- A few of the interfund receivable and liability accounts have high balances in July. This is because the Commonwealth deposited payment for the Green Light Go grant (\$183,000) into the Solid Waste Fund in July, but we account for it in the Capital Fund. Our portion of the costs were paid from the General Fund, and have since been transferred to the Capital Fund. Cash was transferred from Solid Waste to Capital and the necessary entries were made on August 1.
- July YTD reflects a loss due to the transfer of \$1,554,095 to Capital to purchase the 144 Byers Road property in early July. The Township will be reimbursed from Bond proceeds in October.

Upper Uwchlan Township

Treasurer's Report

Cash Balances As of July 31, 2025

General Fund

Meridian Bank	\$ 2,879,784
Meridian Bank - Payroll	85,639
Meridian Bank - ARPA Funds	21,615
Meridian Bank MMA - restricted	41,328
Meridian Bank-restricted-Meadow Creek	1,108,665
First Resource Bank	294,194
Fulton Bank	-
First Resource Bank - Turf Field	324,980
Petty cash	300
Total General Fund	4,756,505

Certificate of Deposit - 1/2/2026 (First Resource)	305,411
Certificate of Deposit - Fulton	276,078
	581,489

Total General Fund \$ 5,337,994

Solid Waste Fund

Meridian Bank - Solid Waste	553,314
First Resource Bank	835,580
Fulton Bank - Solid Waste	-
Total Solid Waste Funds	1,388,894

Total Solid Waste Fund 1,388,894

Liquid Fuels Fund

First Resource Bank/Fulton	763,506
Certificate of Deposit -	550,319
	1,313,825

Total Liquid Fuels Fund 1,313,825

Capital Projects Fund

First Resource Bank	140,138
PSDLAF	5,110
Fulton Bank - 2019 Bond Proceeds, ICS Sweep	-
	145,248

Total Capital Projects Fund 145,248

Act 209 Impact Fund

First Resource Bank	58,625
Certificate of Deposit - First Resource	1,109,530
	1,168,155

Total Act 209 Impact Fund 1,168,155

Water Resource Protection Fund

First Resource/Fulton	517,678
	517,678

Total Water Resource Protection Fund 517,678

Sewer Fund

PSDLAF	84
First Resource Bank/Fulton Bank	114,643
	114,727

Total Sewer Fund 114,727

Total - Upper Uwchlan Township	\$ 9,986,520
---------------------------------------	---------------------

Municipal Authority	\$ 5,948,364
----------------------------	---------------------

Developer's Escrow Fund	\$ 279,061
--------------------------------	-------------------

Upper Uwchlan Township
Schedule of Investments

As of July 31, 2025

	Institution	Amount Invested	Type of Investment	Maturity Date	Interest Rate	Market Value
<u>General Fund</u>						
General Fund	First Resource Bank	250,000.00	Certificate of Deposit	1/2/2026	4.980%	250,000.00
		46,788.88	Interest accrued			46,788.88
	Accrued interest - YTD	8,621.69				8,621.69
		<u>305,410.57</u>				<u>305,410.57</u>
General Fund	Fulton Bank	250,000.00	Certificate of Deposit	5/19/2026	3.100%	250,000.00
		19,092.72				19,092.72
	Accrued interest - YTD	6,985.23	Interest accrued			6,985.23
		<u>276,077.95</u>				<u>276,077.95</u>
<u>Liquid Fuels Fund</u>						
Liquid Fuels	Presence Bank	500,000.00	Certificate of Deposit	4/18/2026	3.580%	500,000.00
		35,680.07				35,680.07
	Accrued interest - YTD	14,729.19	Interest accrued			14,729.19
		<u>550,409.26</u>				<u>550,409.26</u>
<u>Act 209 Fund</u>						
Act 209	First Resource Bank	1,047,633.90	Certificate of Deposit	4/28/2026	3.900%	1,047,633.90
		33,266.36				33,266.36
	Accrued interest - YTD	28,630.00	Interest accrued			28,630.00
		<u>1,109,530.26</u>				<u>1,109,530.26</u>
<u>Sewer Fund - General Obligation Bonds (2014 Bonds)</u>						
Sewer Fund	PSDLAF	83.59	MAX account (MMF)			83.59
	PSDLAF	-	MAX account (MMF)	-	0.02%	-
		<u>83.59</u>				<u>83.59</u>
<u>Capital Fund</u>						
Capital Fund	PSDLAF	5,109.71	Collateralized CD Pool		0.100%	5,109.71
		<u>5,109.71</u>				<u>5,109.71</u>
	Fulton Bank - 2019	0.55				0.55
		<u>0.55</u>				<u>0.55</u>
Total Capital Fund		<u>5,110.26</u>				<u>5,110.26</u>

Upper Uwchlan Township
Accounts Receivable
As of July 31, 2025

Turf and Field Fees Receivable - Account 01-145-000-080 and 085

	Total Amount 7/31/2025	Total Amount 6/30/2025	Less than 30 days	31 - 60	61 - 90	Over 90 days	Over 180 days	Total
GEYA Soccer	-	1,070.00	-	-	-	-		-
Next Level Sports	40.00	40.00	-	-	-	40.00		40.00
Downingtown Dawgs Lacrose	220.00	220.00	-	-	-	220.00		220.00
Kirk Oates	50.00	50.00	-	-	-	50.00		50.00
Freedom LAX	-	-	-	-				-
Downingtown Rugby	-	-	-	-				-
Balance at July 31, 2025	<u>\$ 310.00</u>	<u>\$ 1,380.00</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 310.00</u>	<u>\$ -</u>	<u>\$ 310.00</u>

Upper Uwchlan Township
Accounts Receivable
As of July 31, 2025

Engineering and Legal Receivables - 01-145-000-200 and 300
Reimbursable CU Fees - 01-145-000-021

	Total Amount Due 7/31/2025	Total Amount Due 6/30/2025	Less than 30 days	31 - 60	61 - 90	Over 90 days	Over 180 days	Total
Alpha Phylte Fitness	827.55	827.55	-	-	-	827.55	-	827.55
Chester Co./Struble	2,722.00	2,722.00	-	2,722.00	-	-	-	2,722.00
Hankin	2,784.05	2,784.05	-	209.65	-	2,574.40	-	2,784.05
Montesano	-	-	-	-	-	-	-	-
Toll Brothers - Greenbridge Rd	-	-	-	-	-	-	-	-
Natural Lands Trust	-	-	-	-	-	-	-	-
Wertz Farm	-	-	-	-	-	-	-	-
Moser	642.50	642.50	-	-	-	642.50	-	642.50
Joshua Verner	-	-	-	-	-	-	-	-
Balance at July 31, 2025	\$ 6,976.10	\$ 6,976.10	\$ -	\$ 2,931.65	\$ -	\$ 4,044.45	\$ -	\$ 6,976.10

Upper Uwchlan Township
Accounts Receivable
As of July 31, 2025

Misc Accounts Receivable - Account 01-145-000-095

	Amount 7/31/2025	Amount 6/30/2025	Aging					Total
			Less than 30 days	30 days	60 days	90 days	180 days & over	
Franchise fees	45,000.00	45,000.00	-	45,000.00	-			45,000.00
Inspira	-	-	-	-				-
Chester County cell tower	2,000.00	6,000.00	2,000.00	-				2,000.00
Balance at July 31, 2025	<u>\$ 47,000.00</u>	<u>\$ 51,000.00</u>	<u>2,000.00</u>	<u>45,000.00</u>	<u>-</u>	<u>-</u>	<u>-</u>	<u>47,000.00</u>

Upper Uwchlan Township
General Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash

01-100-000-150	General Checking - First Resource Bank	\$	294,193.63
01-100-000-200	Meridian Bank		2,879,783.84
01-100-000-210	Meridian Bank - Payroll		85,639.23
01-100-000-220	Meridian Bank MMA - restricted		41,327.95
01-100-000-230	Meridian Bank - ARPA		21,615.16
01-100-000-250	Fulton Bank - Turf Field		-
01-100-000-255	First Resource Bank - Turf Field		324,979.86
01-100-000-260	Meridian Bank - Meadow Creek Lane		1,108,664.78
01-100-000-300	Petty Cash		300.00
	Total Cash		<u>4,756,504.45</u>

Investments

01-120-000-100	Certificate of Deposit - First Resource		305,410.57
01-120-000-110	Certificate of Deposit - Fulton		276,077.95
01-120-000-120	Certificate of Deposit - Meadow Creek		-
01-120-000-130	Certificate of Deposit - ARPA		-
			<u>581,488.52</u>

Accounts Receivable

01-145-000-020	Engineering Fees Receivable		3,504.65
01-145-000-021	Engineering Fees Receivable-CU		3,191.95
01-145-000-030	Legal Fees Receivable		279.50
01-145-000-040	R/E Taxes Receivable		-
01-145-000-050	Hydrant Tax Receivable		-
01-145-000-060	Domestic Relations Receivable		-
01-145-000-080	Field Fees Receivables		310.00
01-145-000-085	Turf Field Receivables		-
01-145-000-086	EIT Receivable		47,358.48
01-145-000-090	RE Transfer Tax Receivable		65,000.00
01-145-000-095	Misc accounts receivable		47,000.00
01-145-000-096	Traffic Signals Receivable		-
01-145-000-097	Advertising Fees Reimbursable		-
	Total Accounts Receivable		<u>166,644.58</u>

Other Current Assets

01-130-000-001	Due From Municipal Authority		136,583.92
01-130-000-003	Due From Liquid Fuels		-
01-130-000-004	Due from ACT 209 Fund		-
01-130-000-005	Due From Capital Fund		257,715.50
01-130-000-006	Due from Solid Waste Fund		-
01-130-000-007	Due from Water Resource Protection Fund		-
01-130-000-008	Due from the Sewer Fund		-
01-130-000-009	Due from Developer's Escrow Fund		-
01-131-000-000	Suspense Account		-
	Total Other Current Assets		<u>394,299.42</u>

Prepaid Expense

01-155-000-000	Prepaid expenses		-
	Total Prepaid Expense		<u>-</u>

Total Assets	\$	5,898,936.97
---------------------	-----------	---------------------

Upper Uwchlan Township
General Fund
Balance Sheet
As of July 31, 2025

LIABILITIES AND FUND BALANCE

Accounts Payable

01-200-000-000	Accounts Payable	-
01-252-000-001	Deferred Revenues	-
	Total Accounts Payable	-

Other Current Liabilities

01-199-000-000	Suspense Account	-
01-210-000-000	Payroll Liabilities	-
01-210-000-001	Federal Tax Withheld	-
01-211-000-000	FICA Tax Withheld	-
01-212-000-000	Earned Income Tax W/H	3,999.72
01-214-000-000	Non-Uniform Pension	-
01-214-000-100	NU Pension Plan #2	31.18
01-215-000-000	Police Pension Withheld	-
01-216-000-000	Domestic Relation W/H	(1,202.17)
01-217-000-000	State Tax Withheld	-
01-218-000-000	Police Association Dues	5,440.00
01-219-000-000	LST Tax Withheld	10.00
01-220-000-000	State Unemployment W/H	183.88
01-221-000-000	Benefit Deduction-Aflac	(665.84)
01-221-000-100	Benefit Deduction-Aflac After Tax	1,238.20
01-222-000-000	457 Contribution Deduction	-
01-223-000-000	Direct Deposit	-
01-224-000-000	Payroll Deduction Adjustments	-
01-239-000-001	Due to Municipal Authority	-
01-239-000-002	Due to MA Capital Fund	-
01-239-000-003	Due To Liquid Fuels	-
01-239-000-004	Due to Act 209 Fund	-
01-239-000-005	Due to Capital Fund	7,129.00
01-239-000-006	Due to Solid Waste Fund	-
01-239-000-007	Due to Water Resource Protection Fund	-
01-239-000-008	Due to Developer's Escrow Fund	-
01-258-000-000	Accrued Expenses	-
	Total Other Current Liabilities	16,163.97

Total Liabilities	\$	16,163.97
--------------------------	-----------	------------------

EQUITY

01-272-000-000	Opening Balance Equity	812,921.60
01-272-000-001	Retained Earnings	6,519,396.17
	Current Period Net Income (Loss)	(1,449,544.77)
	Total Equity	5,882,773.00

Total Fund Balance	\$	5,882,773.00
---------------------------	-----------	---------------------

Total Liabilities & Fund Balance	\$	5,898,936.97
---	-----------	---------------------

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
01-301-000-010	Current Real Estate Taxes	\$ 1,098,609.65	\$ 1,222,603.00	\$ (123,993.35)	89.9%
01-301-000-013	Real Estate Tax Refunds	-	(3,000.00)	3,000.00	0.0%
01-301-000-030	Delinquent Real Estate Taxes	16,796.08	30,000.00	(13,203.92)	56.0%
01-301-000-071	Hydrant Tax	66,950.66	68,000.00	(1,049.34)	98.5%
01-301-000-072	Delinquent Hydrant Taxes	-	500.00	(500.00)	0.0%
01-310-000-010	Real Estate Transfer Taxes	391,379.21	695,000.00	(303,620.79)	56.3%
01-310-000-020	Earned Income Taxes	3,091,945.26	4,750,000.00	(1,658,054.74)	65.1%
01-310-000-021	EIT commissions paid	(37,181.67)	(64,600.00)	27,418.33	57.6%
01-320-000-010	Building Permits	205,874.18	475,000.00	(269,125.82)	43.3%
01-320-000-011	Building Permits - credit card fees	(356.35)	-	(356.35)	#DIV/0!
01-320-000-020	Use & Occupancy Permit	6,660.00	12,000.00	(5,340.00)	55.5%
01-320-000-030	Sign Permits	-	100.00	(100.00)	0.0%
01-320-000-040	Contractors Permit	900.00	2,000.00	(1,100.00)	45.0%
01-320-000-050	Refinance Certification Fees	1,020.00	3,000.00	(1,980.00)	34.0%
01-321-000-080	Cable TV Franchise Fees	89,770.24	186,000.00	(96,229.76)	48.3%
01-331-000-010	Vehicle Codes Violation	39,175.25	75,000.00	(35,824.75)	52.2%
01-331-000-011	Reports/Fingerprints	675.00	2,000.00	(1,325.00)	33.8%
01-331-000-012	Solicitation Permits	85.00	500.00	(415.00)	17.0%
01-331-000-013	Donations for Police Department	19,200.00	1,000.00	18,200.00	1920.0%
01-331-000-050	Reimbursable Police Wages	933.28	5,000.00	(4,066.72)	18.7%
01-341-000-001	Interest Earnings	158,288.36	230,601.00	(72,312.64)	68.6%
01-342-000-001	Rental Property Income	14,000.00	24,000.00	(10,000.00)	58.3%
01-354-000-010	County Grants	-	-	-	#DIV/0!
01-354-000-020	State Grants	-	-	-	#DIV/0!
01-354-000-030	Police Grants	-	-	-	#DIV/0!
01-354-000-035	Other Grants	-	-	-	#DIV/0!
01-355-000-001	PURTA	-	6,500.00	(6,500.00)	0.0%
01-355-000-004	Alcoholic Beverage Tax	300.00	800.00	(500.00)	37.5%
01-355-000-005	State Aid, Police Pension	-	122,000.00	(122,000.00)	0.0%
01-355-000-006	State Aid, Non-Uniform Pension	-	60,000.00	(60,000.00)	0.0%
01-355-000-007	Foreign Fire Insurance Tax	-	110,000.00	(110,000.00)	0.0%
01-360-000-010	Vehicle Storage Fees	-	1,000.00	(1,000.00)	0.0%
01-361-000-030	Zoning/SubDivision Land Development	8,300.00	6,000.00	2,300.00	138.3%
01-361-000-032	Fees from Engineering	(3,794.37)	40,000.00	(43,794.37)	-9.5%
01-361-000-033	Admin Fees from Engineering	(115.22)	4,000.00	(4,115.22)	-2.9%
01-361-000-035	Admin Fees from Legal	-	1,000.00	(1,000.00)	0.0%
01-361-000-036	Legal Services Fees	66.00	30,000.00	(29,934.00)	0.2%
01-361-000-038	Sale of Maps & Books	180.00	250.00	(70.00)	72.0%
01-361-000-039	Fire Inspection Fees	-	2,000.00	(2,000.00)	0.0%
01-361-000-040	Fees from Engineering - CU	(111.25)	5,000.00	(5,111.25)	-2.2%
01-361-000-041	Property Inspection Fees	-	8,000.00	(8,000.00)	0.0%
01-361-000-042	Copies	-	100.00	(100.00)	0.0%
01-361-000-043	Fees from Traffic Signals Reimbursables	-	500.00	(500.00)	0.0%
01-361-000-044	Fees from Advertising Reimbursables	331.16	-	331.16	#DIV/0!
01-367-000-010	Recreation Donations	-	-	-	#DIV/0!
01-367-000-014	Pavillion Rental	230.00	500.00	(270.00)	46.0%
01-367-000-020	Tennis Fees	-	-	-	#DIV/0!
01-367-000-021	Field Programs	16,832.50	35,000.00	(18,167.50)	48.1%
01-367-000-025	Turf Field Fees	23,580.00	50,000.00	(26,420.00)	47.2%
01-367-000-030	Community Events Donations	18,500.00	20,000.00	(1,500.00)	92.5%
01-367-000-040	History Book Revenue	5.00	200.00	(195.00)	2.5%
01-367-000-045	Upland Farms Barn Rental Fees	15,265.00	35,000.00	(19,735.00)	43.6%
01-367-000-089	Donations for Park Equipment	-	-	-	#DIV/0!
01-380-000-001	Miscellaneous Revenue	18,046.46	20,000.00	(1,953.54)	90.2%
01-380-000-010	Insurance Reimbursement	-	10,000.00	(10,000.00)	0.0%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
01-392-000-008	Municipal Authority Reimbursement	171,694.60	272,195.00	(100,500.40)	63.1%
01-392-000-020	Transfer from Capital Fund	-	-	-	#DIV/0!
01-392-000-030	Transfer from Solid Waste Fund	-	-	-	#DIV/0!
01-395-000-000	Refund of Prior Year Expenses	-	-	-	#DIV/0!
Total Revenue		\$ 5,434,034.03	\$ 8,554,749.00	\$ (3,120,714.97)	63.5%

GENERAL GOVERNMENT

01-400-000-113	Supervisors Wages	\$ 7,312.50	\$ 9,750.00	(2,437.50)	75.0%
01-400-000-150	Payroll Tax Expense	559.44	746.00	(186.56)	75.0%
01-400-000-320	Telephone	1,118.33	2,000.00	(881.67)	55.9%
01-400-000-340	Public Relations	2,020.02	2,000.00	20.02	101.0%
01-400-000-341	Advertising	1,285.00	7,500.00	(6,215.00)	17.1%
01-400-000-342	Printing	4,303.61	5,000.00	(696.39)	86.1%
01-400-000-344	Community Notice	-	2,000.00	(2,000.00)	0.0%
01-400-000-350	Insurance-Bonding	2,807.00	3,166.00	(359.00)	88.7%
01-400-000-352	Insurance-Liability	19,360.20	25,814.00	(6,453.80)	75.0%
01-400-000-420	Dues/Subscriptions/Memberships	2,583.00	3,100.00	(517.00)	83.3%
01-400-000-460	Meeting & Conferences	6,123.50	6,000.00	123.50	102.1%
01-400-000-461	Bank Fees	145.00	2,000.00	(1,855.00)	7.3%
01-400-000-463	Misc expenses	7,028.73	2,000.00	5,028.73	351.4%
01-400-000-464	Wallace Twp. Tax Agreement	-	4,700.00	(4,700.00)	0.0%
		54,646.33	75,776.00	(21,129.67)	72.1%

EXECUTIVE

01-401-000-100	Administration Wages	362,883.69	641,362.00	(278,478.31)	56.6%
01-401-000-150	Payroll Tax Expense	27,767.80	49,064.00	(21,296.20)	56.6%
01-401-000-151	PSATS Unemployment Compensation	630.01	542.00	88.01	116.2%
01-401-000-156	Employee Benefit Expense	38,983.73	83,677.00	(44,693.27)	46.6%
01-401-000-157	ACA Fees	343.33	326.00	17.33	105.3%
01-401-000-159	Employer HSA Contribution	11,550.00	11,550.00	-	100.0%
01-401-000-160	Non-Uniform Pension	44,874.21	59,832.00	(14,957.79)	75.0%
01-401-000-165	Employer 457 Match	-	12,000.00	(12,000.00)	0.0%
01-401-000-174	Tuition Reimbursements	-	5,000.00	(5,000.00)	0.0%
01-401-000-181	Longevity Pay	4,200.00	9,150.00	(4,950.00)	45.9%
01-401-000-183	Overtime Wages	1,841.27	5,000.00	(3,158.73)	36.8%
01-401-000-200	Supplies	11,884.35	15,000.00	(3,115.65)	79.2%
01-401-000-205	Meals & Meal Allowances	-	200.00	(200.00)	0.0%
01-401-000-215	Postage	3,558.32	4,500.00	(941.68)	79.1%
01-401-000-230	Gasoline & Oil	844.23	2,200.00	(1,355.77)	38.4%
01-401-000-235	Vehicle Maintenance	543.93	1,000.00	(456.07)	54.4%
01-401-000-252	Repair & Maintenance	-	2,000.00	(2,000.00)	0.0%
01-401-000-316	Training & Seminars	4,239.59	8,000.00	(3,760.41)	53.0%
01-401-000-317	Parking/Travel	150.64	1,200.00	(1,049.36)	12.6%
01-401-000-322	Ipad Expenses	-	600.00	(600.00)	0.0%
01-401-000-352	Insurance - Liability	216.75	289.00	(72.25)	75.0%
01-401-000-353	Insurance-Vehicle	405.66	541.00	(135.34)	75.0%
01-401-000-354	Insurance-Workers Compensation	597.09	796.00	(198.91)	75.0%
01-401-000-420	Dues/Subscriptions/Memberships	3,297.89	5,100.00	(1,802.11)	64.7%
01-401-000-450	Contracted Services	10,064.09	61,685.00	(51,620.91)	16.3%
		528,876.58	980,614.00	(451,737.42)	53.9%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
AUDIT					
01-402-000-450	Contracted Services	30,300.00	30,300.00	-	100.0%
		30,300.00	30,300.00	-	100.0%
TAX COLLECTION					
01-403-000-110	Deputy Treasurer Expense	-	13,000.00	(13,000.00)	0.0%
01-403-000-200	Supplies	-	-	-	#DIV/0!
01-403-000-215	Postage	-	-	-	#DIV/0!
01-403-000-350	Insurance-Bonding	-	-	-	#DIV/0!
01-403-000-450	Contracted Services	13.14		13.14	#DIV/0!
		13.14	13,000.00	(12,986.86)	0.1%
LEGAL					
01-404-000-305	Reimbursable Legal Fees - CU	1,318.20	500.00	818.20	263.6%
01-404-000-310	Reimbursable Legal Fees	-	9,500.00	(9,500.00)	0.0%
01-404-000-311	Non Reimbursable Legal	15,901.25	65,200.00	(49,298.75)	24.4%
01-404-000-450	Contracted Services	2,714.00	5,000.00	(2,286.00)	54.3%
		19,933.45	80,200.00	(60,266.55)	24.9%
MUNICIPAL AUTHORITY ADMINISTRATOR					
01-406-000-100	Administrator Wages	47,551.43	86,400.00	(38,848.57)	55.0%
01-406-000-101	Employee Cost Transferred to MA	(49,427.82)	(93,100.00)	43,672.18	53.1%
01-406-000-150	Payroll Tax Expense	1,786.39	6,610.00	(4,823.61)	27.0%
01-406-000-151	PSATS Unemployment Compensation	90.00	90.00	-	100.0%
		0.00	-	0.00	#DIV/0!
TECHNOLOGY					
01-407-000-200	Supplies	-	2,000.00	(2,000.00)	0.0%
01-407-000-220	Software	45,479.39	88,748.00	(43,268.61)	51.2%
01-407-000-222	Hardware	-	12,000.00	(12,000.00)	0.0%
01-407-000-240	Web Page	17,544.82	7,000.00	10,544.82	250.6%
01-407-000-450	Contracted Services	42,293.62	65,000.00	(22,706.38)	65.1%
		105,317.83	174,748.00	(69,430.17)	60.3%
ENGINEERING					
01-408-000-305	Reimbursable Conditional Use	-	25,000.00	(25,000.00)	0.0%
01-408-000-310	Reimbursable Engineering	1,048.50	75,000.00	(73,951.50)	1.4%
01-408-000-311	Traffic Engineering	13,726.00	25,000.00	(11,274.00)	54.9%
01-408-000-313	Non Reimbursable Engineering	42,214.84	30,000.00	12,214.84	140.7%
01-408-000-365	Act 209	-			
01-408-000-366	Ordinance Update	-	4,500.00	(4,500.00)	0.0%
01-408-000-367	General Planning	21,878.72	10,000.00	11,878.72	218.8%
01-408-000-368	MS4 Expenses	11,126.07	-	11,126.07	#DIV/0!
01-408-000-369	Reimbursable Traffic Signals	-	-	-	#DIV/0!
01-408-000-370	Reimbursable Advertising	-	-	-	#DIV/0!
		89,994.13	169,500.00	(79,505.87)	53.1%
TOWNSHIP PROPERTIES					
<u>Public Works Building</u>					
01-409-001-200	Supplies	1,062.85	2,000.00	(937.15)	53.1%
01-409-001-231	Propane & heating - PW bldg	12,517.34	15,000.00	(2,482.66)	83.4%
01-409-001-250	Maint & Repair	5,565.07	20,500.00	(14,934.93)	27.1%
01-409-001-320	Telephone	2,023.50	4,000.00	(1,976.50)	50.6%
01-409-001-351	Insurance - property	24,938.64	33,252.00	(8,313.36)	75.0%
01-409-001-360	Utilities	5,980.97	10,000.00	(4,019.03)	59.8%
01-409-001-450	Contracted Services	7,696.69	12,000.00	(4,303.31)	64.1%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
<u>Township Building</u>					
01-409-003-101	Employee Cost Allocated	-	-	-	#DIV/0!
01-409-003-200	Supplies	914.51	4,000.00	(3,085.49)	22.9%
01-409-003-231	Propane & Heating Oil	-	5,000.00	(5,000.00)	0.0%
01-409-003-250	Maintenance & Repairs	817.91	3,000.00	(2,182.09)	27.3%
01-409-003-320	Telephone	7,250.34	7,000.00	250.34	103.6%
01-409-003-351	Insurance Property	29,095.08	38,793.00	(9,697.92)	75.0%
01-409-003-360	Utilities	15,879.93	25,000.00	(9,120.07)	63.5%
01-409-003-450	Contracted Services	26,226.41	40,000.00	(13,773.59)	65.6%
<u>Milford Road</u>					
01-409-004-200	Supplies		500.00	(500.00)	0.0%
01-409-004-231	Propane	(1,470.15)	2,000.00	(3,470.15)	-73.5%
01-409-004-250	Maintenance & Repairs	-	5,000.00	(5,000.00)	0.0%
01-409-004-320	Telephone	2,156.84	3,000.00	(843.16)	71.9%
01-409-004-351	Insurance - property	4,156.44	5,542.00	(1,385.56)	75.0%
01-409-004-360	Utilities	687.74	2,000.00	(1,312.26)	34.4%
01-409-004-450	Contracted Services	-	5,000.00	(5,000.00)	0.0%
		145,500.11	242,587.00	(97,086.89)	60.0%
POLICE EXPENSES					
01-410-000-100	Police Wages	1,132,293.38	2,009,903.00	(877,609.62)	56.3%
01-410-000-105	Police Wages - Part time	17,763.00	45,000.00	(27,237.00)	39.5%
01-410-000-110	Police Wages - WC reimbursement	-	-	-	#DIV/0!
01-410-000-150	Payroll Tax Expense	94,431.86	157,200.00	(62,768.14)	60.1%
01-410-000-151	PSATS Unemployment Compensation	1,743.74	1,800.00	(56.26)	96.9%
01-410-000-156	Employee Benefit Expense	248,476.58	401,259.00	(152,782.42)	61.9%
01-410-000-158	Medical Expense Reimbursements	4,587.11	12,000.00	(7,412.89)	38.2%
01-410-000-159	Employer HSA Contribution	52,250.00	52,250.00	-	100.0%
01-410-000-160	Pension Expense	315,675.68	418,855.00	(103,179.32)	75.4%
01-410-000-161	Pension Expense - Non Uniform	679.73	2,719.00	(2,039.27)	25.0%
01-410-000-165	Employer 457 Match	-	36,000.00	(36,000.00)	0.0%
01-410-000-174	Tuition Reimbursment	4,011.00	12,000.00	(7,989.00)	33.4%
01-410-000-181	Longevity Pay	11,400.00	34,100.00	(22,700.00)	33.4%
01-410-000-182	Education incentive	6,250.00	5,750.00	500.00	108.7%
01-410-000-183	Overtime - Patrol Functions	27,693.36	45,000.00	(17,306.64)	61.5%
01-410-000-184	Overtime - Shift Coverage	13,763.97	38,000.00	(24,236.03)	36.2%
01-410-000-185	Overtime - Holiday Worked	25,755.67	35,236.00	(9,480.33)	73.1%
01-410-000-186	PD Overtime Reimbursement	2,307.64	-	2,307.64	#DIV/0!
01-410-000-187	Courttime Wages	9,079.99	14,000.00	(4,920.01)	64.9%
01-410-000-190	ARPA - COVID Pay	-	-	-	#DIV/0!
01-410-000-191	Uniform/Boot Allowances	17,375.00	17,550.00	(175.00)	99.0%
01-410-000-200	Supplies	7,428.72	12,000.00	(4,571.28)	61.9%
01-410-000-215	Postage	9.90	750.00	(740.10)	1.3%
01-410-000-221	K-9	22,689.56	10,000.00	12,689.56	226.9%
01-410-000-230	Gasoline & Oil	32,396.37	50,000.00	(17,603.63)	64.8%
01-410-000-235	Vehicle Maintenance	23,558.86	20,000.00	3,558.86	117.8%
01-410-000-238	Clothing/Uniforms	14,408.19	17,000.00	(2,591.81)	84.8%
01-410-000-250	Maintenance & Repairs	-	-	-	#DIV/0!
01-410-000-260	Small Tools & Equipment	9,025.74	17,000.00	(7,974.26)	53.1%
01-410-000-311	Non-Reimburseable-Legal	-	-	-	#DIV/0!
01-410-000-316	Training/Seminar	11,038.28	28,838.00	(17,799.72)	38.3%
01-410-000-317	Parking & travel	1,944.82	1,000.00	944.82	194.5%
01-410-000-320	Telephone	5,335.25	7,000.00	(1,664.75)	76.2%
01-410-000-322	Ipad Expense	-	-	-	#DIV/0!

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
01-410-000-327	Radio Equipment M & R	689.95	1,000.00	(310.05)	69.0%
01-410-000-340	Public Relations	5,879.93	10,000.00	(4,120.07)	58.8%
01-410-000-342	Police Accreditation	2,010.00	4,000.00	(1,990.00)	50.3%
01-410-000-352	Insurance - Liability	11,633.25	15,511.00	(3,877.75)	75.0%
01-410-000-353	Insurance - Vehicles	1,986.75	2,649.00	(662.25)	75.0%
01-410-000-354	Insurance - Workers Compensation	37,616.19	50,155.00	(12,538.81)	75.0%
01-410-000-420	Dues/Subscriptions/Memberships	1,570.20	1,000.00	570.20	157.0%
01-410-000-450	Contracted Services	15,097.07	53,064.00	(37,966.93)	28.5%
01-410-000-740	Computer/Furniture	3,758.02	7,000.00	(3,241.98)	53.7%
		2,193,614.76	3,646,589.00	(1,452,974.24)	60.2%
FIRE/AMBULANCE					
01-411-000-354	Insurance - Workers Compensation	8,663.58	7,100.00	1,563.58	122.0%
01-411-000-420	Dues/Subscriptions/Memberships	-	-	-	#DIV/0!
01-411-000-450	Contracted Services	-	-	-	#DIV/0!
01-411-000-451	Hydrant expenses-Aqua	53,564.95	70,000.00	(16,435.05)	76.5%
01-411-001-001	Ludwigs	49,598.00	99,196.00	(49,598.00)	50.0%
01-411-001-002	Lionville	49,680.00	99,360.00	(49,680.00)	50.0%
01-411-001-003	Lionville Capital	-	-	-	100.0%
01-411-001-004	Glenmoore	5,717.50	11,435.00	(5,717.50)	50.0%
01-411-001-005	E. Brandywine	23,139.50	46,279.00	(23,139.50)	50.0%
01-411-001-006	Reimbursement - Uwchlan Township	-	2,300.00	(2,300.00)	0.0%
01-411-001-007	Reimbursement - East Brandywine Twp.	-	1,000.00	(1,000.00)	0.0%
01-411-002-530	Contributions-Fire Relief	-	110,000.00	(110,000.00)	0.0%
		190,363.53	446,670.00	(256,306.47)	42.6%
AMBULANCE					
01-412-000-540	Uwchlan Ambulance	42,500.00	85,000.00	(42,500.00)	50.0%
01-412-000-544	Uwchlan Ambulance - Capital	-	-	-	#DIV/0!
		42,500.00	85,000.00	(42,500.00)	50.0%
CODES ADMINISTRATION					
01-413-000-100	Code Administrator Wages	159,028.38	287,140.00	(128,111.62)	55.4%
01-413-000-150	Payroll Tax Expenses	12,298.42	21,966.00	(9,667.58)	56.0%
01-413-000-151	PSATS Unemployment Compensation	270.00	270.00	-	100.0%
01-413-000-156	Employee Benefit Expense	29,088.46	51,338.00	(22,249.54)	56.7%
01-413-000-159	Employer HSA Contribution	8,250.00	8,250.00	-	100.0%
01-413-000-160	Pension	24,268.05	32,357.00	(8,088.95)	75.0%
01-413-000-165	Employer 457 Match	-	6,000.00	(6,000.00)	0.0%
01-413-000-181	Longevity Pay	3,000.00	6,750.00	(3,750.00)	44.4%
01-413-000-183	Overtime	-	2,000.00	(2,000.00)	0.0%
01-413-000-200	Supplies	2,136.95	2,000.00	136.95	106.8%
01-413-000-230	Gasoline & Oil	1,922.46	3,600.00	(1,677.54)	53.4%
01-413-000-235	Vehicle Maintenance	-	1,200.00	(1,200.00)	0.0%
01-413-000-316	Training/Seminar	523.00	3,000.00	(2,477.00)	17.4%
01-413-000-317	Parking/Travel	-	400.00	(400.00)	0.0%
01-413-000-320	Telephone	714.33	3,100.00	(2,385.67)	23.0%
01-413-000-322	Ipad Expense	-	-	-	#DIV/0!
01-413-000-352	Insurance - Liability	216.75	289.00	(72.25)	75.0%
01-413-000-353	Insurance - Vehicle	405.66	541.00	(135.34)	75.0%
01-413-000-354	Insurance - Workers Compensation	597.09	796.00	(198.91)	75.0%
01-413-000-420	Dues/Subscriptions/Memberships	290.00	3,000.00	(2,710.00)	9.7%
01-413-000-450	Contracted Services	1,485.00	3,000.00	(1,515.00)	49.5%
01-413-000-460	Meetings & Conferences	-	1,000.00	(1,000.00)	0.0%
		244,494.55	437,997.00	(193,502.45)	55.8%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
PLANNING & ZONING					
01-414-001-116	Compensation	-	-	-	#DIV/0!
01-414-001-200	Supplies	-	500.00	(500.00)	0.0%
01-414-001-301	Court Reporter	237.50	1,500.00	(1,262.50)	15.8%
01-414-001-315	Legal Fees	-	3,000.00	(3,000.00)	0.0%
01-414-001-365	Comp Plan Update	4,341.72	25,000.00	(20,658.28)	17.4%
01-414-001-366	Ordinance Update	-	20,000.00	(20,000.00)	0.0%
01-414-001-367	General Planning	-	3,000.00	(3,000.00)	0.0%
01-414-001-368	Advertising	3,119.08	500.00	2,619.08	623.8%
01-414-001-451	ACT 209	-	-	-	#DIV/0!
		7,698.30	53,500.00	(45,801.70)	14.4%
VILLAGE CONCEPT					
01-414-002-367	General Planning	10,595.01	1,000.00	9,595.01	1059.5%
		10,595.01	1,000.00	9,595.01	1059.5%
ZONING					
01-414-003-100	Compensation	-	800.00	(800.00)	0.0%
01-414-003-301	Court Reporter	672.75	2,000.00	(1,327.25)	33.6%
01-414-003-315	Legal Fees	2,713.50	6,000.00	(3,286.50)	45.2%
01-414-003-450	Contracted Services	1,605.22	1,000.00	605.22	160.5%
		4,991.47	9,800.00	(4,808.53)	50.9%
EMERGENCY OPERATIONS					
01-415-000-200	Supplies	5,928.70	3,100.00	2,828.70	191.2%
01-415-000-260	Small Tools & Equipment	3,393.03	23,500.00	(20,106.97)	14.4%
01-415-000-316	Training/Seminar	247.35	1,250.00	(1,002.65)	19.8%
01-415-000-317	Parking/Travel	776.86	500.00	276.86	155.4%
01-415-000-320	Telephone	-	250.00	(250.00)	0.0%
01-415-000-330	Other Services/Charges	-	350.00	(350.00)	0.0%
01-415-000-420	Dues/subscriptions/memberships	442.00	250.00	192.00	176.8%
01-415-000-450	Contracted Services	35.99	500.00	(464.01)	7.2%
01-415-000-740	Computer/Office Equipment	1,028.80	750.00	278.80	137.2%
		11,852.73	30,450.00	(18,597.27)	38.9%
ANIMAL CONTROL/OTHER					
01-422-000-530	Contributions - SPCA	12,408.00	9,200.00	3,208.00	134.9%
01-422-000-601	Contributions - DARC	24,189.00	25,398.00	(1,209.00)	95.2%
01-422-000-603	Downingtown Senior Center	-	2,000.00	(2,000.00)	0.0%
01-422-000-605	Natural Lands Trust	-	-	-	
		36,597.00	36,598.00	(1.00)	100.0%
SIGNS					
01-433-000-200	Supplies	1,367.70	8,000.00	(6,632.30)	17.1%
01-433-000-450	Contracted Services	-	1,000.00	(1,000.00)	0.0%
		1,367.70	9,000.00	(7,632.30)	15.2%
SIGNALS					
01-434-000-450	Contracted Services	5,241.24	64,200.00	(58,958.76)	8.2%
		5,241.24	64,200.00	(58,958.76)	8.2%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
PUBLIC WORKS					
01-438-000-100	Public Works Wages	317,030.57	573,611.00	(256,580.43)	55.3%
01-438-000-101	Employee Cost Allocated	(13,457.87)	(24,988.00)	11,530.13	53.9%
01-438-000-150	Payroll Tax Expense	26,233.75	43,881.00	(17,647.25)	59.8%
01-438-000-151	PSATS Unemployment Compensation	720.00	720.00	-	100.0%
01-438-000-156	Employee Benefit Expense	98,770.10	183,658.00	(84,887.90)	53.8%
01-438-000-159	Employer HSA Contribution	21,450.00	21,450.00	-	100.0%
01-438-000-160	Pension	32,193.06	42,924.00	(10,730.94)	75.0%
01-438-000-165	Employer 457 Match	-	16,000.00	(16,000.00)	0.0%
01-438-000-181	Longevity	4,950.00	7,350.00	(2,400.00)	67.3%
01-438-000-183	Overtime Wages	22,236.95	26,000.00	(3,763.05)	85.5%
01-438-000-200	Supplies	34,329.74	40,000.00	(5,670.26)	85.8%
01-438-000-205	Meals & Meal Allowances	-	600.00	(600.00)	0.0%
01-438-000-230	Gasoline & Oil	19,440.32	45,000.00	(25,559.68)	43.2%
01-438-000-235	Vehicle Maintenance	31,194.50	30,000.00	1,194.50	104.0%
01-438-000-238	Uniforms	4,356.20	6,000.00	(1,643.80)	72.6%
01-438-000-245	Highway Supplies	14,124.47	35,000.00	(20,875.53)	40.4%
01-438-000-260	Small Tools & Equipment	4,099.81	16,000.00	(11,900.19)	25.6%
01-438-000-316	Training/Seminar	2,001.95	7,500.00	(5,498.05)	26.7%
01-438-000-317	Parking & travel	52.50	800.00	(747.50)	6.6%
01-438-000-320	Telephone	712.01	4,300.00	(3,587.99)	16.6%
01-438-000-322	Ipad Expense	181.97	1,200.00	(1,018.03)	15.2%
01-438-000-341	Advertising	-	-	-	#DIV/0!
01-438-000-342	Accreditation	-	5,000.00	(5,000.00)	0.0%
01-438-000-352	Insurance - Liability	1,135.20	1,514.00	(378.80)	75.0%
01-438-000-353	Vehicle Insurance	1,622.64	2,164.00	(541.36)	75.0%
01-438-000-354	Insurance - Workers Compensation	10,747.50	14,330.00	(3,582.50)	75.0%
01-438-000-420	Dues and Subscriptions	410.00	500.00	(90.00)	82.0%
01-438-000-450	Contracted Services	2,734.84	87,100.00	(84,365.16)	3.1%
01-438-000-463	Miscellaneous	-	-	-	#DIV/0!
01-438-000-720	Road Resurfacing	-	10,000.00	(10,000.00)	0.0%
		637,270.21	1,197,614.00	(560,343.79)	53.2%
<u>Public Works - Facilities Division</u>					
01-438-001-100	Wages	150,736.30	266,399.00	(115,662.70)	56.6%
01-438-001-101	Employee Costs Allocated	(129,293.86)	(242,429.00)	113,135.14	53.3%
01-438-001-150	Payroll Tax Expense	12,493.29	20,380.00	(7,886.71)	61.3%
01-438-001-151	PSATS Unemployment Compensation	552.73	630.00	(77.27)	87.7%
01-438-001-156	Employee Benefit Expense	17,675.00	45,839.00	(28,164.00)	38.6%
01-438-001-159	Employer HSA Contribution	9,900.00	8,250.00	1,650.00	120.0%
01-438-001-160	Pension Expense	12,606.27	16,808.00	(4,201.73)	75.0%
01-438-001-165	Employer 457 Match	-	6,000.00	(6,000.00)	0.0%
01-438-001-174	Tuition Reimbursement	-	-	-	#DIV/0!
01-438-001-181	Longevity	2,700.00	4,050.00	(1,350.00)	66.7%
01-438-001-183	Overtime Wages	9,116.44	8,000.00	1,116.44	114.0%
01-438-001-200	Supplies	1,967.80	2,500.00	(532.20)	78.7%
01-438-001-230	Gasoline & Oil	5,796.42	18,000.00	(12,203.58)	32.2%
01-438-001-235	Vehicle Maintenance	7,055.78	15,000.00	(7,944.22)	47.0%
01-438-001-238	Uniforms	-	1,500.00	(1,500.00)	0.0%
01-438-001-260	Small Tools & Equipment	70.97	500.00	(429.03)	14.2%
01-438-001-316	Training & Seminars	-	1,600.00	(1,600.00)	0.0%
01-438-001-352	Insurance - Liability	1,135.20	1,514.00	(378.80)	75.0%
01-438-001-353	Insurance - Vehicles	1,622.64	2,164.00	(541.36)	75.0%
01-438-001-354	Insurance - Workers Compensation	7,164.99	9,553.00	(2,388.01)	75.0%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
01-438-001-450	Contracted Services	70.00	-	70.00	#DIV/0!
		111,369.97	186,258.00	(74,888.03)	59.8%
ROAD CONSTRUCTION					
01-439-000-752	East West Link	-	-	-	#DIV/0!
		-	-	-	#DIV/0!
PARK & RECREATION					
<i>Parks - General</i>					
01-454-000-150	Scholarships for Youth Groups	-	-	-	#DIV/0!
01-454-001-101	Park wages allocation	129,293.86	242,429.00	(113,135.14)	53.3%
01-454-001-200	Supplies	3,603.58	18,000.00	(14,396.42)	20.0%
01-454-001-201	Park & Rec Special Events	3,426.16	21,000.00	(17,573.84)	16.3%
01-454-001-202	Community Day	25,663.43	30,000.00	(4,336.57)	85.5%
01-454-001-230	Gasoline & Oil	-	-	-	#DIV/0!
01-454-001-235	Vehicle Maintenance	-	1,500.00	(1,500.00)	0.0%
01-454-001-250	Maintenance & Repairs	1,007.65	1,000.00	7.65	100.8%
01-454-001-260	Small Tools & Equipment	38.69	6,500.00	(6,461.31)	0.6%
01-454-001-316	Training/Seminars	-	1,000.00	(1,000.00)	0.0%
01-454-001-340	Public Relations	-	-	-	#DIV/0!
01-454-001-354	Insurance - Workers Compensation	2,985.39	3,981.00	(995.61)	75.0%
01-454-001-420	Dues/Subscriptions/Memberships	-	300.00	(300.00)	0.0%
01-454-001-427	Waste Disposal	-	1,500.00	(1,500.00)	0.0%
01-454-001-450	Contracted Services	500.00	-	500.00	#DIV/0!
		166,518.76	327,210.00	(160,691.24)	50.9%
HICKORY PARK					
01-454-002-200	Supplies-Hickory	2,854.74	6,000.00	(3,145.26)	47.6%
01-454-002-231	Propane	1,043.72	2,000.00	(956.28)	52.2%
01-454-002-250	Maintenance & Repairs	9,427.16	8,000.00	1,427.16	117.8%
01-454-002-312	Engineering Fees	-	-	-	#DIV/0!
01-454-002-351	Insurance-Property	8,312.88	11,084.00	(2,771.12)	75.0%
01-454-002-360	Utilities	1,965.44	5,000.00	(3,034.56)	39.3%
01-454-002-450	Contracted Services	22,822.94	38,000.00	(15,177.06)	60.1%
		46,426.88	70,084.00	(23,657.12)	66.2%
FELLOWSHIP FIELDS					
01-454-003-200	Supplies	46.03	3,000.00	(2,953.97)	1.5%
01-454-003-250	Maintenance & Repairs	913.80	10,000.00	(9,086.20)	9.1%
01-454-003-312	Engineering Fees	-	-	-	#DIV/0!
01-454-003-320	Telephone	874.93	2,500.00	(1,625.07)	35.0%
01-454-003-351	Insurance Property	8,312.88	11,084.00	(2,771.12)	75.0%
01-454-003-360	Utilities	4,389.63	12,000.00	(7,610.37)	36.6%
01-454-003-450	Contracted Services	21,896.00	20,000.00	1,896.00	109.5%
		36,433.27	58,584.00	(22,150.73)	62.2%
LARKINS FIELD					
01-454-004-200	Supplies-Larkins	-	500.00	(500.00)	0.0%
01-454-004-250	Maintenance & Repair	64.00	500.00	(436.00)	12.8%
01-454-004-312	Engineering Fee-Larkins	-	-	-	#DIV/0!
01-454-004-450	Contracted Services	3,290.00	3,000.00	290.00	109.7%
		3,354.00	4,000.00	(646.00)	83.9%

Upper Uwchlan Township
General Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
UPLAND FARMS					
01-454-005-200	Supplies	2,964.96	7,500.00	(4,535.04)	39.5%
01-454-005-231	Propane & Heating Oil	3,197.77	2,500.00	697.77	127.9%
01-454-005-250	Repairs & Maintenance	7,298.85	20,000.00	(12,701.15)	36.5%
01-454-005-351	Insurance - Building	8,312.88	11,084.00	(2,771.12)	75.0%
01-454-005-360	Utilities	14,985.17	25,000.00	(10,014.83)	59.9%
01-454-005-450	Contracted Services	10,974.36	25,000.00	(14,025.64)	43.9%
01-454-005-513	Engineering Fees	-	-	-	#DIV/0!
		47,733.99	91,084.00	(43,350.01)	52.4%
	Total Parks and Recreation	300,466.90	550,962.00	(250,495.10)	54.5%
LIBRARY and EAC					
01-455-000-450	EAC Contracted Services	5,353.81	10,000.00	(4,646.19)	53.5%
01-456-000-530	Library Contributions	-	5,000.00	(5,000.00)	0.0%
		5,353.81	15,000.00	(9,646.19)	35.7%
HISTORICAL COMMISSIONS					
01-459-000-200	Supplies	624.13	2,500.00	(1,875.87)	25.0%
01-459-000-320	Telephone	-	250.00	(250.00)	0.0%
01-459-000-450	Contracted Services	500.00	14,500.00	(14,000.00)	3.4%
		1,124.13	17,250.00	(16,125.87)	6.5%
	Total Expenditures Before Operating Transfers	4,779,482.88	8,554,613.00	(3,775,130.12)	55.9%
	Excess of Revenues over Expenses Before Operating Transfers	654,551.15	136.00	654,415.15	481287.6%
OPERATING TRANSFERS					
	Transfer from Turf Field Cash account	-	-	-	#DIV/0!
01-492-000-030	Transfer to Capital Projects Fund	2,104,095.92	860,000.00	1,244,095.92	244.7%
01-492-000-031	Transfer to Solid Waste Fund	-	-	-	#DIV/0!
01-492-000-033	Transfer to Developer's Escrow Fund	-	-	-	#DIV/0!
01-492-000-034	Transfer to Act 209 Fund	-	-	-	#DIV/0!
01-492-000-035	Transfer to Municipal Authority	-	-	-	#DIV/0!
01-492-000-036	Transfer to Water Resource Protection Fund	-	186,000.00	(186,000.00)	0.0%
		2,104,095.92	1,046,000.00	1,058,095.92	201.2%
	Total Expenditures after Operating Transfers	6,883,578.80	9,600,613.00	(2,717,034.20)	71.7%
EXCESS OF REVENUES OVER EXPENSES		\$ (1,449,544.77)	\$ (1,045,864.00)	\$ (403,680.77)	138.6%

Upper Uwchlan Township
Liquid Fuels Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash		
04-100-000-100	Cash - First Resource Bank	763,505.74
	Total Cash	<u>763,505.74</u>
Investments		
04-120-000-100	Certificate of Deposit - Presence Bank	550,319.26
	Total Certificates of Deposit	<u>550,319.26</u>
Other Current Assets		
04-130-000-001	Due from General Fund	-
04-130-000-002	Due from Capital Fund	-
04-132-000-000	Other Assets	-
	Total Other Current Assets	<u>-</u>
Total Assets		\$ 1,313,825.00

LIABILITIES AND FUND BALANCE

Accounts Payable		
04-200-000-000	Accounts Payable	-
04-258-000-000	Accrued Expenses	-
	Total Accounts Payable	<u>-</u>
Other Current Liabilities		
	Other Liabilities	
04-230-000-010	Due To General Fund	-
04-252-000-000	Deferred Revenues	-
	Total Other Current Liabilities	<u>-</u>
Total Liabilities		-
Equity		
04-272-000-001	Opening Balance Equity	192,790.66
04-272-000-002	Retained Earnings	914,013.60
04-272-000-003	Transfer from Other Funds	-
	Unrestricted Net Assets	-
	Current Period Net Income (Loss)	207,020.74
	Total Equity	<u>1,313,825.00</u>
Total Fund Balance		\$ 1,313,825.00
Total Liabilities & Fund Balance		\$ 1,313,825.00

Upper Uwchlan Township
Liquid Fuels Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
04-341-000-000	Interest Earnings	\$ 34,710.20	\$ 50,000.00	(15,289.80)	69%
04-355-000-002	Motor Fuel Vehicle Taxes	419,263.94	390,888.00	28,375.94	107%
04-389-000-001	Winter Snow Agreement	-	600.00	(600.00)	0%
04-389-000-002	Turnback Maintenance	-	14,520.00	(14,520.00)	0%
	Total Revenues	\$ 453,974.14	\$ 456,008.00	\$ (2,033.86)	177%
EXPENDITURES					
Equipment					
04-400-000-074	Equipment Purchases	146,657.00	245,000.00	(98,343.00)	60%
	Total Equipment	146,657.00	245,000.00	(98,343.00)	60%
Snow					
04-432-000-239	Snow & Ice Supplies	85,546.40	77,250.00	8,296.40	111%
04-432-000-250	Vehicle Maintenance & Repair		4,120.00	(4,120.00)	0%
04-432-000-450	Snow & Ice Contracted Services	14,750.00	24,720.00	(9,970.00)	60%
	Total Snow	100,296.40	106,090.00	(5,793.60)	170%
Road Projects					
04-438-000-239	Road Project Supplies	-	31,930.00	(31,930.00)	0%
04-438-000-450	Road Project Contracted Services	-	-	-	#DIV/0!
	Total Road Projects	-	31,930.00	(31,930.00)	#DIV/0!
Highway Construction					
04-439-001-250	Resurfacing		565,813.00	(565,813.00)	0%
04-439-002-250	Base Repairs - Pa. Drive	-	6,180.00	(6,180.00)	0%
	Total Highway Construction	-	571,993.00	(571,993.00)	-
	Total Expenditures	\$ 246,953.40	\$ 955,013.00	\$ (708,059.60)	26%
	Excess of Revenues over Expenditures	\$ 207,020.74	\$ (499,005.00)	\$ 706,025.74	-41%

Upper Uwchlan Township
Solid Waste Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash

05-100-000-010	Meridian Bank	\$	553,314.20
05-100-000-120	Cash - First Resources		835,579.64
	Total Cash		<u>1,388,893.84</u>

Accounts Receivable

05-130-000-045	WIPP Receivable from MA		14,465.53
05-145-000-010	Solid Waste Receivable		104,584.83
05-145-000-095	Misc. Receivable		-
			<u>119,050.36</u>

Other Current Assets

05-130-000-010	Due from General Fund		-
05-130-000-020	Due from Capital Fund		-
05-130-000-050	Due from Municipal Authority		42.50
05-155-000-010	Prepaid Attorney Fees		-
	Other Assets		-
	Total Other Current Assets		<u>42.50</u>

Total Assets	\$	1,507,986.70
---------------------	-----------	---------------------

LIABILITIES AND FUND BALANCE

Accounts Payable

05-200-000-020	Accounts Payable		-
05-258-000-000	Accrued Expenses		-
	Total Accounts Payable		<u>-</u>

Other Current Liabilities

05-239-000-010	Due To General Fund		-
05-239-000-020	Due To Capital Fund		183,749.64
05-239-000-030	Due to Liquid Fuels Fund		-
05-239-000-040	Due to Act 209 Fund		-
05-239-000-050	Due to Municipal Authority		-
05-239-000-055	Due to Water Resource Protection Fund		-
05-252-000-010	Deferred Revenues		77,668.23
	Total Other Current Liabilities		<u>261,417.87</u>

Total Liabilities	261,417.87
--------------------------	-------------------

Equity

05-272-000-001	Opening Balance Equity		984,603.98
05-272-000-004	Unrestricted Net Assets		(288,318.91)
	Current Period Net Income (Loss)		550,283.76
	Total Equity		<u>1,246,568.83</u>

Total Fund Balance	\$	1,246,568.83
---------------------------	-----------	---------------------

Total Liabilities & Fund Balance	\$	1,507,986.70
---	-----------	---------------------

Upper Uwchlan Township
Solid Waste Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
05-341-000-000	Interest Earnings	\$ 25,651.38	\$ 36,161.00	(10,509.62)	71%
05-364-000-010	Solid Waste Income	1,251,501.25	1,583,776.00	(332,274.75)	79%
05-364-000-015	Resident Refunds	-	(2,000.00)	2,000.00	0%
05-364-000-020	Recycling Income	52,717.38	5,000.00	47,717.38	1054%
05-364-000-025	Hazardous Waste Event	1,911.26	2,000.00	(88.74)	96%
05-364-000-030	Leaf Bags Sold	95.00	500.00	(405.00)	19%
05-364-000-032	Mattress Tags Sold	1,380.00			
05-364-000-035	Scrap Metal Sold	198.80	500.00	(301.20)	40%
	Equipment Purchase Grant (Pa.)	-	-	-	#DIV/0!
05-364-000-040	Performance Grant	-	25,000.00	(25,000.00)	0%
05-380-000-000	Misc Income	-	-	-	#DIV/0!
	Total Revenues	\$ 1,333,455.07	\$ 1,650,937.00	\$ (318,861.93)	#DIV/0!
EXPENDITURES					
Operations					
05-427-000-101	Employee Cost Allocation	13,457.87	24,988.00	(11,530.13)	54%
05-427-000-150	Bank Fees	-	200.00	(200.00)	0%
05-427-000-200	Supplies	-	2,000.00	(2,000.00)	0%
05-427-000-210	Utility Billing Expenses	1,840.01	3,000.00	(1,159.99)	61%
05-427-000-220	Postage	2,092.91	2,300.00	(207.09)	91%
05-427-000-230	Toters	-	36,069.00	(36,069.00)	0%
05-427-000-314	Legal Fees	4,388.49	10,000.00	(5,611.51)	44%
05-427-000-316	Training & Seminars	-	500.00	(500.00)	0%
05-427-000-420	Dues/Subscriptions/Memberships	-	200.00	(200.00)	0%
05-427-000-450	Contracted Services - Solid Waste	404,830.00	702,563.00	(297,733.00)	58%
05-427-000-460	Contracted Services - Recycling	192,277.80	334,888.00	(142,610.20)	57%
05-427-000-700	Tipping Fees	144,034.79	280,000.00	(135,965.21)	51%
05-427-000-725	Tipping Fees - Recycling	20,249.44	60,000.00	(39,750.56)	34%
05-427-000-800	Recycling Disposal	-	9,000.00	(9,000.00)	0%
05-427-000-805	Electronic Waste Event	-	8,000.00	(8,000.00)	0%
05-427-000-810	Hazardous Waste Event	-	2,000.00	(2,000.00)	0%
	Total Operations	783,171.31	1,475,708.00	(692,536.69)	53%
Operating Transfers					
05-492-000-030	Transfer to Capital Fund	-	-	-	#DIV/0!
	Transfer to General Fund	-	-	-	#DIV/0!
	Total Operating Transfers	-	-	-	#DIV/0!
	Total Expenditures	\$ 783,171.31	\$ 1,475,708.00	\$ (692,536.69)	53%
	Excess of Revenues over Expenditures	\$ 550,283.76	\$ 175,229.00	\$ 373,674.76	314%

Upper Uwchlan Township
Water Resource Protection Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash

08-100-000-200	Cash - First Resources	\$ 517,677.73
	Total Cash	<u>517,677.73</u>

Other Current Assets

08-130-000-010	Due from General Fund	-
08-130-000-020	Due from Municipal Authority	-
08-130-000-030	Due from Solid Waste Fund	-
08-145-000-095	Misc. Receivable	<u>-</u>
	Total Other Current Assets	-

Total Assets	\$ 517,677.73
---------------------	----------------------

LIABILITIES AND FUND BALANCE

Accounts Payable

08-200-000-000	Accounts Payable	-
08-258-000-000	Accrued Expenses	<u>-</u>
	Total Accounts Payable	-

Other Current Liabilities

08-230-000-010	Due To General Fund	-
08-230-000-020	Due to Municipal Authority	-
08-230-000-030	Due to Capital Fund	-
	Deferred Revenues	<u>-</u>
	Total Other Current Liabilities	-

Total Liabilities	-
--------------------------	----------

Equity

08-272-000-100	Unrestricted Net Assets	677,739.14
08-272-000-200	Restricted Net Assets	(153,468.80)
	Current Period Net Income (Loss)	<u>(6,592.61)</u>
	Total Equity	517,677.73

Total Fund Balance	\$ 517,677.73
---------------------------	----------------------

Total Liabilities & Fund Balance	\$ 517,677.73
---	----------------------

Upper Uwchlan Township
Water Resource Protection Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
08-341-000-010	Interest Earnings	\$ 14,099.60	\$ 12,500.00	1,599.60	113%
08-351-000-010	Federal Grants	-	-	-	#DIV/0!
08-354-000-010	County Grants	-	-	-	#DIV/0!
08-354-000-020	State Grants	-	275,000.00	(275,000.00)	0%
08-361-000-100	Water Resource Protection Fees	-	186,000.00	(186,000.00)	0%
08-392-000-010	Transfer from the General Fund	-	-	-	#DIV/0!
08-392-000-020	Transfer from Municipal Authority	-	-	-	#DIV/0!
08-395-000-000	Refund of Prior Year Expenditures	-	-	-	#DIV/0!
	Miscellaneous Revenue	-	-	-	#DIV/0!
				-	#DIV/0!
Total Revenues		\$ 14,099.60	\$ 473,500.00	\$ (459,400.40)	3%
EXPENDITURES					
Operations					
08-404-000-310	Wage Allocation	-	-	-	#DIV/0!
08-404-000-311	Legal Fees	-	-	-	#DIV/0!
08-406-000-010	Grant Application Fees	-	-	-	#DIV/0!
08-406-000-340	Public Relations	-	-	-	#DIV/0!
08-408-000-010	Engineering	-	5,000.00	(5,000.00)	0%
08-408-000-020	Feasibility Studies	-	-	-	#DIV/0!
08-420-000-035	Permits	2,500.00	-	2,500.00	#DIV/0!
08-420-000-260	Small Tools & Equipment	1,678.08	1,500.00	178.08	112%
08-446-000-101	Allocated Wages	-	-	-	#DIV/0!
08-446-000-200	Supplies	900.00	27,112.00	(26,212.00)	3%
08-446-000-230	Gasoline & Oil	-	2,400.00	(2,400.00)	0%
08-446-000-235	Vehicle maintenance	7,555.65	10,000.00	(2,444.35)	76%
08-446-000-250	Maintenance & Repair	4,996.09	25,600.00	(20,603.91)	20%
08-446-000-316	Training & Seminars	-	2,000.00	(2,000.00)	0%
08-446-000-450	Contracted Services	3,062.39	20,000.00	(16,937.61)	15%
08-446-000-600	Construction	-	-	-	#DIV/0!
08-446-004-600	Construction - Upland Farms	-	-	-	#DIV/0!
08-446-005-600	Construction - Marsh Creek	-	-	-	#DIV/0!
08-446-001-250	Maintenance & Repair - MA	-	-	-	#DIV/0!
08-446-001-600	Construction - MA	-	-	-	#DIV/0!
Total Operations		20,692.21	93,612.00	(72,919.79)	22%
Operating Transfers					
Transfer to General Fund		-	-	-	#DIV/0!
Total Operating Transfers		-	-	-	#DIV/0!
Total Expenditures		\$ 20,692.21	\$ 93,612.00	\$ (72,919.79)	22%
Excess of Revenues over Expenditures		\$ (6,592.61)	\$ 379,888.00	\$ (386,480.61)	-2%

Upper Uwchlan Township
Act 209 Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash		
09-100-000-020	Cash - First Resources	\$ 58,624.61
	Total Cash	<u>58,624.61</u>
Investments		
09-120-000-100	Certificate of Deposit	1,109,530.26
	Total Certificates of Deposit	<u>1,109,530.26</u>
Other Current Assets		
09-130-000-000	Due from General Fund	-
09-130-000-001	Due from Capital Fund	-
09-191-000-000	Other Assets	-
	Reserve - Accounts Receivable	-
	Total Other Current Assets	<u>-</u>
Total Assets		\$ 1,168,154.87

LIABILITIES AND FUND BALANCE

Accounts Payable		
09-200-000-000	Accounts Payable	-
09-258-000-000	Accrued Expenses	-
	Total Accounts Payable	<u>-</u>
Other Current Liabilities		
09-297-000-000	Other Liabilities	-
09-297-000-001	Due To General Fund	-
09-297-000-002	Due To Capital Fund	-
	Total Other Current Liabilities	<u>-</u>
Total Liabilities		\$ -

Equity		
09-272-000-001	Opening Balance Equity	299,600.19
09-272-000-002	Permanently Restricted Net Assets	-
09-272-000-003	Retained Earnings	587,060.08
09-272-000-004	Temporarily Restricted Net Assets	-
09-272-000-005	Unrestricted Net Assets	251,305.53
	Current Period Net Income (Loss)	30,189.07
	Total Equity	<u>1,168,154.87</u>

Total Fund Balance	\$ 1,168,154.87
---------------------------	------------------------

Total Liabilities & Fund Balance	\$ 1,168,154.87
---	------------------------

Upper Uwchlan Township
Act 209 Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
09-341-000-000	Interest Income	\$ 30,189.07	\$ 40,000.00	\$ (9,810.93)	75.5%
09-354-000-030	Grant Revenue - Federal	-	-	-	#DIV/0!
09-354-000-140	Grant Revenue - State (ARLE)	-	-	-	#DIV/0!
09-379-000-010	Transportation Impact Fees	-	-	-	#DIV/0!
09-379-000-020	Transportation Impact Fees - Reserve	-	-	-	#DIV/0!
09-380-000-000	Misc Revenue	-	-	-	#DIV/0!
09-395-000-100	Transfer from General Fund	-	-	-	#DIV/0!
09-395-000-200	Transfer from Capital Fund	-	-	-	#DIV/0!
Total Revenue		30,189.07	40,000.00	(9,810.93)	#DIV/0!
09-427-000-150	Bank Fees	-	150.00	-	-
09-489-000-000	Arle Grant - Act 209	-	-	-	#DIV/0!
09-489-000-010	Engineering Fees	-	-	-	#DIV/0!
09-489-000-020	Construction	-	-	-	#DIV/0!
09-489-000-045	Contracted Services	-	-	-	#DIV/0!
09-489-000-600	Capital Construction	-	-	-	#DIV/0!
Total Expenditures		-	150.00	-	#DIV/0!
Excess of Revenues over Expenditures		\$ 30,189.07	\$ 39,850.00	\$ (9,810.93)	#DIV/0!

Upper Uwchlan Township

Sewer Fund

Balance Sheet

As of July 31, 2025

ASSETS

Cash

15-100-000-110	Cash - First Resource Bank	\$	114,642.76
15-100-000-200	Cash - Construction Fund (PSDLAF)		83.59
	Total Cash		<u>114,726.35</u>

Other Current Assets

15-130-000-001	Due from General Fund	-
15-130-000-002	Due from Municipal Authority	-
15-136-000-100	Interest Receivable	-
	Other Assets	-
	Total Other Current Assets	<u>-</u>

Long-Term Assets

15-130-000-005	Due from Municipal Authority - 2019 Bonds	5,205,556.07
15-161-000-100	Sewer Easements	-
15-163-000-100	Capital Assets - Plant	26,452,253.00
15-163-000-110	Construction in Progress	-
15-163-000-200	Capital Assets - Expansion	116,592.00
15-163-000-500	Accumulated Depreciation	(6,365,336.08)
15-157-000-100	Discount on Bonds - Series of 2019	24,042.35
15-157-000-110	OID Amortization - Series of 2019	(4,818.91)
		<u>25,428,288.43</u>

Total Assets	\$	25,543,014.78
---------------------	-----------	----------------------

LIABILITIES AND FUND BALANCE

Current Liabilities

15-200-000-000	Accounts Payable	-
15-230-000-001	Due To General Fund	-
15-230-000-002	Due to Municipal Authority	-
15-258-000-000	Accrued Expenses	-
15-258-000-045	Retainage Payable	-
15-258-000-100	Interest Payable on Bonds - 2014	-
15-258-000-105	Interest Payable on Bonds - 2019	33,882.26
15-258-000-110	Interest Payable on Bonds - Series A of 2019	18,529.20
	Total Accounts Payable	<u>52,411.46</u>

Long Term Liabilities

15-261-000-100	General Obligation Bonds- Series of 2014	-
15-261-000-105	General Obligation Bonds- Series of 2019	5,115,000.00
15-261-000-110	General Obligation Bonds- Series A of 2019	4,015,000.00
15-261-000-200	Premium on Bonds - Series of 2014	76,705.65
15-261-000-210	Premium on Bonds - Series A of 2019	139,547.90
15-261-000-250	Accrued Amortization on Bond Premium - 2014	-
15-261-000-260	Accrued Amortiz on Bond Premium - Series of 2019	(41,532.14)
		<u>9,304,721.41</u>

Total Liabilities	9,357,132.87
--------------------------	---------------------

Equity

15-272-000-100	Unrestricted Net Assets	16,206,969.26
	Current Period Net Income (Loss)	(21,087.35)
	Total Equity	<u>16,185,881.91</u>

Total Fund Balance	\$	16,185,881.91
---------------------------	-----------	----------------------

Total Liabilities & Fund Balance	\$	25,543,014.78
---	-----------	----------------------

Upper Uwchlan Township
Sewer Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
15-341-000-000	Interest Earnings	\$ 2,847.99	\$ 7,000.00	(4,152.01)	41%
15-342-000-200	Operations Mgmt Agreement Fees - 2019 bonds	101,591.70	238,294.00	(136,702.30)	43%
15-342-000-300	Operations Mgmt Agreement Fees - Series A of 2019 (MA)	55,554.49	331,175.00	(275,620.51)	17%
Total Revenues		\$ 159,994.18	\$ 576,469.00	\$ (416,474.82)	100%
EXPENDITURES					
General					
15-400-000-461	Bank Fees	1,000.00	1,000.00	-	100%
15-400-000-463	Misc Expenses	-	1,000.00	(1,000.00)	0%
15-404-000-100	Legal Fees	-	-	-	#DIV/0!
		1,000.00	2,000.00	(1,000.00)	1.00
Bond expenses					
15-472-000-105	Bond Interest Expense - Series of 2019	118,445.31	203,294.00	(84,848.69)	58%
15-472-000-110	Bond Interest Expense - Series A of 2019	64,544.11	111,175.00	(46,630.89)	58%
15-472-000-200	Bond Issuance Costs	-	-	-	#DIV/0!
15-472-000-300	Bond Amortization Expense - 2014 Bonds	-	-	-	#DIV/0!
15-472-000-305	Bond Amortization Expense - 2019 Bonds	414.68	829.00	(414.32)	50%
15-472-000-310	Bond Amortization Expense - 2019A Bonds	(3,322.57)	(6,645.00)	3,322.43	50%
Total Debt Expenses		180,081.53	308,653.00	(131,479.58)	#DIV/0!
Other					
15-493-000-083	Depreciation	-	-	-	#DIV/0!
		-	-	-	#DIV/0!
Total Expenditures before Transfers		\$ 181,081.53	\$ 310,653.00	\$ (132,479.58)	58%
Transfers					
15-492-000-010	Transfer to Municipal Authority	-	-	-	#DIV/0!
Total Transfers		-	-	-	#DIV/0!
Total Expenditures and Transfers		181,081.53	310,653.00	(132,479.58)	#DIV/0!
Excess of Revenues over Expenditures		\$ (21,087.35)	\$ 265,816.00	\$ (283,995.24)	-8%

Upper Uwchlan Township
Capital Projects Fund
Balance Sheet
As of July 31, 2025

ASSETS

Cash

30-100-000-015	Cash - First Resource Bank	\$	140,138.37
30-100-000-020	PSDLAF		5,109.71
30-110-000-100	Fulton Bank - 2019 Bond Proceeds		
30-110-000-200	Fulton Bank - 2019 Bond Proceeds - ICS		0.55
	Total Cash		<u>145,248.63</u>

Accounts Receivable

30-130-000-001	Due from General Fund		7,129.00
30-130-000-002	Due From Municipal Authority		-
30-130-000-003	Due from Escrow Fund		-
30-130-000-004	Due from Solid Waste Fund		183,749.64
30-130-000-005	Due From Liquid Fuels Fund		-
30-130-000-006	Due from Act 209 Fund		-
30-130-000-007	Due from Water Resource Protection Fund		-
30-130-000-008	Due from MA Capital Fund		-
	Total Accounts Receivable		<u>190,878.64</u>

Other Current Asset

30-155-000-000	Prepaid Expenses		33,280.00
30-191-000-000	Other Assets		-
	Total Other Current Asset		<u>33,280.00</u>

Total Assets	\$	369,407.27
---------------------	-----------	-------------------

LIABILITIES AND FUND BALANCE

Accounts Payable

30-200-000-000	Accounts Payable		-
30-258-000-000	Accrued Expenses		-
30-258-000-100	Interest Payable - 2019 Bonds		28,533.30
30-261-000-100	General Obligation Bonds - Series of 2019		4,495,000.00
30-261-000-150	Premium on GO Bonds - Series of 2019		247,103.30
30-261-000-160	Accrued Amortization - Series of 2019		(74,130.97)
	Total Accounts Payable		<u>4,696,505.63</u>

Long Term Liabilities

30-297-000-000	Other Liabilities		-
	Total Long Term Liabilities		<u>-</u>

Other Current Liabilities

30-230-000-000	Due to General Fund		34,000.00
30-230-000-001	Due To Liquid Fuels		-
30-230-000-002	Due to Act 209		-
30-230-000-003	Due to Solid Waste Fund		-
30-230-000-004	Due to Municipal Authority		-
30-230-000-005	Due To Escrow Fund		-
30-230-000-006	Due to MA Capital Fund		2,667.70
	Total Other Current Liabilities		<u>36,667.70</u>

Total Liabilities	\$	4,733,173.33
--------------------------	-----------	---------------------

Equity

30-272-000-001	Opening Balance Equity		948,398.39
30-272-000-004	Unrestricted Net Assets		(5,501,743.23)
	Current Period Net Income (Loss)		189,578.78
	Total Equity		<u>(4,363,766.06)</u>

Total Fund Balance	\$	(4,363,766.06)
---------------------------	-----------	-----------------------

Total Liabilities & Fund Balance	\$	369,407.27
---	-----------	-------------------

Upper Uwchlan Township
Capital Projects Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
REVENUES					
30-341-000-000	Interest Earnings	\$ 4,305.07	10,000.00	(5,694.93)	43%
30-354-000-010	Grant Revenue - County		-	-	#DIV/0!
30-354-000-020	Grant Revenue - State	183,749.64	100,000.00	83,749.64	184%
30-354-000-030	Grant Revenue - Federal		-	-	#DIV/0!
30-354-000-040	Grant Revenue - Other		5,800.00	(5,800.00)	0%
30-391-000-100	Sale of Fixed Assets	7,129.00	5,000.00	2,129.00	143%
30-392-000-001	Transfer from General Fund	2,104,095.92	860,000.00	1,244,095.92	245%
30-392-000-005	Transfer from Solid Waste Fund		-	-	#DIV/0!
30-392-000-020	Transfer from Act 209 Fund	-	-	-	#DIV/0!
30-392-000-030	Transfer from the Municipal Authority	-	-	-	#DIV/0!
30-393-000-000	Capital Income	-	-	-	#DIV/0!
30-393-000-020	Proceeds from Long Term Debt	-	-	-	#DIV/0!
30-393-000-400	Other financing sources	-	-	-	#DIV/0!
		2,299,279.63	980,800.00	1,318,479.63	234%

Total Revenues	\$ 2,299,279.63	\$ 980,800.00	\$ 1,318,479.63	234%
-----------------------	------------------------	----------------------	------------------------	-------------

CAPITAL EXPENSES

Township Properties					
30-409-000-700	Capital Purchases-General	-	-	-	#DIV/0!
30-409-001-700	Capital Purchases-Executive	32,975.00	40,000.00	(7,025.00)	82%
30-409-002-600	Capital Construction - Township Bldg	32,500.00	-	32,500.00	#DIV/0!
30-409-002-610	Township Bldg Expansion 2018-2020	-	-	-	#DIV/0!
30-409-002-625	Capital Construction - 128 Byers Road	1,601,808.02	-	1,601,808.02	#DIV/0!
30-409-002-700	Capital Purchases - Twp Bldg	-	-	-	#DIV/0!
30-409-003-600	Capital Construction - PW Bldg	-	-	-	#DIV/0!
30-409-003-700	Capital Purchases - PW Bldg	-	-	-	#DIV/0!
30-409-004-600	Capital Construction - Milford Rd.	-	-	-	#DIV/0!
30-409-004-700	Capital Purchases - Milford Rd.	-	-	-	#DIV/0!
	Total Township	1,667,283.02	40,000.00	1,627,283.02	4168%

Police					
30-410-000-700	Capital Purchases- Police	137,053.35	119,500.00	17,553.35	115%
	Future Purchase	-	-	-	#DIV/0!
		137,053.35	119,500.00	17,553.35	115%

Codes					
30-413-000-700	Capital Purchases	-	-	-	#DIV/0!
		-	-	-	#DIV/0!

Emergency Management					
30-415-000-700	Capital Purchases	-	-	-	#DIV/0!
		-	-	-	#DIV/0!

Public Works					
30-438-000-700	Capital Purchases-Vehicles	-	-	-	#DIV/0!
30-438-000-701	Capital Purchases - Equipment	20,170.00	278,000.00	(257,830.00)	7%
	Total Public Works	20,170.00	278,000.00	(257,830.00)	#DIV/0!

Roads					
30-502-434-700	Traffic Signals	132,165.80	11,093.00	121,072.80	1191%
	Little Conestoga Road Crosswalk	-	-	-	#DIV/0!
	Lyndell Road Bridge	-	-	-	#DIV/0!
		132,165.80	11,093.00	121,072.80	1191%

Upper Uwchlan Township
Capital Projects Fund
Statement of Revenues and Expenditures
For the Period Ending July 31, 2025

Parks					
All Parks					
30-454-000-700	Capital Purchases - All Parks	26,525.00	-	26,525.00	#DIV/0!
Hickory Park					
30-454-001-600	Capital Construction - Hickory	18,299.15	-	18,299.15	#DIV/0!
30-454-001-700	Capital Purchases - Hickory	-	-	-	#DIV/0!
Fellowship Fields					
30-454-002-600	Capital Construction - Fellowship	-	-	-	#DIV/0!
30-454-002-700	Capital Purchases - Fellowship	-	-	-	#DIV/0!
Larkins Field					
30-454-003-600	Capital Construction - Larkins	-	-	-	#DIV/0!
30-454-003-700	Capital Purchases - Larkins	-	-	-	#DIV/0!
Upland Farms					
30-454-004-600	Capital Construction - Upland	-	-	-	#DIV/0!
30-454-004-610	Fund Raising - Upland	-	-	-	#DIV/0!
30-454-004-700	Capital Purchases - Upland	15,102.96	-	15,102.96	#DIV/0!
Village of Eagle Pocket Park					
30-506-000-100	Design	-	-	-	#DIV/0!
30-506-000-600	Capital Construction	-	-	-	#DIV/0!
30-506-000-700	Capital Purchases	-	-	-	#DIV/0!
	Total Parks Capital	59,927.11	-	59,927.11	#DIV/0!
Trails					
30-455-000-650	Grant-Trails/Bridge - Struble Trail	-	-	-	#DIV/0!
30-455-000-651	Phase IV-Pk Rd Trail	-	250,000.00	(250,000.00)	0%
30-455-000-652	Side Path Project	-	-	-	#DIV/0!
	Total Trails	-	250,000.00	(250,000.00)	0%
Debt Service					
30-472-000-100	Interest Expense - Series of 2019	99,279.15	171,200.00	(71,920.85)	58%
30-472-000-200	Cost of Issuance - Series of 2019	-	-	-	#DIV/0!
30-472-000-300	Bond Amortization Expense - Series of 2019	(6,177.58)	(12,355.00)	6,177.42	50%
30-500-471-003	Capital Lease - Principal	-	-	-	#DIV/0!
30-500-472-003	Capital Lease - Interest	-	-	-	#DIV/0!
	Total Debt Service	93,101.57	158,845.00	(65,743.43)	59%
Village Concept					
30-506-000-100	Design - Village of Eagle	-	-	-	#DIV/0!
30-506-000-600	Construction - Village of Eagle	-	-	-	#DIV/0!
30-507-000-010	Misc Expenses	-	-	-	#DIV/0!
		-	-	-	#DIV/0!
Total Expenditures before Operating Transfers					
		\$ 2,109,700.85	\$ 857,438.00	\$ 1,252,262.85	246%
Operating Transfers					
30-505-000-010	Transfers to the General Fund	-	-	-	#DIV/0!
30-505-000-020	Transfers to the Solid Waste Fund	-	-	-	#DIV/0!
30-505-000-030	Transfers to the Act 209 Fund	-	-	-	#DIV/0!
	Total Operating Transfers	-	-	-	#DIV/0!
Excess of Revenues over Expenditures and Operating Transfers					
		\$ 189,578.78	\$ 123,362.00	\$ 66,216.78	153.68%

**Upper Uwchlan Township
Developers Escrow Fund
Balance Sheet
As of July 31, 2025**

ASSETS

Cash		
40-100-000-200	Cash - First Resource Bank	279,060.58
	Total Cash	279,060.58
Other Current Assets		
40-130-000-010	Due from General Fund	9,443.72
40-130-000-020	Due from Solid Waste Fund	-
40-130-000-030	Due from Municipal Authority	340.00
	Total Other Current Assets	9,783.72
Total Assets		\$ 288,844.30

LIABILITIES AND FUND BALANCE

Accounts Payable		
40-200-000-000	Accounts Payable	-
	Total Accounts Payable	-
Other Current Liabilities		
40-230-000-010	Due To General Fund	-
40-230-000-020	Due to Solid Waste Fund	-
40-230-000-030	Due to Municipal Authority	98,759.87
	<u>Due to Developers:</u>	
40-248-000-001	Toll Brothers	-
40-248-000-004	Columbia Gas Transmission LLC	8,150.24
40-248-000-005	Chester County - Radio Tower	-
40-248-000-006	Executive Land Holdings	-
40-248-000-007	Park Road Townhomes	-
40-248-000-009	Open Community Corp.	(20,197.91)
40-248-000-010	Sunoco Reed Road	4,147.14
40-248-000-011	McHugh	-
40-248-000-012	Marsh Lea	-
40-248-000-013	Eagle Pointe	-
40-248-000-014	Grashof	-
40-248-000-015	McKee Fetters	-
40-248-000-017	Vantage Point Retirement	-
40-248-000-018	CarSense	-
40-248-000-019	Village at Byers	-
40-248-000-020	Milford Rd. Associates	-
40-248-000-021	Townes at Chester Springs	-
40-248-000-022	Eagle Village Parking	-
40-248-000-023	Fish Eye	21,642.48
40-248-000-024	Jankowski	-
40-248-000-025	Eagleview Lot 1C	2,065.01
40-248-000-026	Lot 1B Maintenance Area	4,374.89
40-248-000-027	122 Oscar Way	-
40-248-000-028	Commercial 5C	-
40-248-000-030	Profound Technologies	-
40-248-000-031	Windsor Baptist Church	4,438.85
40-248-000-032	Eagle Village Parking Expansion	-
40-248-000-033	Chester Springs Crossing	8.81
40-248-000-034	Starbucks @ Eaglepoint Village	-
40-248-000-035	The Preserve at Marsh Creek SD	(5,954.16)
40-248-000-036	McKee Toll Traffic Impact Fee	63.75
40-248-000-500	Gunner Properties Performance	-
40-248-000-038	Enclave at Chester Springs	3,012.46

**Upper Uwchlan Township
Developers Escrow Fund
Balance Sheet
As of July 31, 2025**

40-248-000-039	164 Byers Rd QBD	-
40-248-000-040	Aurora Greenridge	-
40-248-000-041	Aurora Greenridge	-
40-248-000-042	DeWees 363 Byers Road	-
40-248-000-043	Black Horse Farm	-
40-248-000-044	Aecom Technology	-
40-248-000-045	11 Senn Drive Parking	429.17
40-248-000-046	Byers Station Parcel 5C Lot 2B	(105.95)
40-248-000-047	461 Font Road	-
40-248-000-048	301 Park Road	5,047.05
40-248-000-049	Eagleview Lot 1A	8,100.71
40-248-000-050	Eagle Animal Hospital	3,327.86
40-248-000-051	Rockhill Real Estate Enterprises	4,563.56
40-248-000-052	100 Greenridge Road	2,423.91
40-248-000-053	J Downend Landscaping	-
40-248-000-054	241 Park Road	(1,005.75)
40-248-000-055	Rotelle Development Co.	2,355.42
40-248-000-056	RDS Enterprises	4,986.98
40-248-001-017	VP Maintenance Bond	7,847.09
40-248-001-019	Villages Dedication	25,867.18
40-248-001-032	Gunner Parking Exp Construction	-
40-248-001-033	Crossings Dedication	-
40-248-001-035	The Preserve at Marsh Creek Sewer	(42,304.99)
40-248-001-038	Enclave at Chester Springs site	48,737.91
40-248-001-046	Celebree Construction	-
40-248-001-054	241 PR Partners Traffic Impact	81,258.60
40-248-002-035	PMC Sewer Phase III	18.50
40-248-002-038	Enclave Station Blvd Improvements	21,015.47
40-248-003-035	PMC Construction Phase III	(4,229.85)
		-

Total Other Current Liabilities	288,844.30
---------------------------------	------------

40-258-000-000	Accrued Expenses	-
----------------	------------------	---

Total Liabilities	\$ 288,844.30
--------------------------	----------------------

Equity

40-279-000-000	Opening Balance Equity	-
	Current Period Net Income (Loss)	-
	Total Equity	-

Total Fund Balance	\$ -
---------------------------	-------------

Total Liabilities & Fund Balance	\$ 288,844.30
---	----------------------

Upper Uwchlan Township
Developers Escrow Fund
Statement of Revenues and Expenditures
For the Period Ended July 31, 2025

GL Account #	Account Description	2025 YTD Actual	2025 Budget	Over (Under) Budget	Actual as % of Budget
40-341-000-000	Interest Income	\$ 5,257.60	\$ -	\$ 5,257.60	-
40-341-000-010	Interest Income - allocated to Developers	(5,257.60)	-	(5,257.60)	-
40-392-000-100	Transfer from General Fund	-	-	-	-
		-	-	-	-
	Total Revenue	-	-	-	-
40-400-000-461	Bank Fees	-	-	-	-
		-	-	-	-
		-	-	-	-
		-	-	-	-
	Total Expenditures	-	-	-	-
	Excess of Revenues over Expenditures	\$ -	\$ -	\$ -	-



GILMORE & ASSOCIATES, INC.
ENGINEERING & CONSULTING SERVICES

ENGINEER'S REPORT – UPPER UWCHLAN TOWNSHIP

Date: August 7, 2025

To: Tony Scheivert - Township Manager
Board of Supervisors

From: David Leh, P.E.

The following is an overview of engineering activities for the previous month:

500 Pottstown Pike (Rockhill Real Estate Enterprises) – . Re-revised plans have been submitted and are under review by the consultants. The application will be reviewed by the Planning Commission at their August 14th meeting.

Byers Station (Parcel 5C – Lot 2B)- [Commercial] – A “Phase 2” has now been submitted for this site which proposed an additional 10,500 SF commercial building on the north end of the property. The plans were reviewed by the consultants and by the Planning Commission at their July 10th meeting where a recommendation of plan approval was received. The application will be before the Board at your August 12th meeting for consideration.

Construction continues at the following developments with no significant issues:

- Preserve at Marsh Creek
- Byers Station (Parcel 5C – Lot 2B)- [Commercial]
- Eagleview Lot 1A
- Eagleview Lot 7



GILMORE & ASSOCIATES, INC.
ENGINEERING & CONSULTING SERVICES

DEVELOPMENT UPDATE – UPPER UWCHLAN TOWNSHIP

Date: August 7, 2025

To: Board of Supervisors

From: David Leh, P.E.

Bold items have been additions since our last report.

100 Greenridge Road (Toll) – The Board granted approval for the Conditional Use for a 64-home subdivision at their October 11th, 2022, meeting. Toll Brothers has now submitted Preliminary Land Development Plans for the project. Consistent with the conditional use approval, 64 homes are proposed. The Planning Commission accepted the plans at their April 13, 2023 meeting. The plans have been reviewed by the Township Consultants and a review letter has been issued. The Zoning Hearing Board's decision regarding the disturbance of Prohibitive Steep Slopes has been appealed.

301 Park Road (PJ Reilly) - A preliminary land development plan has been submitted by PJ Reilly Contractors for a new 8,400 SF office and equipment storage facility. The plan was reviewed by the consultants and the Planning Commission at their December 8th meeting. No action was taken. Revised plans were reviewed by The Planning Commission at their April 13, 2023 meeting, no action was taken. The applicant sought and received zoning relief from the Zoning Hearing Board at their August 30, 2023 meeting. Updated land development plans were reviewed by the Planning Commission at their May 9th Meeting. No action was taken. The Applicant attended the Planning Commission's June 13th, 2024 meeting to obtain further input prior to revising their plans. The Applicant has provided an MPC Clock extension until January 31, 2026 for plan resubmission and consideration by the Board.

500 Pottstown Pike (Rockhill Real Estate Enterprises) – The Applicant has submitted a conditional use application for a 415-space service and storage facility. The Board granted approval for the conditional use at their July 15th, 2024 meeting. Land development plans have been submitted for this application and reviewed by the consultants. The Applicant has elected to revise the plans to address the consultants' comments prior to appearing before the Planning

Reference: Development Update

File No. 9991080

August 7, 2025

Commission. Revised land development plans were reviewed by the Planning Commission at their May 8th meeting, no action was requested or taken. **Re-revised plans have been submitted and are under review by the consultants. The application will be reviewed by the Planning Commission at their August 14th meeting.**

Byers Station (Parcel 5C – Lot 2B)- [Commercial] – The Board granted approval for this application at their May 20th, 2024 Meeting. Plans have been recorded, and construction has commenced. A “Phase 2” has now been submitted for this site which proposed an additional 10,500 SF commercial building on the north end of the property. The plans were reviewed by the consultants and by the Planning Commission at their July 10th meeting where a recommendation of plan approval was received. **The application will be before the Board at your August 12th meeting for consideration.**

Eagleview Lot 1A – Hankin has submitted a Preliminary / Final Land Development Application for a one-story, 60,600 SF Flex Use office / warehouse building to be located along Arrowhead Drive. The plan was reviewed by the Planning Commission at their March 9th meeting. No action was taken with regard to plan approval; however, the Planning Commission did recommend the granting of several waivers. The Board granted the requested waivers at their March 20, 2023 meeting. Revised land development plans have been submitted and reviewed by the consultants. The Board granted Preliminary / Final Approval to the application at their June 19, 2023 meeting. Plans have now been recorded, and construction has commenced.

Preserve at Marsh Creek (Fetters Property) - The Board granted Final Land Development Approval at their October 16th, 2017, meeting. Site construction continues in all 3 phases. (i.e.- The entire development) We have received grading permit applications for **358** of the 376 building units to date. Phases 1 and 2 are nearing completion and as such, we are in the process of generating punchlists for the site improvements.

Struble Trail Extension – Chester County has submitted an application to reopen the Conditional Use Hearing as well as land development plans to allow for the continuance of the trail from where it currently terminates to a point on the west side of Dorlan Mill Road. The Planning Commission reviewed the application at their June 13th, 2019, and July 11th, 2019, meetings and recommended Conditional Use Approval as well as Preliminary Land Development approval. The County has submitted revised land development plans which have been reviewed by the

Reference: Development Update

File No. 9991080

August 7, 2025

consultants. The Planning Commission reviewed the application at their June 8th meeting and recommended conditional use as well as Preliminary / Final Land Development Approval. The Board granted conditional Use and land development approval at their July 17th Meeting. The design engineer advised us they have submitted revised Highway Occupancy Plans to Pa-DOT last week and expect approval shortly. The county is hoping to go out to bid on the project this summer.



UPPER UWCHLAN TOWNSHIP

MEMORANDUM

ADMINISTRATION

TO: The Board of Supervisors

FROM: Kathi McGrath *Kathi*
Administrative Assistant

RE: Building/Codes Department Activity Report

DATE: August 1, 2025

=====

Attached, please find the Building Department Activity Report for the month of July, 2025.

Attachments:
Activity Report

/km

UPPER UWCHLAN TOWNSHIP
Permit Analysis
2022-2025

	2022				2023				2024				2025			
	# of Permits	Fees	YTD Permits	YTD Fees	# of Permits	Fees	YTD Permits	YTD Fees	# of Permits	Fees	YTD Permits	YTD Fees	# of Permits	Fees	YTD Permits	YTD Fees
Jan	83	\$ 88,198.96	83	\$ 88,198.96	39	\$16,560.04	39	\$16,560.04	65	\$46,474.10	65	\$46,474.10	44	\$ 9,598.00	44	\$ 9,598.00
Feb	39	\$ 25,249.00	122	\$ 113,447.96	58	\$115,689.00	97	\$132,249.04	53	\$29,120.38	118	\$75,594.48	42	\$29,544.00	86	\$ 39,142.00
Mar	77	\$ 52,822.55	199	\$ 166,270.51	72	\$ 32,216.00	169	\$ 164,465.04	56	\$ 46,201.34	174	\$ 121,795.82	72	\$72,825.00	158	\$ 111,967.90
Apr	84	\$ 63,858.84	283	\$ 230,129.35	51	\$ 37,396.20	220	\$201,864.24	57	\$ 48,358.00	231	\$ 170,153.82	43	\$18,396.00	201	\$ 130,363.90
May	65	\$ 44,000.00	348	\$ 274,129.35	82	\$ 38,301.00	302	\$240,165.24	66	\$ 27,447.00	297	\$ 197,600.82	65	\$25,225.00	266	\$ 155,588.90
Jun	96	\$ 28,606.00	444	\$ 302,735.35	66	\$ 26,909.50	368	\$267,074.74	62	\$ 38,947.57	359	\$ 236,548.39	62	\$34,038.28	328	\$ 189,627.18
Jul	68	\$ 37,917.72	512	\$ 340,653.07	77	\$127,398.14	445	\$394,472.88	62	\$ 18,548.38	421	\$ 255,096.77	70	\$34,169.00	398	\$ 223,796.18
Aug	68	\$112,113.82	580	\$ 452,766.89	65	\$ 68,299.92	510	\$462,772.80	73	\$ 29,601.00	494	\$ 284,697.77				
Sept	73	\$ 46,052.92	653	\$498,819.91	52	\$ 20,696.00	562	\$483,468.80	68	\$ 94,664.42	562	\$ 379,362.19				
Oct	54	\$ 42,196.00	707	\$541,015.91	59	\$ 35,511.00	621	\$518,979.80	96	\$ 72,313.59	658	\$ 451,675.78				
Nov	45	\$ 21,304.00	752	\$562,319.91	57	\$ 32,429.00	678	\$551,408.80	56	\$ 24,214.95	714	\$ 475,890.73				
Dec	47	\$22,295.00	799	\$584,614.91	27	\$ 11,076.28	705	\$562,485.08	36	\$ 26,840.00	750	\$ 502,730.79				



JULY 2025 REPORT UPPER UWCHLAN TOWNSHIP PUBLIC WORKS DEPARTMENT

The following projects were underway since we last met:

Ongoing:

- **Aside from regular routine maintenance, the following work orders were submitted last month.**

Tracking of work orders through Traisr: 184

- **Municipal Authority & PA 1-calls**
 - **80 Work orders completed**
- **Parks**
 - **5 Work orders completed**
- **Public Works**
 - **39 Work orders completed**
- **Solid Waste**
 - **50 Work orders completed**
- **Vehicles and Equipment (All Depts.)**
 - **10 Work orders completed**
- **Base repairs completed on Magnolia Drive.**
- **Tree trimming along the roadways in Eagle Hunt and Byers Station in preparation for upcoming Road Milling and Paving.**
- **Assist Charles Higgins by removing trees around signal box.**
- **Assisted the Boy Scouts with their Eagle Scout Project at Upland Farms.**
- **Multiple call in's to clean up trees and storm debris throughout the township.**
- **Assisted with the Summer Concert held at Upland on July 13th.**

- Remove and clean up landscaping around new township building, 144 Byers Rd.
- Continue road mowing throughout the township.
- Repaired multiple potholes throughout the Township.
- Collected trash, recycling and yard waste that was missed by the haulers.
- Minor maintenance issues were handled at the Township Building.
- Tree trimming for sight distance at intersections and along roadways where trees are hanging into the road.
- Sign replacements at various locations.
- Cleared inlets on multiple roadways throughout the Township.
- Toter swaps and deliveries were done as requested.
- Preventive maintenance, repairs, and Pa State Inspections.
- Serviced Police vehicles.
- PA 1-Calls were responded to as they came in.

Workforce

- Public Works is interviewing for a Roadworker position.
- Summer help is busy cutting grass at the parks and sewer plants.

Respectfully submitted,

**Michael Esterlis
Public Works Director
Upper Uwchlan Township**

BUCKLEY, BRION, McGUIRE,
& MORRIS LLP
By: KRISTIN S. CAMP, Esquire
Attorney I.D. # 74593
118 West Market Street, Suite 300
West Chester, Pennsylvania 19382
(610) 436-4400

IN RE: CONDITIONAL USE : BEFORE THE UPPER UWCHLAN
APPLICATION OF MICHAELA KOLBE : TOWNSHIP BOARD OF SUPERVISORS
: CHESTER COUNTY, PENNSYLVANIA

DECISION AND ORDER

On or about May 13, 2025, Michaela Kolbe (“Applicant”) filed an application (the “Application”) seeking conditional use approval pursuant to Section 200-13.C(7) of the Upper Uwchlan Township Zoning Ordinance of 1989, as amended in Ordinance No. 2024-02 adopted on September 16, 2024 (the “Ordinance”), to use two dwellings on the property located at 705 N. Reeds Road (Chester County Tax Parcel No. 32-5-15) as a short term rental.

The Board conducted a public hearing on the Application on July 21, 2025.

Applicant appeared at the hearing *pro se*. The Board was represented by Township Solicitor, Kristin S. Camp, Esquire.

No individuals requested to be admitted as parties.

The Board voted at the August 12, 2025 public meeting to approve the Application with the conditions listed in this Decision.

From the testimony and exhibits presented at the hearings, the Board makes the following:

FINDINGS OF FACT

1. All facts set forth in the introductory paragraphs above are incorporated herein by reference and are deemed to be factual findings of the Board.

2. The hearing scheduled for July 21, 2025 was advertised in the *Daily Local News* on July 3, 2025 and July 10, 2025. See Exhibit B-2.

3. The Property was posted with a copy of the public notice of the July 21, 2025 hearing on July 2, 2025. See Exhibit B-4.

4. Applicant is the owner of the Property with her mother, Sonya J. Pritchard, pursuant to a deed dated November 4, 2021, recorded November 23, 2021, in the Chester County Recorder of Deeds in Book 10744, page 672.

5. The Property contains 1.0 acre and is improved with two single family detached dwellings (individually a “Dwelling” and collectively the “Dwellings”).

6. The Property is located in the R-1 District.

7. Applicant has made significant external and internal renovations to the Dwellings and Property after her acquisition of same including installing a new HVAC system, well pump, roof on the rear Dwelling and safety features required by Township Code.

8. Each Dwelling contains one bedroom and one bathroom.

9. Applicant resides in the rear Dwelling and has marketed the front Dwelling on Airbnb since April of 2023.

10. Applicant intends to continue to renovate the rear Dwelling and hopes to someday move and list that Dwelling as a short term rental.

11. Applicant only permits a maximum of two guests to stay overnight in the Dwelling and intends to continue that restriction for both Dwellings if they are rented as short term rentals.

12. The Property is serviced by on-lot sewer and water.

13. Applicant requires the tenants of the short term rental to adhere to specific

rules such as no smoking, no firepits, no parties and two car maximum. She does allow the guests to bring pets and requires the pets to always be leashed and not left unattended.

14. Applicant would agree to a restriction limiting the number of dogs to two.

15. Consistent with the rules on Airbnb, Applicant requires the guests to be a minimum of 18 years old.

16. When asked if she would agree to require her guests to be a minimum of 25 years old, she expressed concern over how she would enforce this and prefers to allow individuals who are 18 and older to rent the short term rentals.

17. Both Dwellings are accessed from a common driveway that has sufficient space to accommodate a maximum of four parking spaces- two for each Dwelling.

18. If Applicant moves from the rear Dwelling, she intends to stay local or her mother who resides within 5 minutes from the Property would serve as the local agent to handle problems that arise from the short term rentals.

19. Several neighbors attended the hearing and expressed that they have not had any issues with Applicant renting the front Dwelling as a short term rental. They expressed concern over the rear Dwelling also being rented as a short term rental.

20. The Planning Commission recommended that the Board approve the application. See Exhibit B-5.

21. The Board introduced the following exhibits at the hearing:

B-1: Conditional Use Application dated May 13, 2025

B-2: Proof of Publication in *Daily Local News* on July 3, 2025 and July 10, 2025

B-3: Aerial Tax Map of Property

B-4: Affidavit of Posting on July 2, 2025

B-5: Draft Planning Commission Minutes from Meeting held on June 12, 2025

B-6: Email from Applicant dated May 27, 2025 agreeing to extend MPC clock for Board to hold hearing.

CONCLUSIONS OF LAW

1. Applicant as owner of the Property has standing to file the Application.
2. The conditional use hearing was duly advertised in accordance with the provisions of the Pennsylvania Municipalities Planning Code (“MPC”) and the Ordinance and the Property was posted in accordance with the requirements of the MPC.
3. The Property is located in the R-1 District.
4. Section 200–13.C(7) of the Ordinance allows a single family detached dwelling to be used as a short term rental in the R-1 District by conditional use of the Board of Supervisors subject to compliance with the provisions in Sections 200-72.3.and 200-117.
5. A short term rental is defined as: “[a]ny single family detached dwelling unit rented for the purpose of overnight transient lodging for a period of less than 30 days. The leasing of one or more bedrooms in a dwelling unit shall not be permitted as a short term rental.”
6. Applicant demonstrated compliance with the provisions in Section 200-72.3 of the Ordinance through the Application and her testimony.
7. Subject to Applicant’s compliance with the conditions imposed herein by the Board, Applicant has proven compliance with the objective requirements for conditional use in Section 200-117 of the Ordinance.

DISCUSSION

A conditional use is a use permitted in a particular zoning district pursuant to the provisions in Article VI of the MPC, 53 P.S. §10603. A conditional use concerns only a proposed use of land, not particular design details of the proposed development. *Joseph v. North Whitehall Township Board of Supervisors*, 16 A.3d 1209 (Pa.Cmwlt. 2011). A conditional use is a special exception which falls within the jurisdiction of the municipal legislative body rather than the zoning hearing board. 53 P.S. §10603(c). *Id.* A conditional use is not an exception to the zoning ordinance, but rather a use to which the applicant is entitled provided the specific standards enumerated in the ordinance for conditional use are met by the applicant. *In Re Appeal of AMA/American Marketing Association*, 142 A.3d 923 (Pa.Cmwlt. 2015). The fact that a use is permitted as a conditional use, rather than prohibited, reflects a legislative decision that the use is not *per se* adverse to the public interest. *Appeal of Richboro CD Partners LP*, 89 A.3d 742 (Pa Cmwlt. 2014).

An applicant seeking a conditional use initially bears the burden of establishing that the application complies with the objective standards and criteria in the zoning ordinance. *Id.* Once the applicant has satisfied this initial burden, there is a legislative presumption that the use is consistent with the health, safety and welfare of the community. *Id.* The burden then shifts to the objectors to rebut this presumption by establishing that the use will have a detrimental impact on the surrounding community. The objectors must prove with a high degree of probability that allowing the conditional use will create a substantial risk of harm to the community and will impose detrimental impacts exceeding those ordinarily to be expected from the use at issue. *Id.* The degree of harm required to justify denial of a conditional use must be greater than that which

normally flows from the proposed use. *In Re Cutler Group, Inc.*, 880 A.2d 39 (Pa. Cmwlth. 2005).

In granting a conditional use, the Board has the authority to impose reasonable conditions and safeguards if such conditions are necessary to implement the purposes of the Ordinance and to protect the health, safety and welfare of the surrounding property owners. 53 P.S. §10603(c)(2). Conditions imposed by the Board are designed to protect the public interest of surrounding property owners. *Ford v. Zoning Hearing Bd. Of Caernarvon Twp.*, 616 A.2d 1089 (Pa. Cmwlth. 1992).

The Board adopted Ordinance 2024-02 in September of 2024 to allow short term rentals in single family detached dwelling units by conditional use in the R-1 and R-2 Districts. Applicant had the burden of proving that the proposed short term rentals meet all objective criteria in Section 200-72.3 and the subjective criteria in Section 200-117 of the Ordinance.

The Board finds Applicant to be a credible and conscientious owner and landlord who is cognizant of the potential nuisances that may arise if a short term rental is not properly managed. The Board appreciates the work that Applicant has done to the Property to renovate both Dwellings and improve the value and aesthetic appeal of the Property. The Board believes that if Applicant enforces the rules that she explained in the application and testimony, as well as the conditions in this Decision, both Dwellings may be used as short term rentals without causing a detriment to the public health, safety and welfare.

The Board appreciates the neighbors' comments and concerns and will impose conditions to address some of the objections raised.

Wherefore, the Board will vote to approve the conditional use subject to Applicant's compliance with the conditions imposed in this Decision and Order.

ORDER

AND NOW, this 12th day of August 2025, the Board hereby approves a conditional use pursuant to Section 200-13.C(7) of the Ordinance to allow Applicant to use both Dwellings on the Property as short term rentals subject to the conditions below.

CONDITIONS OF APPROVAL

1. The short term rentals shall be operated and used in accordance with the Application and all testimony and evidence presented at the hearing and the terms of this Order.

2. This approval shall be specific to Applicant and her use of the Property as a short term rental. The approval herein shall not run with the land and shall expire if the Property is transferred to an individual or entity other than Applicant or her mother.

3. There shall be a maximum of two dogs allowed in each Dwelling if used as short term rentals.

4. Applicant shall not operate the short term rentals until she obtains rental permits for each Dwelling from the Township which rental permits must be renewed annually.

5. Applicant shall install a fenced-in area in the rear or side yard of the Property which is enclosed by a minimum four foot tall fence which shall be used for renters to take their dogs outside. Applicant shall include in the rules and regulations for the rental that this area shall be used when taking dogs outside.

6. Applicant shall require that the proposed guests of the short term rentals must be a minimum of 25 years old.

7. Applicant shall include in all marketing and listings of the short term rental the following: (i) there is no on street parking permitted; (ii) the maximum number of guests is limited to two per Dwelling; and (iii) quiet hours at the short term rental are from 10:00pm to 7:00 am.

**UPPER UWCHLAN TOWNSHIP
BOARD OF SUPERVISORS**

Jennifer F. Baxter, Chairperson

Andrew P. Durkin, Vice Chairperson

Sandra M. D'Amico, Supervisor

**UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY, PENNSYLVANIA**

ORDINANCE NO. 2025 -- _____

AN ORDINANCE OF THE UPPER UWCHLAN TOWNSHIP BOARD OF SUPERVISORS, CHESTER COUNTY, PENNSYLVANIA (THE “TOWNSHIP”) PURSUANT TO THE ACT OF THE GENERAL ASSEMBLY OF THE COMMONWEALTH OF PENNSYLVANIA, KNOWN AS THE LOCAL GOVERNMENT UNIT DEBT ACT, 53 Pa. C.S. Chs. 80-82 (THE “DEBT ACT”); SETTING FORTH ITS INTENTION TO AUTHORIZE AND DIRECT THE INCURRING OF NON-ELECTORAL DEBT THROUGH THE ISSUANCE OF GENERAL OBLIGATION BONDS (COLLECTIVELY, THE “BONDS”) IN A MAXIMUM AGGREGATE PRINCIPAL AMOUNT OF UP TO \$9,890,000 FOR THE PURPOSE OF PROVIDING FUNDS TO FINANCE (1) THE ACQUISITION OF REAL PROPERTY LOCATED WITHIN THE TOWNSHIP AT 144/128 BYERS ROAD (THE “PROPERTY”), RENOVATIONS TO THE PROPERTY AND VARIOUS OTHER CAPITAL IMPROVEMENTS TO THE PROPERTY; (2) RENOVATIONS AND CAPITAL IMPROVEMENTS TO THE FACILITIES LOCATED WITHIN THE TOWNSHIP’S HICKORY PARK AND VARIOUS OTHER CAPITAL PROJECTS THAT BENEFIT THE TOWNSHIP; (3) REPAIRS AND IMPROVEMENTS TO THE TOWNSHIP’S SEWER SYSTEM AND RELATED INFRASTRUCTURE; AND (4) THE PAYMENT OF THE COSTS AND EXPENSES OF ISSUING THE BONDS (COLLECTIVELY, THE “PROJECT”); MAKING CERTAIN FINDINGS AND DETERMINATIONS IN CONNECTION WITH THE PROJECT; ACCEPTING A NEGOTIATED PROPOSAL FOR THE PURCHASE OF THE BONDS; PROVIDING THAT THE BONDS, WHEN ISSUED, SHALL CONSTITUTE A GENERAL OBLIGATION OF THE TOWNSHIP; AUTHORIZING THE DESIGNATION OF EACH SERIES OF BONDS AS A “QUALIFIED TAX-EXEMPT OBLIGATION” AS DEFINED IN SECTION 265(b)(3)(B) OF THE INTERNAL REVENUE CODE OF 1986, AS AMENDED (THE “CODE”) PROVIDING FOR THE MANNER OF EXECUTION AND OTHER PROVISIONS OF THE BONDS; APPOINTING A PAYING AGENT; COVENANTING TO BUDGET IN EACH FISCAL YEAR WHEN DUE; PLEDGING THE FULL FAITH, CREDIT AND TAXING POWER OF THE TOWNSHIP FOR THE TIMELY PAYMENT OF DEBT SERVICE ON THE BONDS; ESTABLISHING A SINKING FUND AND PROVIDING FOR PAYMENT OF THE BONDS THEREFROM; DIRECTING THE DEPOSIT OF THE PROCEEDS OF THE BONDS; AUTHORIZING THE INVESTMENT OF THE PROCEEDS OF THE BONDS AND THE EXECUTION OF ESCROW AGREEMENTS; AUTHORIZING AND DIRECTING THE PREPARATION, VERIFICATION AND FILING OF A DEBT STATEMENT, BORROWING BASE CERTIFICATE AND RELATED MATERIAL WITH THE PENNSYLVANIA DEPARTMENT OF COMMUNITY AND ECONOMIC DEVELOPMENT TOGETHER WITH AN APPLICATION BY THE TOWNSHIP FOR THE APPROVAL OF THE INCURRENCE OF SUCH NON-ELECTORAL DEBT AND ISSUANCE OF THE BONDS; MAKING CERTAIN COVENANTS WITH RESPECT TO THE BONDS RELATING TO FEDERAL TAX AND SECURITIES LAWS; AUTHORIZING AND DIRECTING THE EXECUTION

AND DELIVERY OF THE BONDS AND SUCH OTHER DOCUMENTS AS MAY BE NECESSARY OR ADVISABLE IN CONNECTION WITH THE ISSUANCE OF THE BONDS, UPON THE APPROVAL OF PENNSYLVANIA DEPARTMENT OF COMMUNITY AND ECONOMIC DEVELOPMENT; DIRECTING THE PROPER OFFICERS AND OFFICIALS OF THE TOWNSHIP TO DO ALL THINGS NECESSARY TO CARRY OUT SUCH ORDINANCE; AND RESCINDING ALL INCONSISTENT ORDINANCES.

WHEREAS, Upper Uwchlan Township, Chester County, Pennsylvania (the “Township”) is a political subdivision of the Commonwealth of Pennsylvania and is a “local government unit” under terms of the Pennsylvania Local Government Unit Debt Act, as codified by the Act of December 19, 1996 (P.L. 1158, No. 177) (the “Debt Act”); and

WHEREAS, the Board of Supervisors (the “Board”) of the Township has determined to incur non-electoral indebtedness and issue Bonds (as defined herein), in one or more series, in accordance with the Debt Act, the proceeds of which shall be used for the purpose of providing funds to finance (1) the acquisition of real property located within the Township at 144/128 Byers Road (the “Property”) renovations to the Property and various other capital improvements to the Property; (2) renovations and capital improvements to the facilities located within the Township’s Hickory Park and various other capital projects that benefit the Township; (3) repairs and improvements to the Township’s sewer system and related infrastructure; and (4) the payment of the costs and expenses of issuing the Bonds (collectively, the “Project”); and

WHEREAS, the Board has determined that it is in the best interest of the Township to sell the Bonds through a private negotiated sale, and to establish certain parameters under which it will accept a proposal for the purchase of the Bonds; and

WHEREAS, the Township has received an acceptable proposal for the purchase of the Bonds from RBC Capital Markets, LLC (the “Purchaser” or “Bond Purchaser”) and desires to authorize the acceptance of such proposal (the “Bond Purchase Agreement”) and the issuance of the Bonds for the purposes set forth herein, upon the terms and conditions within, and subject to the parameters set forth herein; and

WHEREAS, the Township desires accept the Bond Purchase Agreement and authorize the issuance of its General Obligation Bonds, Series of 2025, in one or more series, in the aggregate maximum principal amount of \$9,890,000 for the purposes set forth herein, upon the terms and conditions and in the form as herein provided (the “Bonds”), and to authorize the acceptance of the proposal for the purchase of the Bonds.

NOW, THEREFORE, BE IT RESOLVED, by the Board of the Township as follows:

SECTION 1. Establishment of Parameters for Bonds. The Township hereby establishes that the issuance of the Bonds authorized hereunder shall be subject to the Bonds satisfying the following parameters: (a) the Bonds shall not exceed \$9,890,000 in aggregate principal amount (net of original issue discount); (b) the Bonds shall not mature later than the dates set forth on Schedule “A” attached hereto and made part hereof; (c) the purchase price for the Bonds shall not

be less than 90% or more than 125% of the principal amount of the Bonds; (d) the Underwriter's discount shall not exceed \$8.00 per \$1,000 of Bonds; and (e) the maximum principal amounts and the maximum interest rates shall not exceed those stated on Schedule "A." The Township hereby acknowledges receipt of the Bond Purchase Agreement from the Purchaser, pursuant to which the Township agrees to sell the Bonds to the Purchaser satisfying the conditions and parameters set forth therein as shall be confirmed as set forth below. A copy of the Bond Purchase Agreement shall be delivered to the Secretary of the Township and shall be affixed to and shall become part of this Ordinance. The Bond Purchase Agreement is hereby approved and accepted and the Chairperson and Secretary of the Board or the Vice Chairperson or Assistant Secretary, in the absence of the Chairperson or Secretary, respectively, or any duly elected officers, as the case may be, are hereby authorized and directed to execute the Bond Purchase Agreement on behalf of the Township in accordance therewith, and deliver a copy of the executed Bond Purchase Agreement to the appropriate parties. Upon determination by the Chairperson or Vice Chairperson of the Township that the Addendum to the Bond Purchase Agreement (as defined below) submitted to the Township by the Purchaser meets the parameters set forth above, and that the timing of the proposed closing is appropriate, the Chairperson and Secretary of the Board or the Vice Chairperson or Assistant Secretary, in the absence of the Chairperson or Secretary, respectively, or any duly elected officers, as the case may be, are hereby authorized to accept and execute on behalf of the Township an Addendum to the Bond Purchase Agreement (as hereinafter defined) for each series of Bonds in accordance therewith, and deliver a copy of the same to the Secretary of the Board pursuant to the procedure set forth below.

The Board is hereby authorized and directed to review and approve the final terms of the Bonds presented by the Purchaser including, without limitation, the final interest rates, initial offering prices and yields and any other appropriate terms and conditions applicable to the Bonds, and to determine if such terms are within the parameters established hereunder. Upon presentation by the Purchaser of the final terms of the Bonds in satisfaction of the conditions and parameters set forth in the Bond Purchase Agreement and this Ordinance, the Chairperson and Secretary of Board or the Vice Chairperson or Assistant Secretary, in the absence of the Chairperson or Secretary, respectively, or any duly elected officer, as the case may be, are hereby authorized and directed to confirm in writing that such conditions and parameters have been satisfied, to accept the final terms of the Bonds, to execute and deliver an addendum to the Bond Purchase Agreement for each series of Bonds issued pursuant to this Ordinance (each, an "Addendum to the Bond Purchase Agreement") setting forth the final terms of the Bonds and to authorize the release of the Bonds upon settlement.

SECTION 2. Authorization of Issuance of Bonds and Approval of Project. The Township hereby approves the Project described in the recitals hereto and authorizes the incurring of indebtedness pursuant to the Debt Act by the issuance of the Bonds as described in Section 1 hereof in the principal amount of up to \$9,890,000. The Project is being undertaken by the Township for the purpose of providing funds to finance (1) the acquisition of the Property, renovations to the Property and various other capital improvements to the Property; (2) renovations and capital improvements to the facilities located within the Township's Hickory Park and various other capital projects that benefit the Township; (3) repairs and improvements to the Township's sewer system and related infrastructure; and (4) the payment of the costs and expenses of issuing the Bonds.

SECTION 3. Useful Life./Realistic Cost Estimates It is hereby determined and declared that: (i) the projects comprising the Project have varying useful lives and therefore, in accordance with Section 8142(a)(2)(ii) of the Debt Act, the average useful life of the Project is thirty (30) years; and (ii) the Township has obtained realistic estimates of the costs of the Project through bid prices or estimates from persons qualified by experience to provide such estimates.

SECTION 4. Non-Electoral Debt. All of the debt to be incurred upon issuance of the Bonds shall be incurred as non-electoral debt.

SECTION 5. Execution of Debt Statement and Bonds and Filing of Debt Proceedings. The Chairperson and Secretary of the Board or the Vice Chairperson and Assistant Secretary, in the absence of the Chairperson or Secretary, respectively, or any duly appointed successors, as the case may be, are hereby directed to prepare and certify and to file the debt statement required by Section 8110 of the Debt Act, to execute and deliver the Bonds, and to prepare and certify all filings required pursuant to Section 8111 of the Debt Act, pertaining to the submission to the Pennsylvania Department of Community and Economic Development (the "Department"), of the transcript of the proceedings, which shall include certified copies of the Ordinance, proofs of proper publication, the accepted proposal for the purchase of the Bonds and such other documents as may be necessary in connection with the same, including, but not limited to, preparation and filing of any statements required by the Debt Act which are necessary to qualify all or any portion of the Bonds for exclusion from the appropriate debt limit as self-liquidating debt or subsidized debt, and to take all such further action and to execute and deliver such other documents as may be necessary or appropriate to comply with all requirements of the Debt Act or to carry out the intent and purposes of this Ordinance.

SECTION 6. Terms and Form of Bonds. The Bonds when issued shall be general obligation bonds issued in fully registered form, in denominations of Five Thousand Dollars (\$5,000), or in any integral multiple thereof, shall be payable at the place and in the manner, and shall be substantially in the form attached hereto as Schedule "B." The Bonds shall be dated the date of issuance (the "Issuance Date"), shall bear interest from the Issuance Date at rates per annum not to exceed the rates per annum, and mature on the dates set forth in Schedule "A" and as established in accordance with and within the parameters established hereunder and under the Bond Purchase Agreement. The Bonds shall be numbered as issued without regard to denomination or maturity.

SECTION 7. Appointment of Paying Agent and Sinking Fund Depository. Fulton Bank, N.A., is hereby appointed to serve as paying agent, bond registrar, and sinking fund depository (the "Paying Agent") for the Bonds. The Chairperson and Secretary of the Board or the Vice Chairperson and Assistant Secretary, respectively, or any duly appointed successors, as the case may be, are directed to contract with the Paying Agent to obtain its services in the aforementioned capacities. The Township shall cause to be kept, and the Paying Agent is hereby directed to keep, at the designated corporate trust offices of the Paying Agent, books for the registration, exchange and transfer of Bonds in the manner provided herein and therein so long as Bonds shall remain outstanding. The Paying Agent is hereby directed to make such registrations, exchanges and

transfers without charge to bondholders, except for actual costs, including postage, insurance and any taxes or other governmental charges required to be paid with respect to the same.

SECTION 8. Establishment of Sinking Fund. The Township hereby establishes with the Paying Agent a sinking fund (the “Sinking Fund”) for the payment of the Bonds. The Treasurer of the Township shall pay the amounts required for payment of the Bonds into the Sinking Fund, which shall be maintained until such Bonds are paid in full. Sums sufficient to meet the requirements of the semi-annual interest payments and scheduled maturities of the Bonds shall be deposited into the Sinking Fund no later than the date when interest or principal is to become due on the Bonds. The funds in the Sinking Fund shall be subject to withdrawal by the Paying Agent only to pay the principal and interest on the Bonds as the same becomes due and payable in accordance with the terms thereof. The Township hereby covenants that such monies, to the extent required, will be applicable to such purpose. The principal of and interest on the Bonds shall be payable in lawful money of the United States of America at the designated corporate trust offices of the Paying Agent.

SECTION 9. Covenant to Pay Bonds. The Township covenants that, to the fullest extent authorized under law:

a. The amount of the debt service with respect to the Bonds payable in each fiscal year shall be included in the Township budget for that year;

b. The Township shall appropriate such amounts from its general revenues and the Township’s sewer fund necessary for the payment of such debt service;

c. It shall duly and punctually pay, or cause to be paid from the Sinking Fund or any other of its revenues or funds, the principal of and interest due upon the Bonds, to the extent of its obligations, on the dates, at the places and in the manner stated in the Bonds, according to the true intent and meaning thereof; and

d. For such payment, budgeting and appropriation of the Township herewith irrevocably pledges its full faith, credit and taxing power.

The covenants contained in this Section shall be specifically enforceable.

SECTION 10. Sale of Bonds. In compliance with Section 8161 of the Debt Act and after due consideration, the Board hereby determines that a private sale by negotiation is in the best financial interest of the Township. The Bonds shall be sold at private sale by negotiation upon receipt of an acceptable proposal for the purchase thereof from the Purchaser in the form of the Bond Purchase Agreement, which shall be in compliance with the provisions of the Debt Act.

SECTION 11. Acceptance of Proposal for Purchase of Bonds. The Township hereby acknowledges receipt of a proposal from the Purchaser in the form of the Bond Purchase Agreement, which has been delivered to the Secretary of the Board and shall be affixed to and shall become part of this Ordinance. The Township hereby finds the Bond Purchase Agreement to be in conformity with the requirements of the Debt Act and of this Ordinance and is hereby

authorized to accept the Bond Purchase Agreement and upon receipt of the Addendum to the Bond Purchase Agreement pursuant to Section 1 of this Ordinance is further authorized to sell the Bonds to the Purchaser as provided in the Bond Purchase Agreement and the Addendum to the Bond Purchase Agreement. The officers of the Township are hereby authorized to deliver the Bonds to the Purchaser upon receipt of the principal amount thereof and upon compliance with all of the conditions precedent to such delivery required by the Debt Act, the Ordinance, the Bond Purchase Agreement and the Addendum to the Bond Purchase Agreement.

SECTION 12. Execution, Authentication and Delivery of Bonds. The Bonds, when issued, shall be executed either manually or by facsimile by the Chairperson or Vice Chairperson of the Board and shall have the corporate seal or facsimile thereof of the Township affixed thereto and be duly attested by the manual or facsimile signature of the Secretary or Assistant Secretary (or any acting Secretary or Assistant Secretary appointed for such purpose) of the Board. The Bonds shall be authenticated by the manual signature of the Paying Agent. The Chairperson or Vice Chairperson and Secretary (or any acting Secretary or Assistant Secretary appointed for such purpose) are authorized and directed to deliver the Bonds, but only after the Department has certified its approval pursuant to Section 8204 of the Debt Act, and to execute and deliver such other documents and to take such other action as may be necessary or appropriate in order to effectuate the issuance, sale and delivery of the Bonds, all in accordance with the Debt Act, this Ordinance, the Bond Purchase Agreement, and the Addendum to the Bond Purchase Agreement.

SECTION 13. Appointment of Securities Depository. The Bonds shall be issued in book-entry form only, and one bond shall be registered for each maturity in the name of Cede & Co., as nominee for the Depository Trust Company, New York, New York ("DTC"), which shall act as securities depository for the Bonds on behalf of the firms which participate in the DTC book-entry system ("DTC Participants"). Each bond will be in the aggregate principal amount of such maturity as established in accordance with the final terms of the Bonds within the parameters set forth herein. The Township shall cause the Bonds to be delivered to DTC for the benefit of the purchaser of the Bonds on or before the date of issuance of the Bonds.

Pursuant to the book-entry only system, any person for whom a DTC Participant acquires an interest in the Bonds (the "Beneficial Owner") will not receive certificated Bonds and will not be the registered owner thereof. Ownership interest in the Bonds may be purchased by or through DTC Participants. Each DTC Participant will receive a credit balance in the records of DTC in the amount of such DTC Participant's in the Bonds, which will be confirmed in accordance with DTC's standard procedures. Receipt by the Beneficial Owners (through any DTC Participant) of timely payment of principal, premium, if any, and interest on the Bonds, is subject to DTC making such payment to DTC Participants and such DTC Participants making payment to Beneficial Owners. Neither the Township nor the Paying Agent will have any direct responsibility or obligation to such DTC Participants or the persons for whom they act as nominees for any failure of DTC to act or make any payment with respect to the Bonds.

The Township is authorized to execute such documents as may be necessary or desirable in connection with DTC's services as securities depository. DTC may determine to discontinue providing its services with respect to the Bonds at any time by giving notice to the Township and discharging its responsibilities with respect thereto under applicable law. Under such

circumstances, the Township officials then holding the offices set forth in Section 23 of this Ordinance are hereby authorized to designate a successor securities depository or to deliver certificates to the Beneficial Owners of the Bonds.

SECTION 14. Redemption Provisions. Specific redemption provisions, including mandatory redemption provisions, if any, will be as set forth in the Bond Purchase Agreement or the Addendum to the Bond Purchase Agreement, and as further set forth in the Bonds.

The Paying Agent shall give notice of any such redemption by first class mail, postage prepaid, mailed not less than thirty (30) nor more than forty-five (45) days prior to the redemption date to each registered owner of the Bonds to be redeemed at its registered address as it appears on the bond register maintained by the Paying Agent, or such other notice of redemption as deemed appropriate. Such notice having been mailed and funds sufficient for redemption having been deposited with the Paying Agent, the Bonds so called for redemption shall become due and payable on the date fixed for redemption and interest thereafter shall cease to accrue thereon, whether such Bonds shall be presented for payment or not.

SECTION 15. Limitation on Indebtedness. It is declared that the debt to be incurred hereby, together with any other indebtedness of the Township, is not in excess of any limitation imposed by the Debt Act upon the incurring of debt by the Township.

SECTION 16. Federal Tax Covenants. The Township hereby covenants with the holders from time to time of the Bonds that it will at all times do and perform all actions and things within its power which are necessary or desirable in order to assure that interest paid on the Bonds will, for purposes of federal income taxation, be and remain excludable from the gross income of the recipients thereof and that it will refrain from doing or performing any act or thing that would cause such interest not to be so excludable and to otherwise comply with the requirements of Sections 103 and 141 through 150 of the Internal Revenue Code of 1986, as amended (the "Code"). The Township further covenants with the holders from time to time of the Bonds that it will make no investment or other use of the proceeds of the Bonds, which, if such investment or use had been reasonably expected on the date of issuance of the Bonds would cause the Bonds to be "arbitrage bond(s)" within the meaning of Section 148 of the Code, and the regulations applicable thereto and that this covenant shall extend throughout the term of the Bonds and shall apply to all amounts which are proceeds of the Bonds for purposes of said section and regulations. Neither the Treasurer nor any other official or agent of the Township shall make any investment inconsistent with the foregoing covenant. The Treasurer and all other Township officials responsible for investment shall request and follow, if given, the advice or direction of bond counsel for the Township (the "Bond Counsel") as to investments, which may be made in compliance with this covenant. The appropriate officers of the Township are hereby authorized to execute a tax compliance agreement (the "Tax Compliance Agreement") to carry out the foregoing covenants.

The Tax Compliance Agreement shall be substantially in the form acceptable to Bond Counsel, with such changes as may be approved by the officer executing the Tax Compliance Agreement, upon the advice of Bond Counsel, such approval to be conclusively evidenced by such officer's execution of the Tax Compliance Agreement. If required under the Tax Compliance Agreement, there shall be established a "bond rebate fund," which shall be held and maintained

by the Township in accordance with the Tax Compliance Agreement, separate and apart from other funds of the Township. The foregoing tax covenants in this Section 16 may be excused or modified if, and to the extent that, the Township receives an opinion of nationally recognized bond counsel that such absence of compliance will not adversely affect the exemption from federal income taxation of interest on the Bonds.

SECTION 17. Covenant to Provide Continuing Disclosure. The Township is hereby authorized and directed to execute and deliver to the purchaser of the bonds a continuing disclosure certificate or agreement (the “Continuing Disclosure Agreement”), pursuant to which the Township shall covenant with the holders from time to time of the Bonds, in accordance with Rule 15c2-12 of the Securities and Exchange Commission (“Rule 15c2-12”), to provide to the Municipal Securities Rulemaking Board’s Electronic Municipal Market Access (“EMMA”) system or such depository specified pursuant to Rule 15c2-12, on an annual basis, certain financial and other information concerning the Township and notices, from time to time as applicable, of certain specified events as required to be disclosed by Rule 15c2-12, and as set forth in the Continuing Disclosure Agreement. The Chairperson, Vice Chairperson and other proper officers of the Township are hereby authorized to execute and deliver the Continuing Disclosure Agreement containing such provisions as are deemed necessary or appropriate by Bond Counsel and as are approved by such officers of the Township executing the same.

SECTION 18. Approval of Official Statement. The appropriate officers of the Township authorized by this Ordinance to accept the final terms of the Bonds are hereby authorized to approve a Preliminary Official Statement together with any necessary supplements or amendments thereto (collectively, the “Preliminary Official Statement”) for the Bonds in the form to be prepared in connection with any public offering and sale of the Bonds, and such Preliminary Official Statement as so approved shall be “deemed final” by the Township as of its date for purposes of Rule 15c2-12. A final Official Statement together with any necessary supplements and amendments thereto (collectively, the “Official Statement”) setting forth the final terms of the Bonds within the Bond Parameters as accepted by the Township, substantially in the form of the Preliminary Official Statement approved by the appropriate officers of the Township in accordance with the foregoing provisions with such additions and other changes, if any, as may be approved by the appropriate officers of the Township with the advice of the Township Solicitor and containing the final terms of the Bonds, shall be prepared and delivered to the underwriter, bond purchaser, or other persons, as appropriate, within seven (7) business days from the date of the sale of the Bonds, and the Township hereby approves the use thereof in connection with the public offering and the sale of the Bonds.

SECTION 19. Bond Insurance. If the Bond Purchase Agreement and the Addendum to the Bond Purchase Agreement so provides, and lower interest costs would thereby be available to the Township, the officers of the Township are hereby authorized to purchase a policy of insurance guaranteeing the payment of the principal of and interest on all or a portion of the Bonds, to pay the premium for such policy from the proceeds of the Bonds and to execute such documents as may be necessary to effect the issuance of such policy. If applicable, the Bonds issued under this Ordinance may include a statement of the terms of such insurance policy and the Authentication Certificate of the Paying Agent appearing on each Bond may include a statement confirming that the original or a copy of the insurance policy is on file with the Paying Agent.

SECTION 20. Application of Bond Proceeds. The purchase price of the Bonds and any accrued interest payable by the Purchaser shall be paid to the Paying Agent on behalf of the Township. In addition, the Township shall deposit with the Paying Agent any bid security and make such additional deposits of cash from the funds of the Township as shall be necessary to cover all of the issuance costs of the Bonds. Upon receipt of such funds, the Paying Agent shall deposit the same in a settlement account. From the settlement account, the Paying Agent shall make the deposits and disbursements set forth on the Closing Statement executed by the proper officers of the Township for the payment on behalf of the Township of the costs of issuance and sale of the Bonds, upon presentation of proper invoices therefor, and shall deposit the remaining proceeds of the Bonds in a project fund or other segregated account established by the Township in order to fund the capital repairs and improvements authorized as part of the Project in these resolutions.

SECTION 21. Reimbursement. The Township hereby declares its official intent that it may reimburse itself from proceeds of the Bonds for part or all of (a) those costs of the Project which have been paid by the Township during the sixty (60) day period ending on the date of this Ordinance and (b) such other costs of the Project which have been paid by the Township prior to the date of this Ordinance and which may properly be reimbursed from proceeds of the Bonds pursuant to Treasury Regulation 1.150-2.

SECTION 22. Qualified Tax-Exempt Obligations. In accordance with the Internal Revenue Code of 1986, as amended (the "Code"), the Township hereby represents and warrants, after due investigation and to the best of its knowledge, that: (i) the Bonds are not "private activity bonds" within the meaning of Section 141 of the Code, and (ii) the aggregate face amount of "qualified tax-exempt obligations" within the meaning of Section 265(b)(3)(B) of the Code (which includes qualified 501(c)(3) bonds but not any other private activity bonds) issued or to be issued by the Township (and all other issuers which must be aggregated with the Township pursuant to the Code) during the 2025 calendar year (including the Bonds, but excluding current refunding obligations not required to be taken into account for purposes of that Section of the Code and further excluding those bonds "deemed designated" under the Code), is not reasonably expected to exceed \$10,000,000. To the extent that the Bonds are not "deemed designated," the Township may, through its authorized officers, designate each of the Bonds as a "qualified tax-exempt obligation" within the meaning of Section 265(b)(3)(B) of the Code. The Township hereby authorizes the proper officers of the Township to execute a certificate containing such a designation prior to, at the time of, the closing of each series of Bonds issued pursuant to this Ordinance, as applicable.

SECTION 23. Further Actions. The Chairperson and Secretary of the Board or the Vice Chairperson and Assistant Secretary, in the absence of the Chairperson or Secretary, respectively, or any duly appointed successors, as the case may be, are hereby authorized to execute any agreements, instruments or documents and to do or cause to be done any and all acts and things deemed necessary or appropriate for the carrying out of the purposes of this Ordinance and to comply with the Debt Act.

SECTION 24. Act Applicable to Bonds. This Ordinance is adopted pursuant to, and the Bonds issued hereunder shall be subject to, the provisions of the Debt Act and all the mandatory provisions thereof shall apply hereunder whether or not explicitly stated herein.

SECTION 25. Contract with Bond Owners. This Ordinance constitutes a contract with the registered owners of the Bonds outstanding hereunder and shall be enforceable in accordance with the provisions of the laws of the Commonwealth of Pennsylvania.

SECTION 26. Appointment of Financial Team. The Township hereby appoints the following for the issuance of the Bonds: RBC Capital Markets, LLC as Underwriter and Obermayer Rebmann Maxwell & Hippel LLP as Bond Counsel.

SECTION 27. Severability. In the event any provision, section, sentence, clause or part of this Ordinance shall be held to be invalid, such invalidity shall not affect or impair any remaining provision, section, sentence, clause or part of this Ordinance, it being the intent of the Township that such remainder shall be and shall remain in full force and effect.

SECTION 28. Repealer. All prior ordinances or parts thereof inconsistent herewith are hereby repealed.

SECTION 29. Effective Date. This Ordinance shall take effect on the earliest date permitted by the Debt Act.

(Signature page follows)

UPPER UWCHLAN TOWNSHIP

Attest: _____
Name:
Title:

By: _____
Jennifer F. Baxter
Chairperson

SCHEDULE "A"

BOND PRINCIPAL, MAXIMUM INTEREST RATES, MATURITY SCHEDULE

UPPER UWCHLAN TOWNSHIP
Chester County, Pennsylvania
General Obligation Bonds

Summary

Maximum Aggregate Principal Amount \$9,890,000

Principal Maturity (or Mandatory Redemption): December 1

Maximum Annual Principal Payment Amount	Maximum Coupon and Maximum Yield to Maturity	Principal Maturity or Mandatory Sinking Fund Payment Year
\$280,000	6.000%	2026
345,000	6.000%	2027
360,000	6.000%	2028
375,000	6.000%	2029
390,000	6.000%	2030
405,000	6.000%	2031
420,000	6.000%	2032
435,000	6.000%	2033
450,000	6.000%	2034
470,000	6.000%	2035
490,000	6.000%	2036
510,000	6.000%	2037
530,000	6.000%	2038
550,000	6.000%	2039
575,000	6.000%	2040
605,000	6.000%	2041
630,000	6.000%	2042
660,000	6.000%	2043
690,000	6.000%	2044
720,000	6.000%	2045

SCHEDULE "B"

FORM OF BONDS

Unless this certificate is presented by an authorized representative of The Depository Trust Company, a New York corporation ("DTC"), to the Issuer or its agent for registration of transfer, exchange, or payment, and any certificate issued is registered in the name of Cede & Co. or in such other name as is requested by an authorized representative of DTC (and any payment is made to Cede & Co. or to such other entity as is requested by an authorized representative of DTC). ANY TRANSFER, PLEDGE, OR OTHER USE HEREOF FOR VALUE OR OTHERWISE BY OR TO ANY PERSON IS WRONGFUL in as much as the registered owner hereof, Cede & Co., has an interest herein.

NO: R-

\$000,000

**UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY, PENNSYLVANIA
GENERAL OBLIGATION BONDS
SERIES OF 2025**

<u>INTEREST RATE</u>	<u>MATURITY DATE</u>	<u>DATED DATE</u>	<u>CUSIP</u>
0.000%	_____, 20__	Date of Delivery	

REGISTERED OWNER: CEDE & CO.

PRINCIPAL SUM: DOLLARS

UPPER UWCHLAN TOWNSHIP, Chester County, Commonwealth of Pennsylvania (the "Township"), for value received, hereby promises to pay to the registered owner named above, or registered assigns, on the maturity date specified above, unless this Bond shall have been previously called for redemption in whole or in part and payment of the redemption price shall have been duly made or provided for, the principal sum shown above, and to pay interest thereon calculated on the basis of a 360-day year, at the annual rate specified above from the interest payment date next preceding the date of registration and authentication of this Bond, unless: (a) this Bond is registered and authenticated as of an interest payment date, in which event it shall bear interest from such interest payment date; or (b) this Bond is registered and authenticated after a Record Date (as hereinafter defined) and before the next succeeding interest payment date, in which event it shall bear interest from such interest payment date; or (c) this Bond is registered and authenticated on or prior to the Record Date preceding the first interest payment date, in which event it shall bear interest from its Date of Delivery; or (d) as shown by the records of the Paying Agent, interest on this Bond shall be in default, in which event it shall bear interest from the date to which interest was last paid on this Bond. Interest shall be paid initially on [_____] and thereafter, semi-annually on [____ 1] and [____ 1] of each year until the principal sum is paid. The principal or redemption price of and interest on this Bond may be

paid in any coin or currency of the United States of America, which, at the time of payment, is legal tender for the payment of public or private debts.

The principal or redemption price of this Bond is payable upon presentation and surrender hereof at the principal corporate office of Fulton Bank, N.A., as Paying Agent (the "Paying Agent"). Interest shall be paid by check mailed to the registered owner hereof as shown on the registration books kept by the Paying Agent as of the close of business on the applicable Record Date, or at the election of such registered owner of this Bond in a denomination of \$500,000 or more, by wire transfer to a designated account, provided that any such election shall be received by the Paying Agent in writing not less than ten (10) days prior to the first payment of interest to which it relates.

Interest on this Bond is payable by check or draft drawn on the Paying Agent which shall be mailed to the registered owner whose name and address shall appear at the close of business on the fifteenth (15th) day of the calendar month (whether or not a day on which the Paying Agent is open for business) next preceding each interest payment date (the "Record Date"), on the registration books maintained by the Paying Agent, irrespective of any transfer or exchange of this Bond subsequent to such Record Date and prior to such interest payment date, unless the Township shall be in default in payment of interest due on such interest payment date. In the event of any such default, such defaulted interest shall be payable to the person in whose name the Bond is registered at the close of business on a special record date for the payment of such defaulted interest established by notice mailed by the Paying Agent to the registered owners of this Bond not less than ten (10) days preceding such special record date, but not more than thirty (30) days prior to the payment of such defaulted interest. Such notice shall be mailed to the person in whose name this Bond is registered at the close of business on the fifth (5th) day preceding the date of mailing.

If the date for payment of the principal of or interest on this Bond shall be a Saturday, Sunday, legal holiday or a day on which banking institutions in the Commonwealth of Pennsylvania are authorized by law or executive order to close, then the date for payment of such principal or interest shall be the next succeeding day which is not a Saturday, Sunday, legal holiday or a day on which such banking institutions are authorized to close, and payment on such date shall have the same force and effect as if made on the nominal date established for such payment.

[This Bond is not insured.]

This Bond is one of a duly authorized issue of \$9,890,000 aggregate principal amount of General Obligation Bonds, Series of 2025 (the "Bonds") of the Township consisting of Bonds in the denomination of \$5,000 or any integral multiple thereof. The Bonds are issued in accordance with the Local Government Unit Debt Act of the Commonwealth of Pennsylvania, as codified by the Act of December 19, 1996 (P.L. 1158, No. 177) (the "Act"), without the assent of the electors, pursuant to an Ordinance of the Board of Supervisors of the Township adopted August 12, 2025 (the "Ordinance") which authorized the issuance of the Bonds. The Bonds have been issued by the Township for the purposes of financing the costs of a project (the "Project") consisting of providing funds to finance (1) the acquisition of real property located within the Township at 144/128 Byers Road (the "Property") renovations to the Property and various other capital improvements to the Property; (2) renovations and capital improvements to the facilities located

within the Township's Hickory Park and various other capital projects that benefit the Township; (3) repairs and improvements to the Township's sewer system and related infrastructure; and (4) the payment of the costs and expenses of issuing the Bonds.

Reference is hereby made to the Ordinance and the Act for a complete statement of the right of the holders hereof, which by acceptance of this Bond, such holder accepts. Reference is also made to the further provisions of this Bond set forth on the reverse side hereof, which provisions shall for all purposes have the same effect as if set forth in full herein.

This Bond shall not be valid or become obligatory for any purpose unless the Certificate of Authentication shall have been signed by the manual signature of an authorized signatory of the Paying Agent.

[Remainder of page intentionally blank.]

IN WITNESS WHEREOF, the Township has caused this Bond to be executed in its name by the facsimile signature of its Chairperson and attested by the facsimile signature of its Secretary.

UPPER UWCHLAN TOWNSHIP

Attest: _____
Name:
Title:

By: _____
Jennifer F. Baxter
Chairperson

OPTIONAL REDEMPTION

The Bonds stated to mature on or after [] are subject to redemption prior to maturity at the option of the Township in any order of maturities either as a whole, or in part, at any time on or after [], and, if in part, by lot within a maturity, at a redemption price equal to 100% of the principal amount thereof, together with accrued interest to date fixed for redemption.

MANDATORY REDEMPTION

The Bonds maturing on [] of the years 20[] and 20[] (the “Term Bonds”) are subject to mandatory redemption prior to maturity, in part, by lot from monies to be deposited in the sinking fund created under the Ordinance at a redemption price of 100% of the principal amount thereof plus accrued interest to the redemption date. The Township covenants that it will or will cause the Paying Agent to select by lot, give notice of redemption, and redeem Term Bonds at said redemption price in the principal amount, as outlined in the Ordinance, on [] of the following years in the following amounts:

Bonds stated to mature [] 1, 20 []

NOTICE OF REDEMPTION

Notice of any redemption shall be given by depositing a copy of the redemption notice by first class mail, postage prepaid, not less than thirty (30) days nor more than forty-five (45) days prior to the date fixed for redemption addressed to each of the registered owners of Bonds to be redeemed, in whole or in part, at the addresses shown on the registration books maintained by the Paying Agent. Such notice shall be given in the name of the Township, shall identify the Bonds to be redeemed (and, in the case of a partial redemption of any Bonds, the respective principal amount thereof to be redeemed), shall specify the redemption date and the redemption price, and shall state that on the redemption date the Bonds called for redemption will be payable at the designated corporate trust office of the Paying Agent and that from the date of redemption interest will cease to accrue. The Paying Agent shall use "CUSIP" numbers (if then generally in use) in notices of redemption as a convenience to Bond owners, provided that any such notice shall state that no representation is made as to the correctness of such numbers either as printed on the Bonds or as contained in any notice of redemption and that reliance may be placed only on the identification numbers prefixed "R-" printed on the Bonds. Failure to give such notice by mailing, or any defect therein or in the mailing thereof, shall not affect the validity of any proceeding for redemption of other Bonds called for redemption as to which proper notice has been given.

With respect to any optional redemption of Bonds, if at the time of mailing such notice of redemption, the Township shall not have deposited with the Paying Agent moneys sufficient to redeem all the Bonds called for redemption, such notice may state that it is conditional, that is, subject to the deposit of the redemption moneys with the Paying Agent not later than the redemption date, and such notice shall be of no effect unless such moneys are so deposited.

On the date designated for redemption, notice having been provided as aforesaid, and money for payment of the principal and accrued interest being held by the Paying Agent, interest on the Bonds or portions thereof so called for redemption shall cease to accrue and such Bonds or portions thereof shall cease to be entitled to any benefit or security under the Ordinance and registered owners of such Bonds or portions thereof so called for redemption shall have no rights with respect to such Bonds, except to receive payment of the principal of and accrued interest on such Bonds to the date fixed for redemption.

If a Bond is of a denomination larger than \$5,000, a portion of such Bond may be redeemed. For the purposes of redemption, a Bond shall be treated as representing that number of Bonds which is obtained by dividing the principal amount thereof by \$5,000, each \$5,000 portion of such Bond being subject to redemption. In the case of partial redemption of a Bond, payment of the redemption price shall be made only upon surrender of such Bond in exchange for Bonds of the same maturity and in authorized denominations in aggregate principal amount equal to the unredeemed portion of the principal amount thereof.

If the redemption date for any Bonds shall be a Saturday, Sunday, legal holiday or a day on which banking institutions in the Commonwealth of Pennsylvania are authorized by law or executive order to close, then the date for payment of the principal, if any, and interest upon such redemption shall be the next succeeding day which is not a Saturday, Sunday, legal holiday or a

day on which such banking institutions are authorized to close, and payment on such date shall have the same force and effect as if made on the nominal date of redemption.

If the Township deposits with the Paying Agent funds sufficient to pay the principal or redemption price of any Bonds becoming due at maturity, by call for redemption or otherwise, together with interest accrued to the due date, interest on such Bonds will cease to accrue on the due date, and thereafter the holders shall have no rights with respect thereto, except to receive payment of principal to be redeemed and accrued interest thereon to the date fixed for redemption.

This Bond is registered as to both principal and interest on the registration books to be kept for that purpose at the principal office of the Paying Agent, and both principal and interest shall be payable only to the registered owner hereof. This Bond is transferable or exchangeable by the registered owner thereof upon surrender of the Bonds to the Paying Agent, at its principal corporate trust office, accompanied by a written instrument or instruments in form, with instructions, and with guaranty of signature satisfactory to the Paying Agent, duly executed by the registered owner of such Bond in the registration books, or his/her attorney-in-fact or legal representative. The Paying Agent shall enter any transfer of ownership of Bonds in the registration books and shall authenticate and deliver in the name of the transferee or transferees a new fully registered bond or bonds of authorized denominations of the same series, maturity and interest rate for its aggregate principal amount which the registered owner is entitled to receive at the earliest practicable time. The Township and the Paying Agent shall not be required to issue, or register the transfer or exchange of any Bond: (a) during the period beginning at the opening of business on any Record Date for interest payments and ending at the close of business on such interest payment date; (b) during the beginning of the opening of business on the fifteenth (15th) business day next preceding any date of selection of Bonds to be redeemed and ending at the close of business on the day on which the applicable notice of redemption is mailed; (c) during the period beginning at the opening of business on the first business day next succeeding the business day the Paying Agent determines the registered owners of the Bonds to receive notice of any special record date and the close of business on the special record date; or (d) after it has been selected or called for redemption, in whole or in part. Bonds may be exchanged for a like aggregate principal amount of Bonds of other authorized denominations of the same series, maturity and interest rate. The Township and Paying Agent may deem and treat the registered owner of this Bond as the absolute owner hereof for all purposes, whether or not this Bond shall be overdue, and shall not be affected by any notice to the contrary.

This Bond is not valid unless the Paying Agent Authentication Certificate endorsed hereon is duly executed.

The Township, pursuant to recommendations made by the Committee on Uniform Security Identification Procedures, has caused CUSIP numbers to be printed on the Bonds, and has directed the Paying Agent to use such numbers in notices of redemption and other notices, if any, as a convenience to bondholders. No representation is made as to the accuracy of such numbers either as printed on the Bonds or as contained in any notice and reliance may be placed only on the identification number printed hereon.

No recourse shall be had for the payment of principal of, or interest on this Bond, or for any claim based hereon or on the Ordinance against any director, officer, or employee, past, present, or future, of the Township or any such successor body, under any constitutional provision, statute or rule of law, or by the enforcement of any assessment or by any legal or equitable proceeding or otherwise, and all such liability of such directors, officers or employees is released as a condition of and as consideration for the issuance of this Bond.

It is hereby certified that the approval of the Department of Community and Economic Development of the Commonwealth of Pennsylvania for the Township to issue and deliver this Bond has been duly given pursuant to the Act; that all acts, conditions and things required by the laws of the Commonwealth of Pennsylvania to exist, to have happened or to have been performed precedent to or in the issuance of this Bond or in the creation of the debt of which this Bond is evidence, exist, have happened and have been performed in regular and due form and manner as required by law; that this Bond, together with all other indebtedness of the Township, is within every debt and other limit prescribed by the Constitution and the statutes of the Commonwealth of Pennsylvania; and that the Township has established with the Paying Agent as Sinking Fund Depository a sinking fund for the Bonds and shall deposit therein amounts sufficient to pay the principal of and interest on the Bonds as the same shall become due and payable.

This Bond is hereby declared to be a general obligation of the Township. The Township, in the Ordinance authorizing the issuance of the series of Bonds of which this Bond is one, has covenanted with the holders from time to time, of the Bonds of said series that, to the fullest extent authorized by law, the Township will include the amount of the debt service charges on the same for each fiscal year in which such sums are payable, in its budget for that year, that it will appropriate such amount from its general revenues necessary for the payment of such debt service, and will duly and punctually pay or cause to be paid from the Sinking Fund or any other of its revenues or funds, the principal of and interest due upon every Bond, at the dates and places and in the manner stated in the Bonds, according to the true intent and meaning thereof, and for such budgeting, appropriation and payment, the Township has irrevocably pledged its full faith, credit and taxing power. This covenant is specifically enforceable.

[Remainder of page intentionally blank]

AUTHENTICATION CERTIFICATE

This Bond is one of Upper Uwchlan Township's General Obligation Bonds, Series of 2025 described in the within mentioned Ordinance. Attached hereto is the complete text of the opinion of Obermayer Rebmann Maxwell & Hippel LLP, Centre Square West, 1500 Market Street, Suite 3400, Philadelphia, Pennsylvania 19102, Bond Counsel to Upper Uwchlan Township, a signed copy of which is on file with the undersigned, which was dated the date of initial delivery of, and payment for, the Bonds.

FULTON BANK, N.A., or a corporate
affiliate thereof, as Paying Agent

By: _____

Name:

Title:

Date of Authentication: _____, 2025

(FORM OF ASSIGNMENT)
ASSIGNMENT

For value received _____ hereby sells, assigns and transfers unto _____, # _____ (please insert social security or taxpayer-identification number) the within Bond issued by the _____ and all rights thereunder, hereby irrevocably appointing _____ Attorney to transfer said Bond on the Bond Register, with full power of substitution in the premises.

Dated: _____

Signature Guaranteed: _____

Notice: Signature(s) must be guaranteed by an approved eligible guarantor institution, an institution which is a participant in a Securities Transfer Association recognized signature guarantee program.

Notice: The assignor's signature to this assignment must correspond with the name as it appears upon the face of the within Bond in every particular without alteration or any change whatsoever.

(FORM OF ABBREVIATIONS)

The following abbreviations, when used in the inscription on the face of the within Bond shall be construed as though they were written out in full according to applicable laws or regulations:

TEN COM – as tenants in common
TEN ENT – as tenants by the entireties
JT TEN – as joint tenants with the right of survivorship and not as tenants in common

UNIFORM GIFT MIN ACT - _____ Custodian _____
(Cust) (Minor)

under Uniform Gifts to Minors Act _____
(State)

Additional abbreviations may also be used though not in the above list.

BOND PURCHASE AGREEMENT

CERTIFICATE

I, the undersigned, Secretary of the Board of Supervisors of Upper Uwchlan Township, Chester County, Pennsylvania (the "Township"), certify that: the foregoing is a true and correct copy of an Ordinance that was duly adopted by affirmative vote of a majority of all members of the Board of Supervisors of the Township at a meeting duly held on the 12th day of August 2025; said Ordinance has been duly recorded in the minute book of the Board of Supervisors of the Township; a notice with respect to the intent to adopt said Ordinance has been published as required by law; said Ordinance was available for inspection by any interested citizen requesting the same in accordance with the requirements of the Local Government Unit Debt Act of the Commonwealth of Pennsylvania and such notice; and said Ordinance has not been amended, altered, modified or repealed as of the date of this Certificate.

I further certify that the Board of Supervisors of the Township met the advance notice requirements of Act No. 175 of the General Assembly of the Commonwealth of Pennsylvania, approved July 19, 1974, as amended, by advertising the time and place of said meeting and by posting prominently a notice of said meeting at the public building in which said meeting was held.

I further certify that: the total number of members of the Board of Supervisors of the Township is three (3); the vote of members of the Board of Supervisors upon said Ordinance was called and duly was recorded upon the minutes of said meeting; and members of the Board of Supervisors of the Township voted upon said Ordinance in the following manner:

Name

Vote

Mrs. Jennifer F. Baxter, Chairperson
Mr. Andrew P. Durkin, Vice Chairperson
Mrs. Sandra M. D'Amico

IN WITNESS WHEREOF, I set my hand and affix the official seal of the Township this 12th day of August, 2025.

UPPER UWCHLAN TOWNSHIP

(SEAL)

By: _____
Name:
Title:



UPPER UWCHLAN TOWNSHIP
CHESTER COUNTY, PENNSYLVANIA
ORDINANCE _____

**AN ORDINANCE OF THE TOWNSHIP OF UPPER UWCHLAN,
CHESTER COUNTY, PENNSYLVANIA, AMENDING CHAPTER 176
OF THE UPPER UWCHLAN TOWNSHIP CODE TITLED “VEHICLES
AND TRAFFIC” TO AMEND SECTION 176-7 TITLED “TECHNICAL
SPECIFICATIONS FOR TRAFFIC CONTROL SIGNALIZATION” TO
ADOPT AN UPDATED VERSION.**

NOW THEREFORE, BE IT ENACTED AND ORDAINED by the Board of Supervisors of Upper Uwchlan Township that Chapter 176 of the Upper Uwchlan Township Code, titled “Vehicles and Traffic”, shall be amended as follows:

SECTION 1. Section 176-7, titled, “Technical specifications for traffic control signalization” shall be amended as follows:

“The technical specifications for traffic control signalization as prepared by Bowman Consulting Group, Ltd. dated January 14, 2025, which is attached hereto and incorporated herein by reference shall be adopted and strictly followed for all traffic signal construction projects in the Township.”

SECTION 2. Severability. If any sentence, clause, section, or part of this Ordinance is for any reason found to be unconstitutional, illegal, or invalid, such unconstitutionality, illegality or invalidity shall not affect or impair any of the remaining provisions, sentences, clauses, sections, or parts hereof. It is hereby declared as the intent of the Board of Supervisors that this Ordinance would have been adopted had such unconstitutional, illegal, or invalid sentence, clause, section, or part thereof not been included herein.

SECTION 3. Repealer. All ordinances or parts of ordinances conflicting with any provision of this Ordinance are hereby repealed insofar as the same affects this Ordinance.

SECTION 4. Effective Date. This Ordinance shall become effective immediately upon adoption as by law provided.

ENACTED AND ORDAINED this ____ day of _____, 2025.

ATTEST:

**UPPER UWCHLAN TOWNSHIP
BOARD OF SUPERVISORS**

Gwen A. Jonik, Secretary

Jennifer F. Baxter, Chair

Andrew P. Durkin, Vice-Chair

Sandra M. D’Amico, Member

TECHNICAL SPECIFICATIONS
for
TRAFFIC CONTROL SIGNALIZATION

**Upper Uwchlan Township
Chester County, Pennsylvania**

Prepared for:
Upper Uwchlan Township

Prepared by:
Bowman Consulting Group, Ltd. (Bowman)

January 2024
Revised: January 14, 2025 (*revision in italics*)

<u>SECTION</u>	<u>PAGE</u>
1-0 LISTING OF REFERENCE MATERIAL	1
2-0 SUMMARY	2
3-0 REQUIREMENTS AT ALL LOCATIONS WITHIN THE TOWNSHIP	3
General Requirements	3
Utility Notification	3
Inspection of Traffic Signal Work	3
Required Submittals	6
Acceptance of Traffic Signal Installation by Township	7
4-0 TECHNICAL SPECIFICATIONS	9
General	9
Construction Methods	9
Materials	9
A. Traffic Signal Controller Assembly	10
B. Traffic Signal Supports	19
C. Vehicular Detection	20
D. Pedestrian Detection	20
E. Pavement Markings	21
F. Signal Head Assemblies with LED Indications	21
G. Fiber Optic Traffic Control interconnect Cable	22
H. Fiber Optic Data Link	23
I. Emergency Preemption System	24
J. Uninterruptible Power Supply	24
K. Traffic Control Signs	25
L. Wiring, Conduit, and Junction Boxes	25
M. Generator/Utility Transfer Assembly	26

APPENDICES

APPENDIX A - *Material List*

REFERENCE MATERIALS

References should be made to the following Pennsylvania Department of Transportation (PENNDOT) publications:

- **PENNDOT Publication 46, Traffic Engineering Manual, dated March 3, 2014, or latest revision, and its addenda**
- **PENNDOT Publication 111, Traffic Control – Pavement Markings and Signing Standards (TC8600 and TC-8700 series), dated August 17, 2021, or latest revision, and its addenda**
- **PENNDOT Publication 148, Traffic Standards - Signals (TC-8800 series), dated December 12, 2011, or latest revision, and its addenda**
- **PENNDOT Publication 149, Traffic Signal Design Handbook, dated May 2013, or latest revision, and its addenda**
- **PENNDOT Publication 191, Traffic Signal Maintenance Manual, dated December 2020, or latest revision, and its addenda**
- **PENNDOT Publication 212, Official Traffic Control Devices, dated March 2006, or latest revision, and its addenda**
- **PENNDOT Publication 213, Temporary Traffic Control Guidelines, dated April 2022, or latest revision, and its addenda**
- **PENNDOT Publication 408, Specifications, dated April 10, 2020, latest revision, and its addenda**

References should also be made to the following national publications:

- **National Electrical Manufacturer's Association (NEMA), Standards Publication No. TS2, 2016**
- **National Transportation Communications for ITS Protocol (NTCIP) Standards, current edition, as noted.**
- **2020 American Association of State Highway and Transportation Officials (AASHTO) criteria specified.**
- **ANSI/TIA/EIA-568-B, Commercial Building Telecommunications Cabling Standard, dated 2017 or latest revision.**

SUMMARY

This specification implements traffic signal control standards throughout Upper Uwchlan Township to provide a standardization of traffic signal equipment, facilitate a closed-loop system, and to reduce long-term maintenance costs. These standards pertain to materials, construction methods, traffic signal controller assembly, and traffic signal supports and shall be strictly followed for all traffic signal construction projects.

The purpose of this specification is to provide design engineers and qualified traffic signal contractors with the general requirements and administrative details governing the furnishing and installation of traffic signals, signs, and pavement markings within Upper Uwchlan Township.

REQUIREMENTS AT ALL LOCATIONS WITHIN THE TOWNSHIP

GENERAL REQUIREMENTS

Special provisions for vendor specific equipment and non-standard PennDOT items have been included as part of this document. It is the responsibility of the Contractor to contact the Township to determine which special provisions apply for a particular installation.

Contractor shall furnish all materials, implements, machinery, equipment, tools, supplies, transportation, labor and supervision required to construct the facilities as shown on contract drawings and specified herein. It shall be the responsibility of the Contractor to proceed with the work in the matter such limitations as may be prescribed by the Engineers. Construction methods should conform to the latest editions of PennDOT Publication 408 and Traffic Control 8800 Series, unless specified in the Township specifications.

It is the responsibility of the entity installing the traffic signal to secure all necessary PennDOT Highway Occupancy Permit and PennDOT Traffic Signal Permit approvals before work begins.

All traffic signal Contractors working in the Township must be PennDOT pre-qualified to perform traffic signal work. All Contractor personnel setting up work zone controls and performing flag duties must be IMSA Work-Zone Safety certified. All personnel performing electrical work must be IMSA Traffic Signal Technician Level I certified as a minimum requirement. A supervisor must be on-site during the installation of any electrical work and said supervisor must be IMSA Traffic Signal Technician Level II certified. Furnish the Township with a copy of said certificates.

UTILITY NOTIFICATION

The Contractor is responsible for notifying the Pennsylvania One-Call System in accordance with the latest amendment to PA Act 287 of 1974. It is also the Contractors responsibility to protect all utilities from damage during work that has been marked through the PA One-Call System. The Contractor shall notify the Township ten (10) working days prior to any excavation work to verify any local utilities which may be in the excavation area. When a utility is damaged the Contractor must stop all work and notify the appropriate utility and the Township Representative. The Contractor shall be responsible for any damage caused to marked utilities and for any damage caused to unmarked utilities after the initial damage occurs.

INSPECTION OF TRAFFIC SIGNAL WORK

It shall be the responsibility of the Contractor to arrange a pre-construction meeting with the Township on-site to spot signal poles, controller cabinet pad, and vehicle detector locations in the field. The

Section 3-0: Requirements at all Locations within the Township

Township will notify the appropriate parties at PennDOT of the meeting. No work may begin until this meeting has been held and the intended scope of the signal installation is satisfactory to all parties. The contractor must submit a project schedule to the Township at the pre-construction meeting. Once work has begun, the Contractor shall be responsible for notifying the Township 24 hours prior to the following, so that a representative of the Township may be on-site to inspect the following work:

- Installation of signal pole foundations before concrete is placed.
- All concrete pours with full array of testing including making of test cylinders.
- Trenching and conduit installation before backfilling occurs.
- Installation of ground rods for mast arms and controller assemblies.
- Placement of pedestrian pushbutton poles.
- Mast arm installation for load testing (rotational capacity) of connecting bolt per PennDOT 408 Section 1105.02(d).

Contractor shall test all ground rods in the presence of the Township. **Resistance to ground shall be 10 ohms or less.** Copies of the ground rod tests shall be provided to the Township and shall be placed in the controller cabinet. Testing shall be performed with an approved ohm meter for testing earth-ground resistance.

Contractor shall test all switches and push buttons located within the switch compartment in the presence of the Township.

Contractor shall demonstrate operation on non-invasive vehicle detection system in the presence of the Township and/or their representative.

REQUIRED SUBMITTALS

1. All materials must be provided to the Township electronically and approved before construction begins. Include the type of material, manufacturer's name, model number, and PennDOT Approval Numbers (where applicable) for each item to be supplied for review and approval.
2. Provide pole manufacturer mast arm shop drawing and calculations electronically.
3. Provide malfunction management unit bench test certification in controller cabinet and electronically.
4. Provide electronic set of as-built record plans to the Township upon final inspection and acceptance of the signal.
5. Provide three (3) copies of the cabinet wiring diagram to the Township as per PennDOT 408 Section 954.3(k) upon final inspection and acceptance of the signal.

6. Provide electronic copies of the following information for all equipment that has a PennDOT approval number, upon delivery of the controller assembly to the site: Brand name, model name/number, PennDOT approval number, serial number and warranty date (beginning and end).

ACCEPTANCE OF TRAFFIC SIGNAL INSTALLATION BY TOWNSHIP

1. THIRTY-DAY (30) TESTING PERIOD

Upon completion of the Scope of Work, a **THIRTY-DAY (30) TESTING PERIOD** shall begin. The contractor shall hold a meeting with the PennDOT Traffic Unit and the Township to begin the thirty-day testing period. During this period, the Contractor must commence repairs for signal flash or blackout within 1 hour of notification by the Township. Also, the Contractor must correct all failures by repairing or replacing malfunctioning parts or equipment or faulty workmanship, regardless of the cause, within 24 hours after having been notified by the Township. The contractor will be responsible to supply a spare controller unit, MMU, and/or detectors or cameras during the repair period. In addition, during this period the Contractor must guarantee the satisfactory in-service operation of mechanical and electrical equipment, related components, signing, pavement markings, and the controller assembly, regardless of the cause for unsatisfactory operation. After correcting failures for any reason, the thirty (30) day testing period shall be re-started. Guarantee repairs or replacements for the balance of the maintenance period, or 15 days, whichever is the longer period.

2. PRE-DEDICATION MAINTENANCE PERIOD

Upon satisfactory completion of the Thirty-Day (30) Testing Period, a **MAINTENANCE PERIOD** shall begin. The Maintenance Period shall be effective for a minimum period of one-year or as described in the Land Development Agreement from satisfactory completion of the Thirty-Day (30) Testing Period or until satisfactory **completion of all necessary roadway improvements as part of the land development agreement process, whichever is longer**. During the Maintenance Period, the Contractor shall:

- Maintain equipment in the controller cabinet, using additional locks as necessary to prevent entry by others; supply a set of keys for the additional lock(s) to the Township in case of an emergency.
- Repair faulty workmanship, repair or replace defective materials or equipment and correct malfunctions in the controller cabinet within 48 hours after commencing repairs.
- Commence repairs no later than the working day following notification of failures or malfunctions, except for failures or malfunctions which result in a signal flash or blackout in which case repairs must commence within 1 hour upon notification by the Township.
- If the Contractor does not report on-site within 2 hours following notification of a signal flash or blackout, the Township shall have the option to commence repairs through the Township's Traffic Signal Contractor or the Township's certified traffic signal technician. The developer's escrow for the project shall then be billed accordingly for work performed by the Township's Traffic Signal Contractor or the Township's certified in-house technician.
- Guarantee repairs or replacements for the balance of the Pre-Dedication Maintenance Period; Provide the Township's Director of Public Works and the Chief of Police with the name and

Section 3-0: Requirements at all Locations within the Township

telephone number of the person to be notified in the event of failures or malfunctions during the guarantee period or a period agreed to by the developer and made part of the Land Development Agreement.

3. FINAL ACCEPTANCE

The following shall signify **FINAL ACCEPTANCE** of the work:

- Satisfactory completion of the Thirty-Day (30) Testing Period and Maintenance Period.
- Satisfactory completion of all outstanding construction items as outlined by the Engineer.
- Submission of all required documentation to the satisfaction of the Township and Township Engineer.

Required Documents

- As-built record plans (hard copy and electronic)
- Warranties
- Operation Manuals
- Certifications
- Test Results

TECHNICAL SPECIFICATIONS

GENERAL

The work supplied for traffic signal construction shall consist of furnishing all new equipment, supplies, labor, transportation, fuel and power, and performing all work as required by the project Contract and pursuant to this ordinance, in strict conformance with the specifications, schedules and drawings and applicable PennDOT specifications and standards. The item of work shall consist of furnishing and installing the traffic control equipment indicated in the contract documents, including controller cabinets; electrical distribution system; vehicular signal heads; standard traffic signal supports or ornamental traffic signal supports, as indicated; loop detectors; junction boxes; signs; sign posts; pavement markings; and all necessary appurtenant work and materials necessary to complete the work indicated.

The contractor must contact the Township for the scheduling of all meetings requiring representatives from the PennDOT.

The contractor must test each ground rod in each controller cabinet foundation and each pole foundation individually for the entire intersection. The contractor must also test the grounding system as a whole.

CONSTRUCTION METHODS

Construction methods shall be in accordance with the applicable sections of PENNDOT Publication 408, PENNDOT Publication 148, PENNDOT Publication 111, and PENNDOT Publication 149.

MATERIALS

All materials supplied shall be approved by the Township Representative. The Township Representative will require that all materials be in accordance with the Pennsylvania Department of Transportation (PENNDOT) Publication 408 Specifications, National Electrical Manufacturer's Association Standards Publication No. TS2, or these specifications.

Upper Uwchlan Township, in order to minimize maintenance costs, has established a standard **Material List** (See Appendix A) for use at all intersections within the Township.

A. TRAFFIC SIGNAL CONTROLLER ASSEMBLY

NEMA TS2 TYPE 2

1. General

This specification sets forth the minimum requirements for a TS2 Type 2 traffic control cabinet assembly. The cabinet assembly shall meet, as a minimum, all applicable sections of the NEMA Standard Publication No. TS2- "Current Edition". Where differences occur, this specification shall govern.

The following components shall be of the same manufacturer: Timer unit, BIU, Controller Back Panel, Detector Rack, and Power Supply as per **Material List**.

All traffic signal controllers will be equipped with the required internal circuitry to facilitate communication through Ethernet.

2. Cabinet Design and Construction

2.1 All Cabinets shall be Type 1 mounting (pad mounting), unless otherwise identified on the plans and specifications.

2.2 The cabinet shall be constructed from type 5052-H32 aluminum with a minimum thickness of 0.125 inches.

2.3 The cabinet shall be designed and manufactured with materials that will allow rigid mounting, whether intended for pole, base or pedestal mounting. The cabinet must not flex on its mount.

2.3.1 A rain channel shall be incorporated into the design of the main door opening to prevent liquids from entering the enclosure. The cabinet door opening must be a minimum of 80 percent of the front surface of the cabinet. A stiffener plate shall be welded across the inside of the main door to prevent flexing.

2.3.2 The top of the cabinet shall incorporate a 1-inch slope toward the rear to prevent rain accumulation.

2.4 Unless otherwise specified, the cabinet shall be supplied with a natural aluminum finish. Sufficient care shall be taken in handling to ensure that scratches are minimized. All surfaces shall be free from weld flash. Welds shall be smooth, neatly formed, free from cracks, blow holes and other irregularities. All sharp edges shall be ground smooth.

2.5 All seams shall be sealed with RTV sealant or equivalent material on the interior of the cabinet.

Section 4-0: Technical Specifications

2.6 All cabinets shall be supplied with one, or more as required, removable shelf manufactured from 5052-H32 aluminum. Shelf shall be a minimum of 10 inches deep.

2.7 The shelf shall have horizontal slots at the rear and vertical slots at the front of the turned downside flange. The shelf shall be installed by first inserting the rear edge of the shelf on the cabinet rear sidewall mounting studs, then lowering the shelf on the front sidewall mounting studs. The shelf shall be held in place by strut nuts.

2.8 The front edge of the upper shelf shall have holes punched every 6 inches to accommodate tie-wrapping of cables/harnesses.

2.9 Two set of vertical "C" or sawtooth channels shall be mounted on each interior wall of the cabinet for the purpose of mounting the cabinet components. The channels shall accommodate spring mounted nuts or studs. All mounting rails shall extend to within 7 inches of the top and bottom of the cabinets. Sidewall rail spacing shall be 7.88 inches center-to-center. Rear wall rail spacing shall be 18.50 inches center to-center.

2.10 The main door and police door-in-door shall close against a weatherproof and dustproof, closed-cell neoprene gasket seal. The gasket material for the main door shall be a minimum of 0.250 inches thick by 1.0 inch wide. The gasket material for the police door shall be a minimum of 0.250 inches thick by 0.500. 2.0 inches wide. The gaskets shall be permanently bonded to the cabinet.

2.11 The lower section of the cabinet shall be equipped with a louvered air entrance. The air inlet shall be large enough to allow sufficient air flow per the rated fan capacity. Louvers must satisfy the NEMA rod entry test for 3R ventilated enclosures. A non-corrosive, vermin- and insect-proof, removable air filter shall be secured to the air entrance. The filter shall fit snugly against the cabinet door wall. The roof of the cabinet shall incorporate an exhaust plenum with two exhaust fans and a vent screen. Perforations in the vent screen shall not exceed 0.125 inches in diameter.

2.12 The main door on a size 3 or larger cabinet shall be equipped with a three-point latching mechanism. with Nylon Rollers at top and bottom.

2.13 The handle on the main door of a size 4 or larger cabinet shall utilize a shank of 5/8 inches minimum. diameter. The handle on the main door of a size 2 or 3 cabinet shall utilize a shank of 7/16 inches. minimum. diameter. The handle shall include a hasp for the attachment of an optional padlock. The cabinet door handle shall rotate counterclockwise to open. The handle shall not extend beyond the perimeter of the main door at any time. The lock assembly shall be positioned so that the handle shall not cause any interference with the key when opening the cabinet door.

Section 4-0: Technical Specifications

- 2.14 The main door hinge shall be a one-piece, continuous piano hinge with a stainless-steel pin running the entire length of the door. The hinge shall be attached in such a manner that no rivets or bolts are exposed.
- 2.15 The main door of a size 5 or larger cabinet shall include a mechanism capable of holding the door open at approximately 90, 125, and 150 degrees under windy conditions. Manual placement of the mechanism shall not be required by the field technician. The main door of a size 2, 3, or 4 cabinet shall include a mechanism capable of holding the door open at approximately 90 and 150 degrees under windy conditions.
- 2.16 The main door shall be equipped with a tumbler lock as per **Material List**. Two keys shall be supplied. An auxiliary Master padlock model M176xdln keyed to the townships proprietary key.
- 2.17 The police door-in-door shall be provided with a treasury type lock as per **Material List**. Two keys shall be supplied.
- 2.18 All base mounted cabinets shall be supplied with anchor bolts to properly secure the cabinet to its base. The cabinet flange for securing the anchor bolts shall not protrude outward from the bottom of the cabinet. When a size 3, 4, or 5 cabinet is base mounted, two anchor bolts shall be provided. Size 6 and 7 cabinets shall be provided with four anchor bolts.
- 2.19 Each cabinet shall be of sufficient size to accommodate all equipment. **The standard size controller cabinet for Upper Uwchlan Township projects shall be a Size 6 (52" H x 44" W x 24" D) and shall have an 18" riser base with shelves for the batteries.** Other cabinet sizes and/or combinations may be used, upon approval by the Township.

3. Terminals and Facilities/Main Panel Design and Construction

- 3.1 The main panel shall be constructed from 5052-H32 brushed aluminum of 0.090 inches minimum thickness and formed so as to minimize any flexing when plug-in components are installed.
- 3.2 All 12- and 16-position main panels shall be hinged at the bottom to allow easy access to all wiring on the rear of the panel. A minimum of 12 position main panels are required in Upper Uwchlan Township.
- 3.3 The main panel shall be fully wired in the following configuration:

Type 3 Configuration - Twelve load switch sockets expandable to sixteen, six flash transfer relay sockets, one flasher socket, two main panel BIU rack slots with an unwired slot for a third and fourth main panel BIU (or wired rack position if required per the plans and specifications), a minimum of one detector rack each

Section 4-0: Technical Specifications

with one BIU, and one MMU. Additional detector racks may be required dependent on the number of detectors at the intersection.

3.4 All load switch and flash transfer relay socket reference designators shall be silk-screen labeled on the front and rear of the main panel to match drawing designations. Socket pins shall be marked for reference on the rear.

3.5 All load switches shall be mounted in a single horizontal row.

3.6 All load switches shall be supported by a bracket or shelf extending at least half the length of the load switch.

3.7 Rack style mounting shall be provided to accommodate the required BIUs per the configuration listed in section 3.3 above. A dual-row, 64-pin female DIN 41612 Type B connector shall be provided for each BIU rack position. Card guides shall be provided for both edges of the BIU. Terminal and facilities BIU mounting shall be an integral part of the main panel. Detector rack BIU mounting shall be an integral part of the detector rack.

3.7.1 All BIU rack connectors shall have pre-wired address pins corresponding to the requirements of the TS2 specification. The address pins shall control the BIU mode of operation. BIUs shall be capable of being interchanged with no additional programming.

3.8 The 12- and 16-load switch position main panels shall have all field wires contained on one row of horizontally mounted terminal blocks. The upper row shall be wired for the pedestrian and overlap field terminations.

3.9 All field output circuits shall be terminated on an unfused compression type terminal block with a minimum rating of 10 amps.

3.10 All field input/output (I/O) terminals shall be identified by permanent alphanumerical labels. All labels shall use standard nomenclature per the NEMA TS2 specification.

3.11 All field flash sequence programming shall be accomplished at the field terminals with the use of a screwdriver only. It shall also be possible to program which flasher circuit the phase shall be programmed to.

3.11.1 Field terminal blocks shall be wired to use three positions per vehicle overlap phase (green, yellow, red). It shall not be necessary to debus field terminal blocks for flash programming.

3.12 The main panel shall contain at least one flasher socket (silk screen labeled) capable of operating a 15-amp, 2-pole, NEMA solid state flasher. The flasher shall be supported by a bracket that extends at least half its length.

3.13 One RC network shall be wired in parallel with each flash-transfer relay and any other relay coils.

3.14 All logic-level, NEMA-controller and Malfunction Management Unit input and output terminations on the main panel shall be permanently labeled. Cabinet prints shall identify the function of each terminal position.

3.15 At a minimum, four 20-position terminal blocks shall be provided at the top of the main panel to provide access to the controller unit's programmable and nonprogrammable I/O. Terminal blocks for DC signal interfacing shall have a number 6-32 x 7/32-inch screw as minimum.

3.16 All wiring, 14 AWG and smaller, shall conform to MIL-W-16878/1, type B/N, 600V, 19-strand tinned copper. The wire shall have a minimum of 0.010 inches thick PVC insulation with clear nylon jacket and rated to 105 degrees Celsius. All 12 AWG and larger wire shall have UL listed THHN/THWN 90 degrees Celsius, 600V, 0.020 inches thick PVC insulation and clear nylon jacketed.

3.17 All controller and Malfunction Management Unit cables shall be of sufficient. Length to allow the units to be placed on either shelf or the outside top of the cabinet in the operating mode. Connecting cables shall be sleeved in a braided nylon mesh. The uses of exposed tie-wraps or interwoven cables are unacceptable.

3.17.1 All cabinet configurations shall be provided with enough RS-485 Port 1 communication cables to allow full capabilities of that cabinet. Each communication cable connector shall be a 15-pin metal shell D subminiature type. The cable shall be a shielded cable suitable for RS-485 communications.

3.18 All main panels shall be pre-wired for a Type-16 Malfunction Management Unit.

3.19 All wiring shall be neat in appearance. All cabinet wiring shall be continuous from its point of origin to its termination point. Butt type connections/splices are not acceptable.

3.20 All connecting cables and wire runs shall be secured by mechanical clamps. Stick on type clamps are not acceptable.

3.21 The grounding system in the cabinet shall be divided into three separate circuits (AC Neutral, Earth Ground, and Logic Ground). These ground circuits shall be connected at a single point as outlined in the NEMA TS2 Standard.

3.22 All pedestrian push-button inputs from the field to the controller shall be optoisolated through the BIU and operate at logic ground.

Section 4-0: Technical Specifications

3.23 All wire (size 16 AWG or smaller) at solder joints shall be hooked or looped around the eyelet or terminal block post prior to soldering to ensure circuit integrity. Lap joint soldering is not acceptable.

4. Power Panel Design and Construction

4.1 The power panel shall consist of a separate, wholly enclosed module, securely fastened to the right side wall of the cabinet. The power panel shall be wired to provide the necessary power to the cabinet, controller, Malfunction Management Unit, cabinet power supply and auxiliary equipment. It shall be manufactured from 0.090-inch, 5052-H32 aluminum with a removable plastic front cover. The panel shall be of such design so as to allow a technician to access the main and auxiliary breakers without removing the front cover.

4.2 The power panel shall house the following components:

- a. A 30-amp main breaker shall be used for 12, 8, or 4-position cabinets. This breaker shall supply power to the controller, MMU/CMU, signals, cabinet power supply and auxiliary panels. Breakers shall be thermal magnetic type, U.L. listed for HACR service, with a minimum of 10,000 amp interrupting capacity.
- b. A 15-amp auxiliary breaker. This breaker shall supply power to the fans, light, GFI outlet, and auxiliary outlet.
- c. A surge arrester as per **Material List**.
- d. A 50-amp, 125 VAC radio interference line filter.
- e. A normally open, 60-amp, SSR or non-mercury contactor as per **Material List**
- f. Two (2) 15-position neutral bus bars capable of connecting three #12 wires per position shall be provided.
- g. A 7-position ground bus bar capable of connecting three #12 wires per position.
- h. A NEMA type 5-15R GFCI convenience outlet.

5. Auxiliary Cabinet Equipment

5.1 The cabinet shall be provided with two thermostatically controlled (adjustable between 80-150 degrees Fahrenheit) ventilation fan in the top of the cabinet plenum. The fan shall be a ball bearing type fan and shall be capable of drawing a minimum of 100 cubic feet of air per minute.

5.2 An LED lighting fixture shall be mounted on the inside top of the cabinet near the front edge. The lamp shall be wired to a 15-amp door-activated switch mounted near the top of the door.

5.3 An enclosed drawer, of sufficient size to accommodate one complete set of cabinet prints, shall be provided and mounted on the left side of the cabinet below the detector shelf.

5.4 Three sets of complete and accurate cabinet drawings shall be supplied with each cabinet.

5.5 One set of manuals for the controller, Malfunction Management Unit and vehicle detector amplifiers shall be supplied with each cabinet. Supply the municipality with electronic copies.

5.6 The cabinet shall be furnished with an in-door switch to advise master/local controller of a “door-open” condition.

5.7 The cabinet shall be provided with a surge protection package. Surge protection to be solid state design, continuous service current—15A at 120V RMS, rated at 20,000A (8 x 20 microseconds) 20 times, peak clamping voltage: 250V at 20kA. Provide terminals for AC line, AC neutral, AC equipment in, AC equipment out, neutral equipment out and ground.

5.8 A minimum five-position auxiliary outlet strip shall be installed on the right side of the cabinet.

6. Cabinet Test Switches and Police Panel

6.1 A test switch panel shall be mounted on the inside of the main door. The test switch panel shall provide as a minimum the following:

- a. AUTO/FLASH SWITCH. When in the flash position, power shall be maintained to the controller and the intersection shall be placed in flash. The controller shall not be stop timed when in flash. If required by the plans and specifications, an optional RC network shall be provided to give the controller an external start pulse when switch is returned to the auto position. This will force the controller to initiate the start-up sequence when exiting flash.
- b. STOP TIME SWITCH. When applied, the controller shall be stop timed in the current interval.
- c. CONTROL EQUIPMENT POWER ON/OFF. This switch shall control the controller, MMU, and cabinet power supply AC power.

6.2 The police door switch panel shall contain the following:

- a. SIGNALS ON/OFF SWITCH. In the OFF position, power shall be removed from signal heads in the intersection. The controller shall continue to

operate. When in the OFF position, the MMU shall not conflict or require reset.

- b. **AUTO/FLASH SWITCH.** In the flash position, power shall not be removed from the controller and stop time shall be applied. If required by the plans and specifications, an optional RC network shall be provided to give the controller an external start pulse when switch is returned to the auto position. This will force the controller to initiate the start-up sequence when exiting flash.
- c. **AUTO/MANUAL SWITCH.** In the MANUAL position, the intersection shall be controlled by the provided momentary push button and hand cord. The cord and push button shall be stored within the closed police door when not in use.

6.3 All toggle type switches shall be heavy duty and rated 15 amps minimum. Single- or double-pole switches may be provided, as required.

6.4 Any exposed terminals or switch solder points shall be covered with a non-flexible shield to prevent accidental contact.

6.5 All switch functions must be permanently and clearly labeled.

6.6 All wire routed to the police door-in-door and test switch pushbutton panel shall be adequately protected against damage from repetitive opening and closing of the main door.

6.7 All test switch panel wiring shall be connected to the main panel via a 16-pin AMP type connector.

7. Auxiliary Devices

7.1 Load Switches

7.1.1 Load switches shall be solid state and shall conform to the requirements of Section 6.2 of the NEMA TS2 Standard.

7.1.2 Signal load switches shall have a minimum rating of 10 amperes at 120 VAC for a LED lamp load.

7.1.3 The front of the load switch shall be provided with six indicators to show the input and output signals from the controller and load switch.

7.1.4 Load switches shall be dedicated per phase. The yellow of a ped movement may be utilized for flashing yellow arrows. Otherwise, the use of load switches for other partial phases is not acceptable.

7.2 Flashers

7.2.1 The flasher shall be solid state and shall conform to the requirements of section 6.3 of the NEMA TS2 Standard.

7.2.2 Flashing of field circuits for the purpose of intersection flash shall be accomplished by a separate flasher.

7.2.3 The flasher shall be rated at 15 amperes, double pole with a nominal flash rate of 60 FPM.

7.3 Flash Transfer Relays

7.3.1 All flash transfer relays shall meet the requirements of Section 6.4 of the NEMA TS2 Standard.

7.3.2 The coil of the flash transfer relay must be de-energized for flash operation.

7.3.3 The full complement of relays, a minimum of eight, shall be supplied with each cabinet to allow for maximum phase utilization for which the cabinet is designed.

7.4 Malfunction Management Units

7.4.1 Each cabinet assembly shall be supplied with one Malfunction Management Unit (MMU) or one Conflict Monitor Unit (CMU), as per **Material List**, and as defined by the requirements of Section 4 of the NEMA TS2 Standard.

7.4.2 Where two or more cabinet assemblies are supplied on a project, one spare MMU shall be supplied to the Township.

7.5 Bus Interface Units

7.5.1 All Bus Interface Units (BIU) shall meet the requirements of Section 8 of the NEMA TS2 Standard.

7.5.2 The full complement of Bus Interface Units as per **Material List**, a minimum of three (3), shall be supplied with each cabinet to allow for maximum phase and function utilization for which the cabinet is designed.

7.5.3 Each Bus Interface Unit shall include power on, transmit and valid data indicators. All indicators shall be LED.

7.5.4 Two spare BIUs are to be provided to the Township with each cabinet assembly provided.

8. Testing and Warranty

8.1 Testing

8.1.1 Each controller and cabinet assembly shall be tested as a complete entity under signal load for a minimum of 48 hours. The signal load and detector loads shall simulate actual intersection conditions.

8.1.2 Each assembly shall be delivered with a signed document detailing the cabinet final tests results as results as performed.

8.1.3 The cabinet shall be assembled and tested by the controller manufacturer or authorized local distributor to ensure proper component integration and operation.

8.1.4 The Township shall be notified, in writing, at least 48 hours in advance of the bench testing of the controller. At the discretion of the Township, no controller shall be released until an inspection of the controller assembly at the bench test site has been completed by the Township.

8.2 Warranty

8.2.1 The controller and Malfunction Management Unit shall be warranted by the manufacturer against mechanical and electrical defects for a period of 5 years. The manufacturer's warranty shall be supplied in writing with each cabinet and controller. Second party extended warranties are not acceptable. This warranty shall be provided by the manufacturer/vendor/contractor with no regard to selection of other traffic signal components.

8.2.2 The cabinet assembly and all other components shall be warranted for a period of one year starting at the final acceptance of the project.

8.2.3 Any defects shall be corrected by the manufacturer or supplier at no cost to the Township.

9. Spare Equipment

9.1 Supply spare(s) of the following equipment per cabinet:

9.1.1 Four load switches

9.1.2 Two relays

9.1.3 Two Bus Interface Units (BIUs)

9.1.4 One Malfunction Management Unit (MMU) per two (2) cabinets on a project. If less than two cabinets are supplied on a project the spare MMU is not required.

B. TRAFFIC SIGNAL SUPPORTS

1. Standard Traffic Signal Supports

- 1.1 Where directed, all sites specifying the use of “Standard Traffic Signal Supports”, shall meet the requirements set forth in PennDOT Publication 408, specifically Sections 950 and 951.
- 1.2 Manufacturer’s requirements shall follow the guidelines found in PennDOT Publication 408, as well as manufacturer’s requirements for the loadings. Compliance with bolt circles and bases are as set forth in PennDOT Publication 148, Traffic Standards - Signals (TC-8800 series).
- 1.3 All standard traffic signal supports shall be equipped with an overbuild for future highway lighting unless otherwise directed by the Township. When a luminaire is to be provided, it shall be approved LED, Type 3 Cobra Head full cut-off type as per **Material List**. The supplied davit arm shall match the finish of the mast arm, with a standard mounting height of 30’.
- 1.4 *Vertical support poles and mast arms to be designed by manufacturer to adequately support loads as shown on the contract drawings with a luminaire arm or maximum load requirements established by AASHTO specifications for structural supports for highway signs, luminaires and traffic signals, whichever is greater. All poles shall have powder coating in accordance with AAMA rating 2605 and PennDOT Pub. 408 Section 951.2©10 specification with Federal Black color.*

C. VEHICULAR DETECTION

1. Stop Bar Detection

- 1.1 At locations identified by the Township, the use of stop bar detection as per **Material List** will be required.
- 1.2 Provide a detection system which provides detection as shown on the approved permit plan and a minimum of four (4) additional detection zones.

2. Advanced Detection

2.1 At locations identified by the Township, the use of advance detection as per **Material List** will be required.

3. Alternate Detection System

3.1 At locations approved by the Township, the use of alternate detection systems may be permitted with prior approval.

D. PEDESTRIAN DETECTION

1. Pushbuttons

1.1. Contractor shall provide and install pedestrian pushbuttons in accordance with PennDOT Publications 408, 13M, 148, 149 and as per **Material List**.

1.2. Field-test the operation by activating the pushbutton and performing a visual check of the controller indicator lights and a timing check of the pedestrian phase or phase extension.

E. PAVEMENT MARKINGS

All pavement markings shown on the plans shall be either thermoplastic or epoxy pavement markings as per **Material List**. Thermoplastic markings shall not be installed on concrete pavement. Furnish and install pavement markings as shown on the PennDOT approved permit plan. Any pavement marking removal will be considered incidental to the application of the new markings and should be completed in accordance with section 963 of PennDOT 408.

The contractor will be responsible to maintain all pavement markings installed under a contract for a period of 180 days from the conclusion of the 30-day test period. If during this period, the Township Traffic Engineer or PennDOT determines the pavement markings are in need of replacement, it will be the responsibility of the prime contractor to complete this work at no cost to the Township.

F. SIGNAL HEAD ASSEMBLIES WITH LED INDICATIONS

Conforming to PennDOT Sections 955, one way, 8" or 12" adjustable, sectional, LED, complete with all brackets and rigid mounting attachments.

Section 4-0: Technical Specifications

1. All signal wiring must terminate on terminal strips. Splices are only permitted in the traffic signal support hand hole and must be made with silicone-filled wire nuts as per **Material List**. Complete encapsulation of the individual conductors is required.
2. All signals shall have a drip loop attached with UV stabilized nylon wire ties.
3. All signal brackets shall be attached with ¾-inch stainless steel banding. Painted to match the signal support where applicable. All rigid mounted signal heads shall have a cable clamp.
4. Back plates shall be black, one-piece, aluminum, and equipped with minimum 2-inch fluorescent yellow, Type IX retroreflective border. All hardware shall be stainless steel. Back plates shall be designed to properly fit the signal manufacturer's heads that will be furnished.
5. Provide vehicular and pedestrian signal heads as per **Material List**.

G. FIBER OPTIC TRAFFIC CONTROL INTERCONNECT CABLE

1. Fiber Optic Cable

- 1.1 Fiber optic cable to be maintained by Upper Uwchlan Township must be installed in a dedicated conduit system. Fiber optic cable installation should be coordinated with PennDOT for connection to the Regional Fiber Optic Network and Traffic Management Center. Coordinate with PennDOT regarding standards for fiber optic cable to maintained by PennDOT.
- 1.2 The Fiber Optic cable shall be single-mode tight buffered to provide storage and operating stability over the temperature range of -40C to +70C. The cable shall consist of a minimum of 12 optical fibers stranded around an all-dielectric central strength member. Each strand will be color coded with 12 different colors.
- 1.3 Each fiber shall be single-mode and have a core/clad diameter of 9/125 micrometer. The fiber shall have an attenuation of not more than 1.0 dB/km at 1310 NM, and a bandwidth of 100 MHZ/km. The fiber and cladding shall be jacketed with a PVC buffer tube. A second jacket shall envelope the fibers and dielectric strength member. This second jacket shall be wrapped with Kevlar/aramid reinforcement and the completed cable shall be encased in a polyethylene outer jacket.
- 1.4 The cable shall have a minimum diameter of .36 inches and a nominal weight of 50 pounds per 1000 feet. The cable shall withstand tensile loads of 600 pounds during

installation and 80 pounds long term. The cable shall accommodate a seven-inch (7") radius of bending during installation and a six-inch (6") radius long term. Cable for underground installation in conduit need not be armored. Cable for overhead installations will be figure 8 type with a 1/4" minimum galvanized steel messenger. The messenger shall be sized in accordance with the manufacturer's recommendations. The messenger will be terminated at the last pole, and under no circumstances will it enter the control cabinet or conduit system.

1.5 Fiber Wire Identification Tags: Tags shall be manufactured/supplied by the fiber optic cable manufacturer. The tag shall read "Upper Uwchlan Township Fiber Optic Cable". Provide button stops and two (2) 1-inch diameter (T-1) E-2 ties, one to loop through the tag and one to attach to the fiber wire.

1.6 Within the cabinet, the cable shall be broken out a minimum of three feet (3') or as required to allow easy attachment to a rack mounted fiber tray. Provide a minimum of twenty feet (20') of slack in the cabinet. The fiber tray shall be sized to facilitate all fibers within the cabinet. The ends of the fiber shall be terminated using type ST and LC connectors. These connectors shall have a metal body with either a stainless steel or a ceramic ferrule. Installation of the connector and attached to the bulkhead in the patch panel. Dust caps shall be used to protect the ends of all spare fibers. Connection between the Data Link and the patch panel will be made using manufactured ST type duplex patch cords. Patch cords shall be of sufficient length for proper connection and routing within the cabinet.

1.7 Test each fiber optic strand in the presence of the Township or their designated representative to determine dB loss of each fiber optic cable. Test each strand by a certified fiber optic testing technician utilizing standard light measuring instruments. If dB loss is found to be unacceptable by the Township or their designated representative, then OTDR testing must be conducted. Test each fiber optic strand using a certified fiber optic testing technician utilizing standard OTDR instruments. Replace fiber optic cable run if OTDR testing reveals unsatisfactory dB loss, as per manufacturer's recommendation, for the individual run, based on length of run.

2. Fiber Optic Patch Panel

2.1 Furnish and install a fiber optic break-out box as necessary with a capacity for full splice storage and connection functions for a minimum of 24 fiber optic connections and meeting the following requirements:

2.2 Provide on the outside surface of the breakout box a minimum of 24 ST or LC-type female optical connectors in order to plug in jumper cables that connect to the

equipment. Provide twist-lock design for connectors. Provide dust caps for all connectors.

- 2.3 Connector insertion loss: +/- 0.25dB
- 2.4 Provide a removable top panel to protect the splices and pigtails in the breakout box.
- 2.5 Cabinets shall be supplied with a 19" EIA rack mount tray with a minimum of 24 positions.

H. FIBER OPTIC DATA LINK

1. Ethernet Switch

- 1.1. Provide a minimum 10-port Ethernet Managed switch with built-in fiber connection as per **Material List**.
- 1.2. Provide SFPs as needed as per **Material List**.
- 1.3. The switch shall have a lifetime manufacturer's warranty.

I. EMERGENCY PREEMPTION SYSTEM

1. Emergency Preemption

Furnish and install a complete emergency preemption system as per **Material List** that operates in accordance with the PennDOT approved traffic signal permit, and as follows:

- 1.1 Emergency Preemption equipment shall include detectors, verification/LED confirmation beacons, preemption card rack, preemption cards, and wiring as required for proper operation of the system. Provide front panel connector for an auxiliary detector input.
- 1.2 The receiver shall detect approaching emergency vehicles for a minimum range of 2,500 feet. Additional detectors shall be required if minimum required distance is not met.

1.3 The preemption system must be field tested in the presence of the Township. Following testing, the detector head may need to be relocated and/or adjusted in the field to provide acceptable operation as deemed appropriate by the Township and PennDOT.

1.4 All components of the preemption system shall have full 5-year warranties against defects in material and workmanship, except for the emitter lamp, which shall have a 1-year warranty.

J. UNINTERRUPTIBLE POWER SUPPLY

1. Provide a battery back-up system as per **Material List** in the controller cabinet.
2. Provide sufficient field-hardened AGM batteries or super capacitor modules to run the intersection in normal operation for a minimum period of ten hours from the time of AC power loss, minimum four (4) 100AH AGM batteries or Super Capacitors Modules. Provide a recharge rate of 24 hours maximum to reach full charge from a complete discharge.
3. Provide a 1.00-inch green LED indication light on the cabinet exterior to indicate AC power is present from utility company or generator. Install the light at a point visible from a vehicle traveling on the main street of the intersection. Provide watertight seal at LED installation point. Top of cabinet mounting is not permitted.
4. Provide a 1.00-inch red LED indication light on the cabinet exterior to indicate the battery back up in use, and loss of AC power. Install the light at a point visible from a vehicle traveling on the main street of the intersection. Provide watertight seal at LED installation point. Top of cabinet mounting is not permitted.

K. TRAFFIC CONTROL SIGNS

Contractor shall provide and install traffic control signs as shown on the plans and in accordance with PennDOT Publication 408, sections 930 through 936 and as follows:

1. Contractor shall install all mast arm mounted signs with brackets as per **Material List** with a minimum of two (2) Z-Type brackets on pipes that allow adjustment of the signs or on extruded aluminum stiffeners. All signs using stiffeners shall be installed in accordance with Penn DOT Publication 111, TC-8800 Series.

Section 4-0: Technical Specifications

2. All banding for structure-mounted signs shall be ¾-inch stainless steel banding. On painted poles provide epoxy paint coated stainless steel banding. Pedestrian pushbutton signs may use ½-inch stainless steel banding.
3. All post-mounted signs shall be installed on breakaway square signposts. Two posts shall be provided for any signs over 48" wide.
4. Overhead Street Name Signs shall be white legend on green background and shall conform to PennDOT Publication 236 Standards for Series D3-4 and/or D3-5 signs. All signs shall have the township logo and be approved by the Township.

L. WIRING, CONDUIT, AND JUNCTION BOXES

1. At all locations within the Township, all traffic signal wiring shall conform to PennDOT Publication 408 and IMSA 20-1 for cable in conduit systems. IMSA 19-1 shall be used for overhead span wire installations. All cable splices in base of traffic signal poles shall be connected by a waterproof type electrical connector. King Connectors or approved equal shall be used. Install with the head pointed down so sealant does not escape.
2. Rigid Polyvinyl Chloride Conduit for direct burial shall be Schedule 40; 90C with a UL651 Listing for rigid nonmetallic conduit; UL-514 Listing for fittings. Exposed conduit shall be galvanized steel rigid metal conduit (RMC) or RPCC, Schedule 80. UL Listed.
3. Conduit for fiber optic cable, as noted on the plans, shall be 2-inch diameter. A suitable fiber optic locator tape shall be placed between 12 –18" above the top of the conduit or a single 14AWG copper cable installed within the conduit system. All fiber optic conduits shall be installed at a minimum depth of 36" below finished grade.
4. Conduit installed across any roadway shall be a minimum 3-inch diameter. A minimum of two 3-inch conduits and one spare 3-inch conduit shall be installed in the controller cabinet foundation to allow for future expandability. One-inch conduit can only be used for a ground rod. Where conduit is only occupied by fiber optic cable, supply a 14-gauge copper trace wire for the entire conduit run.
5. Install a spare 3-inch conduit from all pole bases to the adjacent junction box. A 2-inch conduit would be acceptable with written approval from the Township.
6. All conduits entering into a junction box must end with a ninety-degree sweep and be high enough to prevent drainage stone from entering conduit. Any change requires written approval from the Township.

7. Provide junction boxes as per **Material List**. Junction box lids shall be labeled “Traffic Signal” or “Fiber Optic Cable” as appropriate and as directed by the Township. All junction boxes must conform to all test provisions of the latest version of the ANSI/SCTE 77 “Specification for Underground Enclosure Integrity” for Tier 15 applications, as a minimum. In areas of deliberate and non-deliberate vehicular traffic and as determined by the Township, junction boxes meeting greater load requirements will be provided.

M. GENERATOR/UTILITY TRANSFER ASSEMBLY

Generator cabinet to be equipped with plugs and cords as per **Material List**.

1. An assembly shall be provided that will allow an automatic transfer between Utility and local fed electrical power in a manner that is both safe and simple to execute. No tools shall be used to affect the transfer. The assembly shall be installed inside of a NEMA 3R cabinet measuring 14”x10”x7” and will be keyed to accommodate a standard traffic cabinet “police door” key.
2. The transfer action shall involve the de-energizing of a 10-amp relay to provide a seamless transition from utility to generator power. When the utility service is active the relay shall energize to supply a seamless transition to utility power. The generator shall be grounded to the controller assembly by a three prong NEMA L5-30 plug.
3. All elements of the assembly shall be designed to carry single phase 120vac 60 Hz. electrical current at 30 amperes. The transfer assembly shall be so constructed to physically prevent either power source from back feeding the other. Only one power source may be connected to the load at any one time. All electrical connections shall be covered or recessed within insulating materials. Instructions & schematic detailing the transfer procedure and connections shall be attached to the assembly.
4. The Utility power shall enter the transfer assembly from a fused or breaker disconnect box. The generator power shall enter the transfer assembly from a compatible three-conductor industry standard inlet (NEMA L5-30P). A breaker, of amp city not to exceed the inlet’s rated current, shall be connected between the inlets “hot” leg and the transfer assembly. A compatible surge protection device shall be connected between the load side of this breaker and an Earth connection. The common leg of the inlet shall be connected directly to the load’s Neutral bus. The ground leg of the inlet shall be connected to an Earth connection.

APPENDIX A

MATERIAL LIST

Upper Uwchlan Township

Technical Specifications for Traffic Control Signalization

MATERIALS LIST

- Traffic Signal Timer Unit: Econolite EOS, Cobalt ATC Series as a minimum
- Power Supply: Same manufacturer as the controller unit.
- Cabinet Door, Main Lock: Corbin tumbler lock # 1548-1
An auxiliary padlock
- Cabinet Door, Police Lock: Corbin # R357SGS or exact equivalent
Auxiliary keyed padlock: Master#M176XDLN
- Power Panel Surge Arrester: HESCO RSL Model HE1750
- Power Panel Mercury Contactor: As provided by the cabinet supplier.
- Malfunction Management Unit (MMU): Reno MMU2-1600GE or EDI
- Conflict Monitor Unit (CMU): EDI SSM-12LE
- Bus Interface Units (BIU): Same manufacturer as the controller unit.
- Luminaire: LED Cobra Head, Watt TBD
- Stop Bar Vehicle Detection: Wavetronix Matrix Radar Detection; No Traffic, Video/Radar, Type 2 Detection; Econolite Autoscope Vision
- Advance Detection Wavetronix Smart Sensor Advance Delema Zone.
No Traffic, Video/Radar, Type 2 Detection
- Pedestrian Pushbuttons Latching Polara BDL3-X or latest model
Latching Polara iNS/iDS or latest model (as needed)
- Durable Pavement Markings: Alkyd Thermoplastic or Epoxy Resin
- Cable Splice Connector: King Connectors or approved equal re-enterable silicon-based connector
- Traffic Signal Heads: Polycarbonate housing with PennDOT approved LED lenses

- Pedestrian Signal Heads: Polycarbonate housing and man/hand symbols and countdown timer with PennDOT approved LED lenses

Upper Uwchlan Township

Technical Specifications for Traffic Control Signalization

MATERIALS LIST

- Managed Ethernet Switch: CommNet Model CNGE8MS or approved equal
- Ethernet SFP Module: CommNet SFP-6 Single Mode
- Emergency Preemption: GTT 762 or 764 modules; Miovision Opticom Preemption or approved equal
Alpha Technologies with Alpha Remote Battery Monitoring System (RBMS)
- Uninterruptible Power Supply System: TechPower Development, Model DBLMXU-36 Series, w/ECO Green Energy Mode or equal.
- Mast Arm Sign Brackets: Pelco Astro Bracket; Northeast American Bracket.
- Signal Head Brackets: Sky Bracket Cable Clamp for rigid mount signal heads; Pelco Astro Bracket; Northeast American.
- Junction Boxes: Quazite Brand or approved equal
- Generator Cabinet: 30A 120vac twist lock male plug and cord with a 30A female twist lock and 15A male straight blade.

** A Garmin GPS, or approved equal, must be supplied with each cabinet unless specifically excluded.*

*** All equipment shall meet and/or exceed all PennDOT and MUTCD requirements. Alternative equipment may be submitted to the Township for approval. Upper Uwchlan Township shall have the final determination as to the manufacturer or equipment.*



UPPER UWCHLAN TOWNSHIP

MEMORANDUM

ADMINISTRATION

TO: Board of Supervisors

FROM: Jill Bukata
Township Treasurer

RE: 2026 Pension Plan MMO

DATE: August 12, 2025

The Township has received the figures for the 2026 Minimum Municipal Obligation (MMO) from Thomas J. Anderson & Associates for all three township pension plans – the two defined benefit plans (Uniform and Non-Uniform) and the Non-Uniform Defined Contribution Plan.

2026 Township Minimum Municipal Obligation

	Uniform MMO	Non-Uniform MMO (DB)	Non-Uniform MMO (DC)
2026 MMO	\$297,814	\$79,812	\$60,000
2025 MMO	\$415,855	\$100,641	\$54,000
Increase (Decrease)	(\$121,041)	(\$20,829)	\$6,000

Excellent market values during 2023 and 2024 were the main contributors to the decrease in the MMO cost from 2025 to 2026. Total payroll remained relatively constant. The addition of new officers in the Police Department were offset by two officers that entered the DROP program. The total decrease in cost for 2026 is \$135,870.

I respectfully recommend that the Board approve the 2026 MMO's for the three plans as follows:

Uniform Plan	\$297,814
Non-Uniform (DB)	\$ 79,812
Non-Uniform (DC)	\$ 60,000