



UPPER UWCHLAN TOWNSHIP
BOARD OF SUPERVISORS WORKSHOP

August 13, 2024
4:00 p.m.

AGENDA

LOCATION: Township Building, 140 Pottstown Pike, Chester Springs PA 19425

- I. Call to Order
 - A. Salute to the Flag
 - B. Moment of Silence
 - C. Inquire if any Attendee plans to audio or video record the Workshop
 - D. An Executive Session was held July 15, 2024 regarding a legal matter for a Conditional Use Decision and regarding personnel – Boards/Commissions candidates.
- II. Park & Recreation Board ~ Junior Member Appointment
- III. Village Concept Plan ~ Updated

Introduction of the Village Concept Plan draft dated June 19, 2024, for the Board of Supervisors' review, as recommended by the Planning Commission at their July 11, 2024 meeting.
- IV. 241 Park Road - Planebrook Partners - Ducklings Daycare
Preliminary, Final Land Development Plan ~ Consider Approval
- V. Open Session
- VI. Adjournment



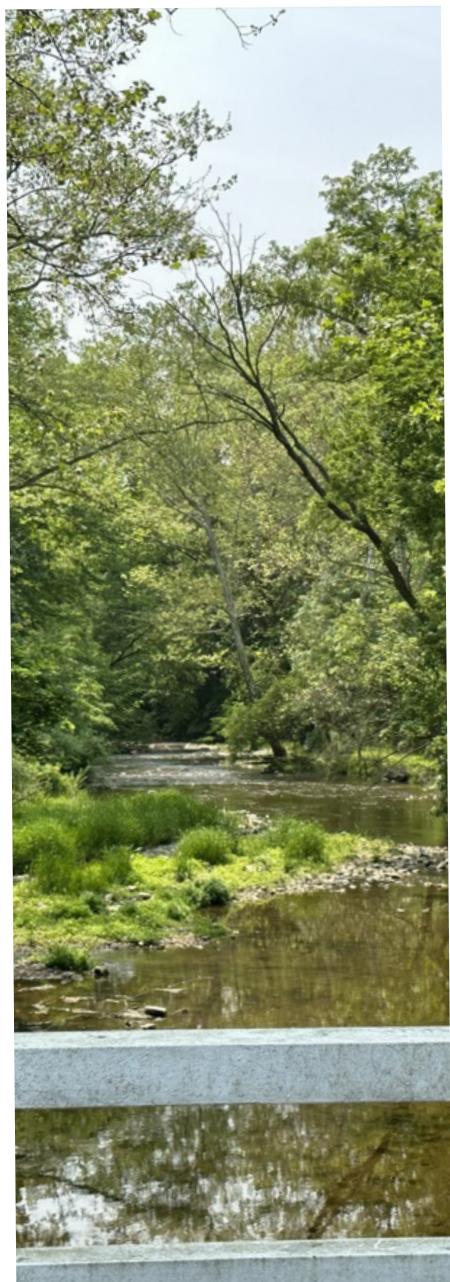
UPPER UWCHLAN TOWNSHIP

TO: Board of Supervisors
CC: Gwen Jonik
FROM: Lindsay Yeager, Assistant Township Manager
RE: Park & Recreation Junior Member
DATE: July 31, 2024

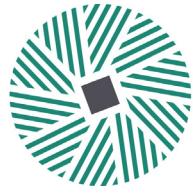
On March 21st, two members of the Park & Recreation Board interviewed one interested candidate for the position of Junior P&R member. The interviewee, Rishav Vignesh, did a great job. The Park & Recreation Board would like to recommend Rishav to the Board of Supervisors to be considered for the Junior Park & Recreation Board member position.

Upper Uwchlan Township

Village Concept Plan 2024



Prepared by



**BRANDYWINE
CONSERVANCY**



**RICHARD GRUBB
& ASSOCIATES**

Upper Uwchlan Township

Village Concept Plan

Update 2024

Upper Uwchlan Township
140 Pottstown Pike, Chester Springs, PA 19425

Township Staff

Tony Scheivert, Township Manager
Gwen Jonik, Township Secretary

Board of Supervisors

Jenn Baxter – Chairperson
Andy Durkin – Vice Chairperson
Sandy D'Amico – Supervisor

Planning Commission

Sally Winterton – Chair
Joe Stoyack – Vice Chair
Chad Adams – Member
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Adopted Month Day, 2024

Concept Plan Task Force

Sandy D'Amico, Township Supervisor
Jim Greaney, Zoning Hearing Board
Vivian McCardell, Historical Commission
Byron Nickerson, Emergency Management
Neil Phillips, Environmental Advisory Council
Gerry Stein, Historical Commission
Joe Stoyack, Planning Commission
Sushila Subramanian, Park and Recreation
Sally Winterton, Planning Commission

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Introduction

The Village Concept Plan is a narrative and guidance document that describes the features envisioned by the Upper Uwchlan Township boards and commissions when new development, redevelopment and infill occurs within the Historic Eagle Village, Byers Station Village, and the identified surrounding landscape. This Plan is not intended to prescribe specific development of properties. It intends to promote the creation of economically and socially integrated, coherent, functional, and vibrant places while preserving historic resources and the integrity of historic and cultural areas, maintaining historical aesthetics, encouraging open space preservation, and providing a wider range of recreational opportunities. This Plan also serves as a steppingstone in developing recommendations for fulfilling the Village Concept Plan vision: maintaining the identity of Upper Uwchlan Township as a community of rural character, especially within the historic crossroads that is the significant social and cultural center of the Township today. That can be accomplished by analyzing the existing features, identifying challenges, recognizing opportunities, and formulating implementation strategies for the future.

According to the Merriam-Webster dictionary,

“village” has several meanings, including a settlement usually larger than a hamlet and smaller than a town or an incorporated minor municipality. The term generally describes a small assemblage of houses in the countryside or a predominantly residential district within more densely developed areas. The “Village” has been a well-recognized land use pattern in Chester County for as long as people settled and inhabited the region. Originally developed to be vital for central places with services and amenities to support rural communities, the village became a part of residential settings with a unique, distinctive characteristic that continues to play a role in our heritage. Villages provide residents and visitors with opportunities for social interaction, education, employment, cultural events and activities, access to transportation and other functions that were also essential to the rural residents of the past. Chester County has long recognized the importance of preserving such heritage and developed the Rural Center Design Guide in December 2022 to provide information on planning a community for purpose with a sense of unity around its diverse characteristics. Modern demands on services, population growth and the attractiveness of historically significant places propel municipalities to devise creative ways to preserve these places of local, regional and/or national historic significance while accommodating the needs of their residents and planning for future growth.

Upper Uwchlan Township has identified the future of the Historic Eagle Village, Byers Station Village, and the surrounding landscape as a high planning priority. In 2010, the Committee was appointed to develop the Concept Plan and the conceptual sketch plan specific to the Village of Eagle/Byers Station (2010). The completed Concept Plan was incorporated within the 2014 Comprehensive Plan Update. The 2010 Plan's purpose was to develop a framework to:

1. Protect and enhance the visual and compositional integrity of the Village of Eagle and Byers (Station Historic District), including other historical assets that lie outside.
2. Determine any additional infrastructure required due to the construction of Graphite Mine Road.
3. Develop access management standards.
4. Recommend architectural standards for new construction and redevelopment in the overlay district and village.
5. Assess existing zoning districts, and evaluate permitted uses based on zoning to determine if adopting additional ordinances supports the goals and vision of the Township.

As a result, several recommendations of that Concept Plan were implemented, including:

1. Successful implementation of a mixed-use development within the Village of Eagle.
2. Adaptive reuse of historic resources throughout the Township.
3. Implementation of an Access Management Ordinance to provide vehicular access to developments in a manner that preserves the safety and efficiency of the transportation system.

4. Development of Village Design Guidelines to encourage a historically sensitive and consistent design within the Village areas.

Since the development of the original Concept Plan, additional challenges have been identified, including the need for developing amendments to existing municipal ordinances to implement the Plan's Village Design Guidelines, the unwillingness of applicants and/or developers to follow the design guidelines due to their branding or other circumstances and a lack of incentives to promote the use of Village Design Guidelines to property owners and developers.

In 2022, the new Village Concept Plan Committee convened to review and revise the 2010 Concept Plan to reflect any changes within the Township and build upon the previously completed efforts to preserve the character of its historic villages and surrounding landscapes.

This Village Concept Plan update is a foundation for updating the Village Design Guidelines developed in 2010. The guidelines are a separate companion document that provides detailed design guidance for all construction, development, and redevelopment in the identified area. The Village Design Guidelines are intended to be used by Township officials, boards and commissions, landowners, developers, and others as an instruction document to balance growth with the preservation of historic resources, maintain a village atmosphere, and to enhance existing economic, social, and cultural assets.

The Plan includes a glossary, mission statement and goals, background information, and Village Concept guiding principles. It also analyzes existing conditions (i.e., land use, zoning regulations, pedestrian and bicycle amenities, access management and circulation), defines visions for distinctive sub-areas and provides recommendations for fulfilling the goals and visions identified in the Plan.

Glossary

For the purposes of this Plan, the following terms are used as follows:

Adaptive Reuse: The process of repurposing existing structures, including historic buildings and structures, for new purposes, consistent with the Township zoning, SLDO and Village Design Guidelines.

Byers Station Historic District: a collection of historic resources centered around the Byers railroad station, with several historic resources nationally designated.

Civic Spaces: Publicly accessible open spaces in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned.

Class I Historic Resource: A resource that is either listed in the National register of Historic Places or has received a Determination of Eligibility from the Pennsylvania Historical and Museum Commission (PHMC)

Class II Historic Resource: A resource that is deemed historically or architecturally significant at the local level.

Clean Energy: Energy that comes from renewable, zero-emission sources that do not pollute the atmosphere when used, as well as energy saved by energy efficiency measures.

Committee: The selected Village Concept Plan Committee responsible for developing this Plan Update.

Compatible Land Uses: Single or combination of uses of land adjacent to a different type of land use that does not create concerns of public safety, health and welfare or aesthetic consistency between sub area [see definitions] for community.

Climate Resilience: Capacity of a system to

maintain function in the face of stresses imposed by climate change and to adapt to be better prepared for future climate impacts.

Concept Plan: A written guidance document that outlines the vision for future development of the specific area and proposes implementation strategies to fulfill that vision. The Concept Plan usually addresses future land use, transportation, and zoning, illustrating improvements including but not limited to historic preservation, recreation, and redevelopment.

Development: Any man-made change to improved or unimproved real estate, including but not limited to the construction, reconstruction, renovation, repair, expansion, or alteration of buildings or other structures; the placement of manufactured homes; streets, and other paving; utilities, filling, grading and excavation; mining; dredging; drilling operations; storage of equipment or materials; and the subdivision of land.

Eagle Village Historic Area: An area designated by the Township that includes a concentration of historic resources within the village of Eagle adjacent to Pottstown Pike (PA Route 100) between Park Road and Byers Road.

Green Stormwater Infrastructure: Range of measures that use plant or soil systems, permeable pavement or other permeable surfaces or substrates, stormwater harvest and reuse, or landscaping to store, infiltrate, or evapotranspiration stormwater and reduce flows to sewer systems or to surface waters.

Historic District: One or more structures, properties or sites making up a historic district that is listed in or determined eligible for the National Register of Historic Places (a list maintained by the U.S. Department of the Interior, or in the case of a determination of eligibility of the Department or the Pennsylvania Historical and Museum Commission).

Historic Resources: All buildings, sites, structures, objects and districts identified by

the Upper Uwchlan Historical Commission and approved by the Board of Supervisors.

Human-Scale Architecture: Proportion of space in relation to human dimension. It is also known as the natural design scale.

Infill Development: New buildings constructed in the space available between existing structures.

Mixed -Use: a land development containing two or more types of uses (typically a mix of residential, commercial, office, and/or institutional).

Multi-Modal Transportation: Transportation involving several modes of transportation, including vehicular, pedestrian, bicyclist, public transit, and ride sharing services.

Pedestrian-Oriented: An area of a community with an interconnected network of sidewalks, crosswalks, and walkways both within the community and to existing pedestrian thoroughfares and/or parks.

Rural Character: A community's identity with the history of the original village, surrounding countryside, while appealing to many rural, rural conveys natural or limited alteration and the Park and Lake are significant alterations. This identity is made of a combination of rolling and wooded hills, farmland landscapes, historic resources and historic district, parks and recreational areas, small shops, and both a strong sense of home and place, and a community spirit of belonging.

Redevelopment: The action or process of developing something again or differently.

Sense of Place: Feeling of emotional connection to a place and/or attachment people develop and/or experience in specific location.

Smart Growth: Range of development and conservation strategies to help protect our health and natural environment and make our communities more attractive, economically stronger and more socially diverse.

Streetscape: Space along a road corridor between the front facades of opposing structures that includes the street, any parking or shoulder area, pedestrian facilities (i.e., trash receptacles, benches, light fixtures), front yards, building facades, porches, landscaping, and all other features in accordance with Township standards.

Sub-Areas: Specific breakdowns of the Village Planning Area based on unique characteristics, including location, existing land use, zoning conditions, building patterns and architecture, the extent of historic resources and degree of development potential.

Village: A unique land use pattern developed in forming settlements that is organized by an intersection of two main streets to create a consolidated community with unique characteristics surrounded by a rural landscape (i.e., village fringe)

Village Planning Area (VPA): The specific geographic area within the Upper Uwchlan Township identified as appropriate for implementation of the planning and design principles set forth in the Plan; in this Plan, the VPA encompasses an area made up of Byers Station Village, the Historic Eagle Village, and other geographically defined areas surrounding them (see map #5 and description on page 10).

Introduction

The mission statement incorporates priorities identified by the Board of Supervisors for the entire Township and the Village Concept Plan Committee's mission for the future of the Village Planning Area.

The purpose of the Village Concept Plan is:

To identify recommendations for accomplishing defined goals while providing considerations for historic and open space preservation, active and passive recreation opportunities, responsible commercial zoning, and protection of environmental resources to continue maintaining a robust and vibrant community that attracts residents and visitors to this Village Planning Area



Goals

The following goals have been identified by the Board of Supervisors, Village Concept Plan Committee, and other stakeholders involved in the Plan update process:

1. Improve and maintain the visual integrity of the historic areas by:
 - a. Preserving Class I and Class II historic resources.
 - b. Developing and/or revising existing standards for new construction and redevelopment (including but not limited to alterations to existing structures).
 - c. Amending, as needed, the uses permitted in each zoning district under existing Township ordinances, as well as adding and/or revising, as needed, existing architectural standards and guidelines.
 - d. Reducing and managing vehicular traffic.
 - e. Implementing consistent streetscapes.
2. Improve walkability for pedestrians and encourage bicycle use.
3. Offer opportunities, such as seasonal events, history walks, a weekly farmers market or restaurant week, to attract more people to the villages of Eagle and Byers.
4. Improve the environmental resiliency of the village area with native plantings, green stormwater infrastructure, and shade trees.
5. Offer vibrant civic spaces that can also provide open green space and preserve or enhance natural resources.



Introduction

The Village Planning Area (VPA) spans approximately 315 acres from the north of the Pennsylvania Turnpike on its southern side to the intersection of Station Road and Graphite Mine Road on the northern side and from past Park Road on the western side to Township Line Road on the eastern side (See Map 3.1). It is predominantly designated based on the Township's main historic, cultural, social, and business core location, and historically significant resources and their connections with surrounding landscapes, natural features and the built environment.

In terms of general planning area designations, the VPA includes the following Sub-Areas (See Map 3.1):

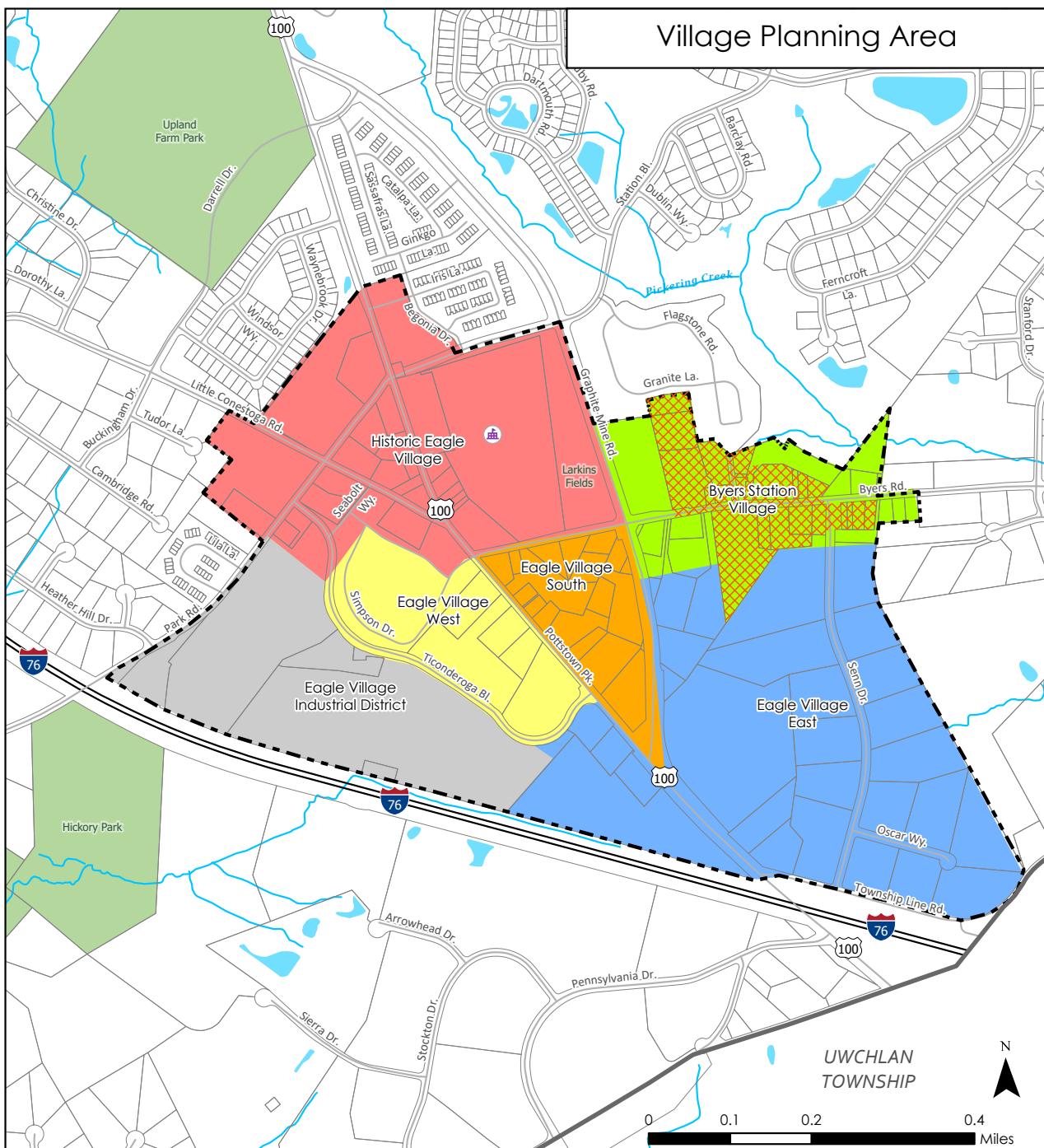
1. Historic Eagle Village— an area centered around Eagle Village and the Township designated cluster of historic resources known as the Eagle Village Historic Area
2. Byers Station Village— a collection

of historic resources centered around the Byers railroad station. This Sub-Area of the VPA includes the nationally designated Historic District of Byers Station, as well as several adjacent properties that either contain historic resources or small commercial enterprises.

3. Supporting Landscapes— designated by the Committee (Eagle Village East, Eagle Village West, Eagle Village South and the Eagle Village Industrial District) to be included in the Concept Plan due to each of the area's proximity to historic resources and opportunities to implement the Village Design Guide lines.

Map 3.1 shows the extent of the VPA boundary and designations within the community.

The Historic Eagle Village has a linear form roughly organized along both sides of Pottstown Pike and Byers Road. The core of the Historic



Village Concept Plan



Upper Uwchlan Township
Chester County, Pennsylvania

DRAFT

Disclaimer: This map is for planning purposes only and does not constitute a valid survey.

Data Source: Base data from Chester County Geographic Information Services, 2022.



Map created: June 28, 2022
Last Revised: June 10, 2024

	Township boundary		Lakes, rivers and streams
	Adjacent municipalities		Lakes, rivers and streams
	Village Planning Boundary		Public Parkland
	Estimated tax parcels		Municipal-owned open space
	Interstate		Byers Station Historic District
	Roads		

Map 3-1

Eagle Village has traditionally centered around the cluster of buildings at the intersection of Pottstown Pike and Little Conestoga Road, including the Township Building.

The core of the Byers Station Village is centered at the intersection of Byers Road and old Eagle Farms Road, now terminated at the Byers Station housing community. The Historic Eagle Village and Byers Station Village are perceived as separate entities because there are few historic resources in the intervening parcels and although public roads connect them, there are gaps in the sidewalks in the Historic Eagle Village that discourage pedestrian connections between the two cores. Significant areas of open space or underdeveloped parcels are found between the Historic Eagle Village and Byers Station Village (such as Larkin's Field, Pickering Valley Elementary School campus, and small residential parcels along Byers Road) that also contribute to the separation of the Historic Eagle Village and Byers Station Village.

Map 3.2 depicts the location and extent of the Historic Eagle Village and Byers Station Historic District and the Township's historic resources from the "Historic Resources Inventory: Upper Uwchlan Township, Chester County, Pennsylvania (Wise Preservation Planning, 2001)" and the Historic Resources Map Update (Richard Grubb and Associates, 2007) that are located within the VPA.

Byers Station Historic District

The Byers Station Historic District is listed on the National Register of Historic Places and consists of twenty-six contributing historic resources at the time of its listing in 2002. The community grew around the Byers railroad station after its opening in 1871 to support the Township residents' rail shipment of agricultural products via Phoenixville to Philadelphia, and the community continued to grow after plumbago (graphite) was discovered in 1875. Byers Station Historic District includes several Italianate and Greek Revival buildings, including the former Byers Hotel (c. 1874), the John Todd House (c. 1875) and a Masonic Hall (1894). With the exception of one reconstructed residence (#67b), all contributing historic resources are Class I historic resources.

Historic Eagle Village

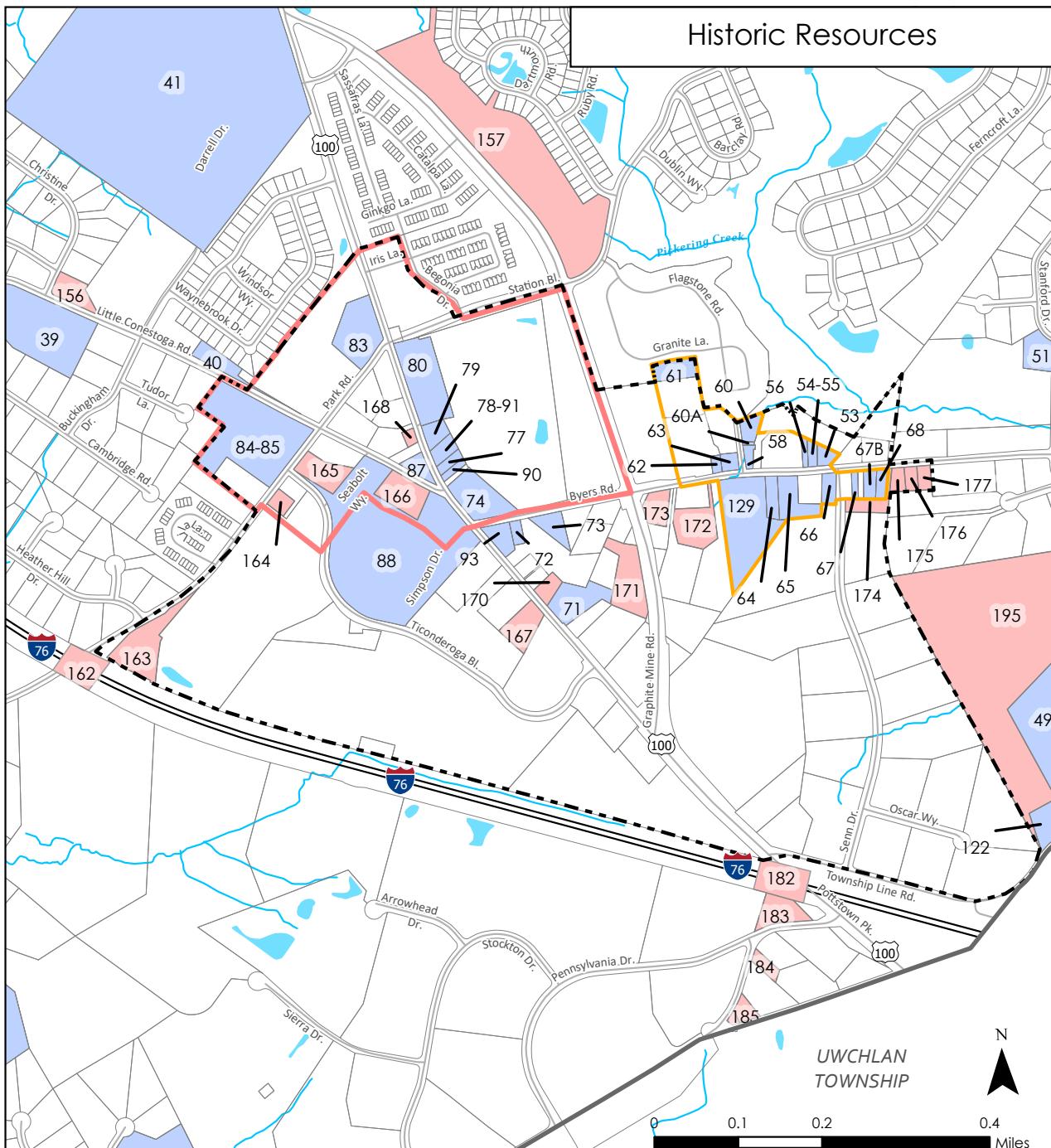
The Eagle Village Historic Area includes eleven historic resources. These historic resources generally date to the 18th or 19th century. Notable buildings include a variety of residences (c. 1750 to c. 1875), a one-room schoolhouse (1858) and a tavern (c. 1858). The Eagle Village Historic Area grew around the tavern located at the crossroads of Pottstown Pike and Little Conestoga Road.

All the historic resources in the Eagle Village Historic Area are generally classified as Class II, recommended to Class I. The exceptions to this are historic resources #74 (Class I) and #168 (Class II).

Supporting Landscape/Village Growth Area

The VPA includes additional areas surrounding the Eagle Village Historic and Byers Station Village. The main rationale for including the supporting landscape in the VPA includes but is not limited to:

1. Guide preservation and any development adjacent to the Historic Eagle Village and Byers Station Village to protect both areas and their village settings.
2. Promote residential use of all historic resources that are former residences within the VPA, including the upper floors of the historic resources being used for commercial purposes.
3. Create a transitional buffer area between both the Historic Eagle Village and Byers Station Village and adjacent land uses.
4. Allow for consistency in landscaping, streetscaping and architectural aesthetics with the Historic Eagle Village and Byers Station Village.
5. Provide opportunities for potential redevelopment and infill that maintain the historic character of the Historic Eagle Village, Byers Station Village and adjacent areas, per guiding principles described in the Plan.
6. Integrate and recognize all the historic



Map 3-2

resources within the VPA (not just those within the Historic Eagle Village or Byers Station Village).

7. Highlight opportunities for bicycle and pedestrian connections beyond the Historic Eagle Village and Byers Station Village.
8. Explore potential for civic spaces within walkable distance to the Historic Eagle Village and Byers Station Village.

Additional historic resources are found throughout the VPA's C-1 and C-3 zoning districts. These additional historic resources are classified as Class II resources. There are no historic resources in the LI zoning district.

Village Guiding Principles

Although many residents recognize the Historic Eagle Village and the Byers Station Village as traditional “Village” centers, there is a high risk of these traditional “village” centers slipping into obscurity as future commercial and retail chains build “branded” buildings that could become more dominant on the Village landscape. Therefore, the Committee has developed specific guiding principles for the entire VPA to maintain the community’s sense of place and belonging while supporting continuing growth.

The Village guiding principles incorporate standards of best planning practices relevant to the Plan’s goals noted above, the Committee’s feedback and consultants’ recommendations to define the planning framework for the future vision of the VPA. The best planning practices consist of planning elements that influence the future look, feel, character and quality of life depending on how they are integrated into the community. They also provide ideas for specific recommendations and implementation strategies to fulfill the Village Concept Plan vision.

Smart Growth

“We envision a country where no matter where you live, or who you are, you can enjoy living in a place that is healthy, prosperous, and resilient.”

SmartGrowthAmerica.org

Smart Growth Principles are design principles developed by the Smart Growth Network and

are widely recognized and accepted in the planning community. The main idea behind smart growth is to encourage development that supports strong communities, environmental health, and economic growth. That can be achieved by implementing creative strategies to preserve natural lands and critical environmental areas, protect water and air quality and reuse already-developed land. Based on the experience of communities across the U.S., the following main Smart Growth Principles are generally utilized in drafting future planning policies and programs:

1. Mix land uses.
2. Take advantage of compact building design.
3. Create a range of housing opportunities and choices.
4. Create walkable neighborhoods.
5. Foster distinctive, attractive communities with a strong sense of place, including but not limited to communities that preserve historic resources.
6. Preserve open space, farmland, natural beauty, and critical environmental areas.
7. Strengthen and direct development toward existing communities.
8. Provide a variety of transportation choices.
9. Make development decisions predictable, fair and cost-effective.
10. Encourage community and stakeholder collaboration in development decisions.

Additionally, Smart Growth principles recognize that the renovation of any historic property, structure or site is frequently a starting point and anchor for the redevelopment of a street, neighborhood or district. Rehabilitating and repurposing historic properties and old buildings promotes energy efficiency, reduces the need

for new buildings, promotes open space preservation and sustainable use of existing resources and maintains the sense of place and a desired character of the community.

Several areas within the VPA, including the Byers Station Village and the Historic Eagle Village, already meet or exceed many elements of smart growth, such as encouraging a mixture of land uses, providing a range of housing options, pedestrian connections, and the protection and adaptive reuse of historic resources. An element that may need additional planning and careful consideration is preservation of remaining open space, or redevelopment of remaining open space for opportunities to create civic spaces for recreation and community gatherings.

Village Planning Guide

“A successful village is one in which many needs are satisfied within the community without detracting from its residential character or degrading its natural resources.”

Chester County Planning Commission

The Village Planning Handbook: Guide for Community Planning developed by the Chester County Planning Commission (1993) provides background information on the history of the development of villages in Chester County and outlines specific characteristics that are typical of village use patterns, including:

1. Central clustered arrangement of structures and buildings surrounded by a relatively rural landscape.
2. Distinctive mix of land use types, including residential and non-residential uses compatible and mutually supportive of each other.
3. The mix of housing types in various configurations, including detached, semi-detached, attached or individual units in a complex.
4. The presence of open space sustains the village’s rural character, including a surrounding buffer (“village fringe”) of open

land which separates the compact built environment from adjacent land areas and within the core of the village.

5. Balance of residential and non-residential use to ensure that community functions are maintained.
6. Pedestrian-oriented streets and pedestrian amenities.
7. Retention of specimen trees, tree lines and woodlands to retain the sense of association with the village feel.

Considering the history of how the area has developed, many characteristics typical of the Villages are still visible today in the Byers Station Village and the Historic Eagle Village. These characteristics are clustering of buildings and structures at the main core, compatibility of distinctive mix of land uses and housing types, and pedestrian oriented streets and amenities. The Township has also been very proactive with implementing regulations aiming to retain specimen trees and healthy woodlands during the land development process. One area of improvement is to potentially invest in green infrastructure and planting of trees to create a visual separation between the VPA and surrounding landscapes.

Landscapes Designations

The Chester County Comprehensive Plan, Landscapes 3 (2018), and the associated “Livable Landscapes” map provided guidance for accommodating expected future growth while maintaining the quality of life in the County. Livable Landscapes recognized the unique character of each community and provided recommendations to maintain that character based on the mix and intensity of land use. The Livable Landscapes map was divided into two core areas — growth areas and rural resource areas, as prescribed by the Pennsylvania Municipalities Planning Code (MPC). Growth Areas consisted of the urban landscape and suburban landscapes and suburban centers meant to accommodate future growth. Rural Resource Areas consisted of the rural landscape, the agri-

cultural landscape, and rural centers and were least appropriate for additional growth. Since 2020, the County has been developing design guides pertaining to each Landscapes category to provide examples and sample ordinance language for creating developments and zoning consistent with the Livable Landscapes map.

It is important to note that the position of Upper Uwchlan Township is unique with its geographic location between very different areas: Exton (West Whiteland Township), designated as a suburban center, to the south, the rural center at Ludwigs Corner (West Vincent Township) to the northeast, and the rural designations in West Vincent, East Nantmeal, and Wallace to its east north and west. Exton has emerged as a desirable hub of activity and social life, surrounded by suburban landscapes, and includes Main Street with restaurants, shopping centers, higher density residential, pedestrian amenities, and connections, and convenient access to major transportation networks running east to west and north to south. On the other hand, Ludwigs Corner is a quaint place at the intersection of Conestoga Road and Pottstown Pike, surrounded by rural landscapes, including small businesses and shops, low-density residential and recently developed apartment complexes, and Class II historic resources.

As with Ludwigs Corner, the VPA is organized at historic crossroads of Byers Road and Pottstown Pike and Little Conestoga Road and Pottstown Pike; it has small-scale businesses, pedestrian amenities, a mixture of low-density residential and commercial structures and clusters of historic resources. Like Exton, the area provides quick access to major regional transportation routes and is surrounded by higher density residential neighborhoods, which have developed at a higher density over the past decade. However, the desired future is to maintain the existing sense of place and strong community bonds by limiting growth and focusing on improvements meant to support the dynamic of modern village life while preserving historic resources and providing opportunities for walkable recreational access.

Rural Center Design Guide

The Chester County Rural Center Landscapes Design Guide (December 2022) provides rec-

ommendations for implementing planning activities in designated rural centers (i.e., Ludwigs Corner) to foster new development that is compatible with the community and historic character, protect residential neighborhoods from adverse impacts, encourage walkability and alternative modes of transportation, protect and restore the environment and guide appropriate development. The guide's purpose and recommendations are more consistent with the vision for the future of the VPA and its sub-areas than the design guide for suburban center landscapes, although the latter does include helpful planning principles and design elements that are applicable for this Village Concept Plan, including stormwater management through green infrastructure, limited parking in front of buildings and interconnected open space systems to encourage recreation. The following are the key planning principles introduced in the Rural Center Landscapes Design Guide:

Growth Outlook

- Limited future growth that contains a mix of housing, commercial, and institutional uses at a village scale and is highly walkable at its core.
- Maintains economic viability that supports local employment and meets fair share housing requirements (the need to provide its fair share of all housing types, including multi-family).
- Serves surrounding rural and agricultural landscapes by creating an economic strategy that enhances the viability of surrounding agricultural lands and conserves natural landscapes and scenic vistas to preserve the sense of place.

Preservation Focus

- Preservation of historic buildings to connect with a community's past, provide a sense of continuity and permanence, and maintain character.
- Reuse of historic buildings to support sustainability.

Land Use Patterns

- Encouraging a variety of housing types

to provide affordable opportunities for diverse populations and local workforce.

- Encouraging a variety in commercial buildings to boost employment opportunities and potential for adaptive reuse of historic buildings.
- Concentrating a mix of uses within the core of rural centers to strengthen community walkability, interaction, and viability.
- Compatibility of institutional (i.e., schools, municipal buildings, and places of worship) in scale and form with existing settings.
- Encouraging farmers' markets and other appropriate agricultural activities.

Infrastructure

- Developing small parks or town squares for community gatherings to facilitate community identity and support civic bonds.
- Creating bicycle and pedestrian facility connections between residential areas and core areas of communities with multiple activities.
- Encouraging infill and development in areas planned for village center growth before extending sewer and water infrastructure.
- Encouraging appropriately scaled road ways that are context sensitive and provide for non-vehicular travel.
- Incorporating modern high quality communication infrastructure (wireless networks, fiber optics, and broadband communications) to support residents and businesses and local economy.

Several areas of the VPA are historically organized and developed as per rural center design elements, including emphasis on walkability in the VPA core, mix and compatibility of land uses and encouraging local business vitality, preservation, and reuse of historic buildings, and focus on multi-modal improvements. The Township can continue sustaining that rural character and sense of place by moving forward

with implementation strategies proposed in this Plan.

Guiding Principles for the VPA

The Committee has developed the following guiding principles that align with the Plan's mission and goals and serve as a foundation for recommendations on implementing the Village Concept Plan in the future. The principles also include recommendations on how each of these principles can be achieved.

Distinctive Sense of Place.

- Implement the Village Design Guide lines to ensure that the rural village feel remains in the community.
- Provide public open spaces, pocket parks and other opportunities for outdoor community gatherings where appropriate.
- Install interpretive signage highlighting the village's history and culture, as well as in the historic district.
- Promote effective emergency management that blends with the community's character while providing a sense of safety.

Mix of Compatible Land Uses.

- Encourage clustering of buildings to preserve unique historic, cultural, and environmental resources.
- Provide opportunities for mixed uses such as shops, offices, services, recreation, apartments, live/work units.
- Encourage a mixture of housing choices to foster housing affordability.

Flexibility in Site and Building Design.

- Incentivize creative parking solutions, including shared parking, where appropriate.
- Maintain setback requirements for principal buildings (where appropriate) to maintain the rural character and historic village feel.
- Foster pedestrian connections to near

- by areas where opportunities exist to expand walking connectivity network.
- Encourage the use of green infrastructure and innovative stormwater management.

Managed Vehicular Access.

- Promote interconnected village streets to disperse traffic.
- Provide design standards for village parking areas.

More Walking and Biking, Less Driving.

- Develop streetscape design standards to include street trees, sidewalks, cross walks, and village light standards to facilitate walking among businesses, shops and restaurants where opportunity exists.
- Provide civic space for outdoor leisure and interpretation of historic resources.
- Provide seating and bicycle racks to promote active transportation and reduce vehicular traffic.
- Facilitate various non-motorized transportation choices.
- Consider bicycle and/or scooter-sharing programs.
- Promote parking facilities for bikes, scooters, compact cars, etc.
- Encourage shared parking among compatible land uses to reduce the amount of required parking.

Quality Architecture and Site Design.

- Promote human-scale (the proportion of space in relation to the human body), architecture.
- Promote the local architectural vernacular.
- Regulate signage.
- Encourage extensive landscaping in parking lots.

Inclusion and Accessibility.

- Provide opportunities for accessibility within the village and connected shopping areas.
- Encourage addressing accessibility issues during the redevelopment process and/or new construction.

Environmental Resiliency.

- Promote the use of clean energy where appropriate within the village planning area while retaining the historic character.
- Promote electric vehicle (EV) charging stations.

Area Analysis

The analysis includes an assessment of existing land use, relevant zoning regulations, building patterns, circulation and assessment management, pedestrian and bicycle amenities, opportunities for clean energy systems and potential future developments expected within the VPA.

Existing Land Use

The Village Planning Area is characterized by a variety of land uses. Map 3.3, Village Planning Area Land Use, displays the extent of a variety of land uses, including commercial, residential, industrial and others within the VPA. The map is based on the existing tax assessment records of the Chester County Office of Tax Assessment, supplemented by aerial analysis and confirmation by the Concept Plan Committee and the Township staff to confirm the current classification of uses according to the current use of the property. The land use categories are summarized below in the table with acreage with most dominant at the top.

Land Use Category	% of Total	Total Acres
Commercial	44%	172
Residential	5%	21
Vacant	15%	59
Municipal-owned	5%	19
Mixed-use residential	1%	5
Open space	15%	60
Institutional	7%	29
Utilities	1%	5
Multi-family	6%	23
Public/Private recreation	<1%	0.4
Total	100%	393

Table 3.1. Land Use categories with percent of total VPA (2022).

There are approximately 393 acres of total developed and undeveloped land within the VPA. The predominant land use is commercial (43%), followed by open space (15%) and vacant (14%). There are several vacant parcels with a total of approximately 68 acres: an area at the intersection of Byers Road and Graphite Mine Road (currently in the development phase), an area off Darrell Drive, an area at the corner of Station Blvd and Pottstown Pike, areas along PA Turnpike and the area between E. Township Line Road and PA Turnpike. Vacant lands provide great opportunities for open space preservation and/or creating civic spaces for passive recreation and community gatherings.

There is also a sizeable commercial property located in the core of the VPA, surrounded by existing commercial development. If redeveloped as per the vision identified in the Plan in the future, there might be an opportunity to create much-needed public civic space while preserving additional open space near the main VPA core that is currently primarily commercial.

It is important to note that several vacant properties are located along the boundary of the VPA, in some instances adjacent to recreational areas. The future preservation of these properties and/or appropriate landscaping will create a village fringe, one of the unique characteristics of rural village character.

Overall, several areas have the potential for new development, redevelopment, or infill development, including:

- Park Road, Little Conestoga Road, and Pottstown Pike core
- West of Pottstown Pike and east of Ticonderoga Boulevard
- Both sides of Byers Road
- Along both sides of Pottstown Pike south of Graphite Mine Road
- Along portions of Graphite Mine Road
- Along both sides of Ticonderoga Road
- Along both sides of Township Line Road

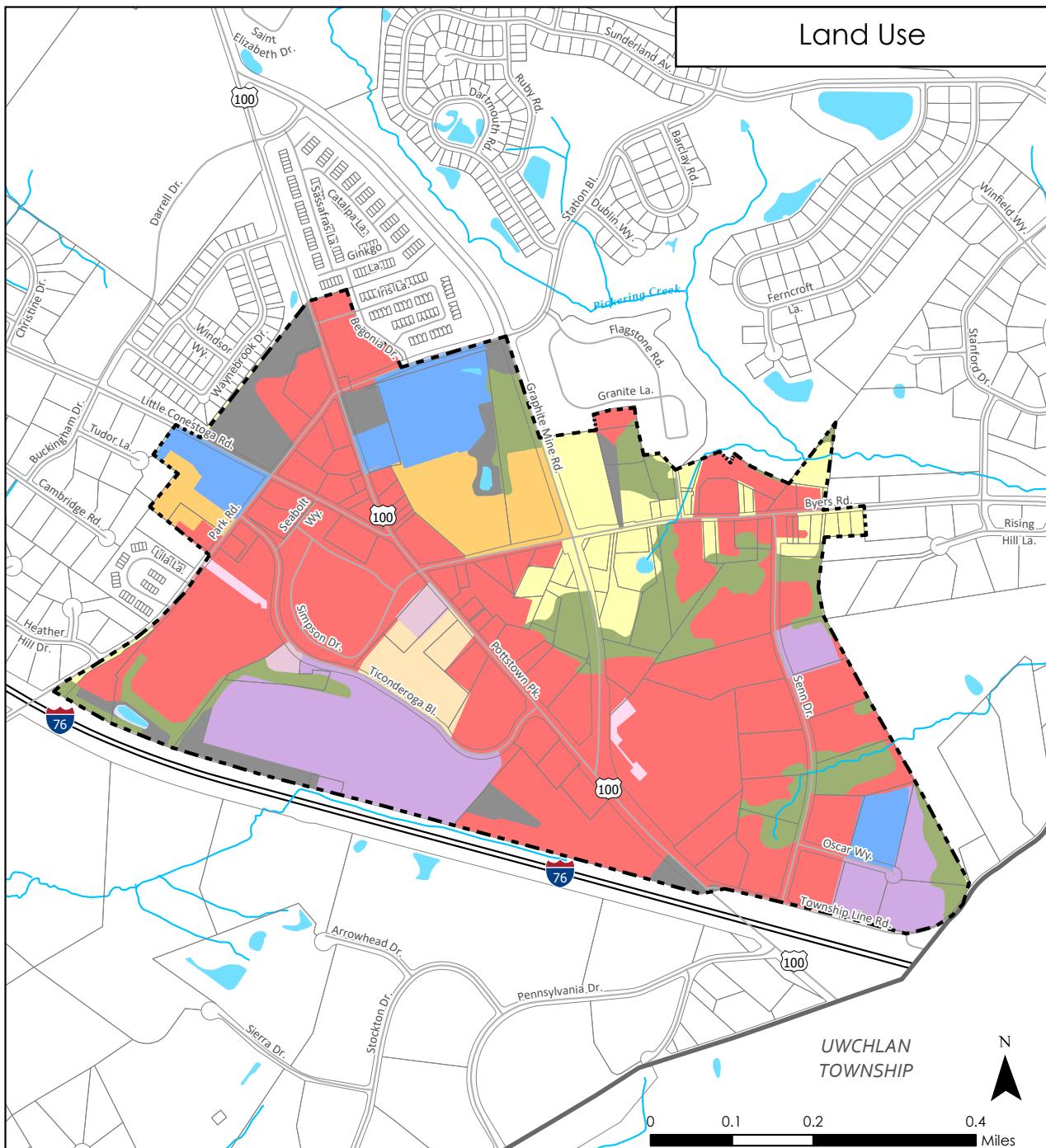
Township Regulations

The Village Planning Area includes the following municipal zoning districts: C-1, C-3, R-2, R-3, R-4, R-2 and LI, as shown on Map 3.4, Existing Zoning Districts . A summary of regulations pertaining to each zoning district, including provisions for historic preservation and adaptive reuse and their relevance to the VPA is described in this section.

Historic Resources Protection Standards

Currently, there are provisions in the Zoning Ordinance and Subdivision and Land Development Ordinance, which are intended to help preserve and protect the Township's historic features.

The subdivision and land development design standards encourage maintenance and preservation of historic resources (i.e., historically, culturally, or architecturally significant buildings, monuments, sites or potential archaeological sites, unique historic landscapes). That includes requirements to consider all available adaptive reuse options that may keep the historic resource viable, as well as tax and other



Village Concept Plan

Upper Uwchlan Township
Chester County, Pennsylvania

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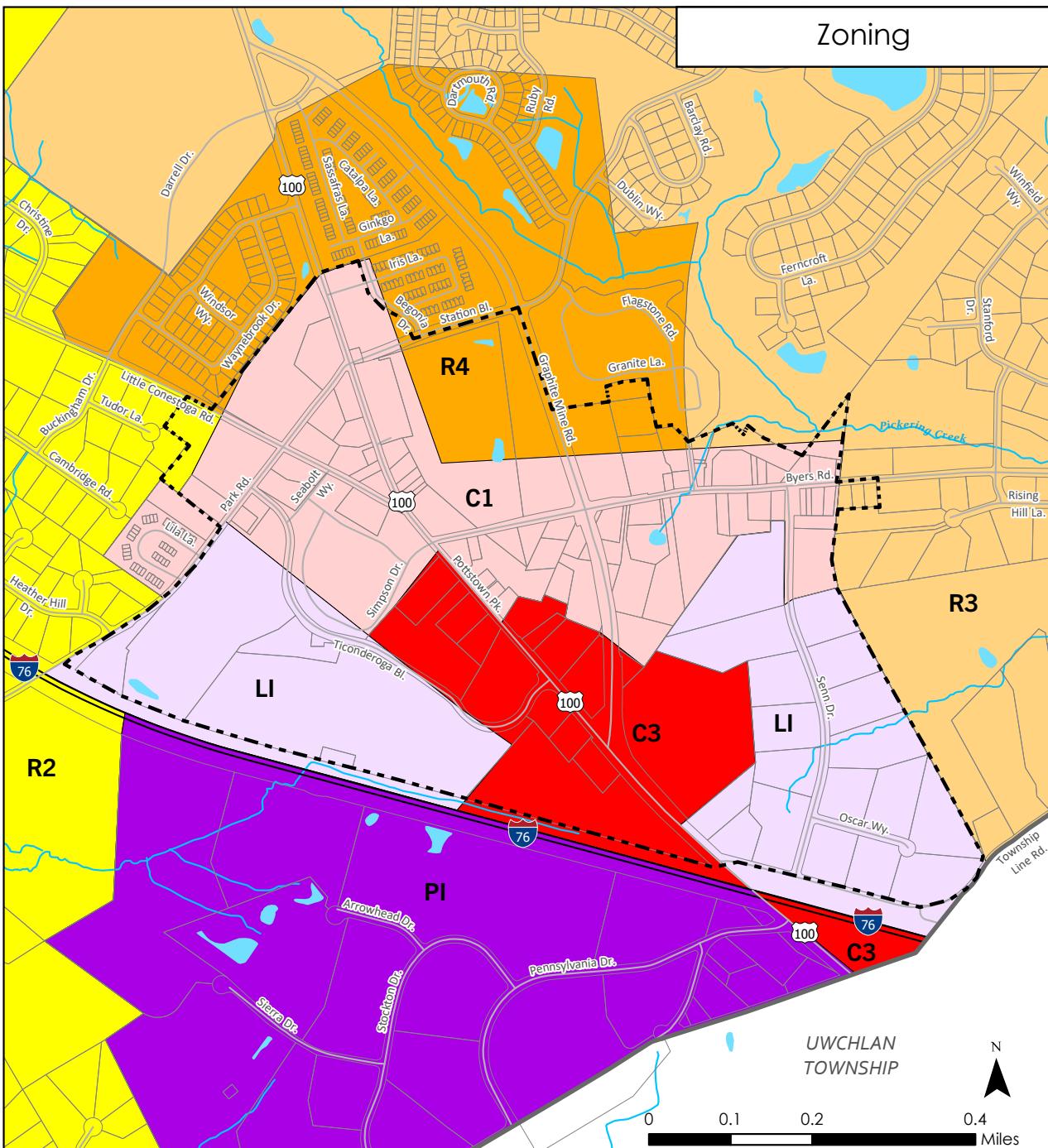
Data Source: Base data from Chester County Geographic Information Services, 2022.



Map created: June 28, 2022
Last Revised: June 10, 2024

Village Planning Boundary	Existing Land Use	Transportation
Township boundary	Agriculture	Undeveloped
Estimated tax parcels	Commercial	Utility
Lakes, rivers and streams	Industrial	Water
Lakes, rivers and streams	Institutional	
	Recreation	
	Residential	

Map 3-3



Village Concept Plan

Upper Uwchlan Township
Chester County, Pennsylvania



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Data Source: Base data from Chester County Geographic Information Services, 2022.



Map created: June 28, 2022
Last Revised: June 10, 2024

- Village Planning Boundary
- Township boundary
- Estimated tax parcels
- Interstate
- Roads
- Lakes, rivers and streams

Zoning

- R1 - Residential District (1 dwelling unit/2 acres) - 2412.272 ac.
- R2 - Residential District (1 dwelling unit/acre) - 3368.329 ac.
- R3 - Residential District (1.5 dwelling units/acre) - 779.681 ac.
- R4 - Residential District (2-4 dwelling units/acre) - 239.306 ac.
- C1 - Village Commercial District - 117.856 ac.
- C3 - Highway Commercial District - 125.485 ac.
- LI - Limited Industrial District - 255.956 ac.
- PI - Planned Industrial/Office District - 206.370 ac.

Map 3-4

incentives available to preserve the historic resource, limiting opportunities for relocating historic resources, and requirements for interpretive signage, easement, and maintenance responsibilities.

Adaptive reuse of historic preservation regulations encourages reuse of any identified historic structures located on a tract listed in the Historic Resources Inventory of Upper Uwchlan Township. Home occupation, professional or business office, cultural studio, day-care center, bed-and-breakfast, and other uses of a similar nature and similar community impact are permitted by right in the C-1, C-3, LI, and PI Zoning Districts. Home occupation and no-impact home-based businesses are permitted by right in the residential zoning districts. Adaptive reuse of any structure as an additional principal use on the same lot is permitted via conditional use process in any zoning district. Professional or business office, cultural studio, day-care center, bed-and-breakfast, and other uses of a similar nature and similar community impact, and multiple-family dwelling are permitted via conditional use process in residential districts. Adaptive reuse provisions also include specific modifications to area and bulk regulations and specific requirements for adaptive reuse for multiple-family dwellings.

Specific design standards for the preservation of historic resources are included in the C-1 Village District and applicable to C-3 Highway Commercial District and R-2, R3, and R-4 Districts as described below (see the Township's Ordinances for any updates since the adoption of this Plan).

C-1 Village

The purpose of the C-1 Village District is to preserve the historical development patterns of the Village of Eagle and the Byers Station Historic District and establish standards for new development and coordinate street and landscape improvements and pedestrian amenities to complement the village setting and provide safe and convenient access and provide for a variety of uses in a manner which facilitates and promotes pedestrian travel within the village setting and preserves historical resources.

Business or professional offices, financial in-

stitutions, individual retail stores, shops, grocery stores, bakeries, personal service establishments and medical marijuana facilities are allowed by right. Educational, cultural studios or facilities, day-care centers, financial institutions with drive-throughs, individual retail stores, mixed-use dwellings, bed-and-breakfast inns, restaurants, and adaptive reuse for historic preservation are allowed via the conditional use process.

The C-1 district regulations include design standards for preserving historic resources, architectural design, pedestrian and vehicular access, streetscape design, and stormwater management, and reference to supplemental land use regulations pertaining to a specific use (i.e., adaptive reuse for historic preservation, shopping center standards), common regulations about specific development features (i.e., parking, landscaping, screening) and signage. These design standards apply to all uses by right and/or conditional use approval and include (as described in the current Zoning Ordinance):

- Preservation of historic structures and historic resources, including historic landscapes, to the greatest degree practicable.
- Required Township's approval for the removal of historic resources.
- Standards for certain modifications of historic resources in compliance with the Secretary of the Interior's Standards for Rehabilitation.
- Requirements for the use of authentic period materials and colors for historic resources visible from any existing or proposed right-of-way.
- New construction is to be designed with either traditional village architectural character or a contemporary expression of traditional styles and forms, respecting scale, proportion, roof pitch, character, and materials of historic examples in Byers Station Historic District, Village of Eagle, and the surrounding area.
- Pedestrian access to be designed to provide convenient, safe, and direct

- access between various uses and other nearby concentrations of development.
- Vehicular access is designed to limit the number of new access points to public roads.
- Parking areas are to be landscaped and appropriately screened and not be provided in the front yard.
- Streetscape landscaping and pedestrian amenities to be provided to meet overall village planning objectives, including potential requirements for trash receptacles, public benches, bike racks, planting strips and pedestrian paving materials.
- Additionally, there are architectural design guidelines for new construction in the C-1 district. These guidelines include maximum façade length, desired materials, color, and aesthetics for pitched roofs, types of exterior wall materials and specific structural elements, and a building size restriction of 6,000 square feet.

C-3 Highway Commercial

The purpose of the C-3 Highway Commercial District is to provide retail and service uses in an area of the Township accessible to a regional highway system. The C-3 District establishes standards for a unified and organized arrangement of buildings, service, and parking areas, to facilitate access management and provide for safe, convenient, and attractive commercial activity in the Township.

Business or professional offices, financial institutions, passenger station for public transportation, individual retail stores, restaurants, personal service establishments (i.e., beauty salons, tailor, barbershop), retail or wholesale establishments, educational or religious and medical marijuana dispensaries are allowed by right. Day-care center, hotel or motel, bed-and-breakfast inn, indoor amusement center, gas stations, auto sales establishments and car wash, mixed-use dwelling, and adaptive reuse for historic preservation are allowed via conditional use process. Additionally, uses permitted in the C-1 Village District whether by right, con-

ditional use process or special exception are permitted in the C-3 district.

The C-3 District regulations also provide a reference to supplemental land use regulations pertaining to a specific use (i.e., adaptive reuse for historic preservation, shopping center standards) and common regulations on specific development features (i.e., parking, landscaping, screening). The design standards specified in the C-1 Village District that are described previously are also applicable within the C-3 District under certain circumstances.

R-2 Residential

The purpose of the R-2 Residential District is to provide for low to moderate density residential development in areas of the Township which are not generally characterized by steep slopes or floodplains, but which nevertheless contain soils conditional for on-lot sewage disposal systems and variable groundwater yields; to encourage continued agricultural, open space and conservation uses in the R-2 District while also permitting low to moderate density residential development compatible with established use patterns; and to encourage and promote phased development throughout the Township by permitting low to moderate density residential uses in the R-2 District until other areas of the Township more accessible to adequate services and transportation are fully developed in medium and high density residential uses. The R-2 District establishes maximum density standards which provide the conservation of surface water and groundwater supplies, soils for on-lot sewage disposal systems, and the control of soil erosion and flooding.

Any uses permitted by right in the R-1 Residential District, including single-family detached and semi-detached (twin) and two-family detached dwellings (duplex), group homes and agriculture, are allowed by right. Educational or religious use, cemeteries and adaptive reuse for historic preservation are allowed via conditional use.

The specific design standards are not included but provide a reference to supplemental land use regulations pertaining to a specific use (i.e., adaptive reuse for historic preservation, shopping center standards), common regulations

pertaining to specific development features (i.e., parking, landscaping, screening) and signage.

R-4 Residential

The purpose of the R-4 Residential District is to provide the opportunity for high-density residential development to occur in areas that are compatible with established use patterns, located in areas of the Township which contain environmental features generally suitable for such development and accessible to transportation systems and community facilities appropriate for such development. The R-4 District is designed to provide the opportunity for accommodating multifamily units at a density consistent with the character of the Township.

Any uses permitted by right in R-1 and R-3 Residential Districts, including single-family detached and semi-detached dwellings (twin) and two-family detached dwellings (duplex), group homes and agriculture, are allowed by right. Multi-family dwellings and adaptive reuse for historic preservation are allowed via conditional use.

The specific design standards are not included but provide a reference to supplemental land use regulations pertaining to a specific use (i.e., adaptive reuse for historic preservation, shopping center standards), common regulations pertaining to specific development features (i.e., parking, landscaping, screening) and signage.

LI Limited Industrial

The purpose of the LI Limited Industrial District is to provide for limited industrial and other related intensive activities in the Township, to encourage the establishment of industrial uses which will offer additional employment opportunities and an increased tax base for the Township; to establish reasonable standards governing industrial development to ensure its compatibility with the character of the area and adjacent land uses, and to locate industrial uses within proximity to major roads to provide safe and efficient access by industrial-related traffic.

Light manufacturing, medical marijuana grower/processor, laboratories, public utility operating facilities, printing or publishing establish-

ments, office buildings, wholesale warehouse and distribution, churches and other religious uses are allowed by right. Surface mining operations, sanitary landfills, junkyards, recycling collection centers, auto body repair, indoor health spas, tower-based communication facilities, adult-oriented uses, and municipal or public uses are allowed via the conditional use process. Additionally, bakeries, retail sales, restaurants and adaptive reuse of historic preservation are allowed via conditional use approval when those uses are established on a property designated by the Township as a Class I or Class II Historic Resource where historic buildings shall be adaptively reused.

Similar to R-4 District regulations, specific design standards are not included but provide a reference to supplemental land use regulations pertaining to a specific use (i.e., adaptive reuse for historic preservation, shopping center standards) and common regulations pertaining to specific development features (i.e., parking, landscaping, screening).

In summary, in both the C-1 and C-3 Districts, mixed-use developments via provisions for any two or more principal uses and mixed-use dwellings are permitted when approved by the Board of Supervisors as a conditional use. Also, opportunities for twins, duplexes, group homes and multifamily dwellings exist within the R-4 district. There is a 35-foot height limitation on structures in all zoning districts. Additionally, all districts within the Village Planning Area highly encourage adaptive reuse for historic preservation. However, only the C-1 Village District and C-3 Highway Commercial District provide specific design guidelines for historic preservation.

Building Patterns

A mixture of historic and modern, residential, and commercial structures is currently located in the Village Planning Area. The historic building patterns are erratic, with various street setbacks, building footprints, and building materials. Historically, building footprints had been small, ranging from 600 to 1,200 square feet. Modern structures, including a grocery store/mall and pharmacy, have footprints in the range of 2,800 to 20,000 square feet. Most structures are either single- or two-story. Several new structures and uses have been added to His-

historic Eagle Village and surrounding areas since 2010 that have influenced the village dynamic:

- Expansion of municipal building and pocket park on Pottstown Pike south of Park Road
- Expansion of Profound Technologies building on Little Conestoga Road
- Construction of daycare on Little Conestoga Road
- Introduction of Starbucks drive-through on Pottstown Pike north of Byers Road
- Introduction of municipal playing fields north of Byers Road at Graphite Mine Road
- New commercial offices on Little Conestoga Road south of Park Road
- A new multi-use trail connects [Village of] Eagle with neighborhoods west of Hickory Park and Marsh Creek State Park
- New sidewalks and trails connect surrounding neighborhoods to the Village Planning Area
- New Upland Farm Park and refurbished barn on Darrell Drive west of Pottstown Pike

Within the Historic Eagle Village and Byers Station Village, historic residential structures along Pottstown Pike and Byers Road continue to be successfully adapted to commercial or office uses and, in some instances, offer residential apartments on upper floors.

Circulation and Access Management

The VPA includes parcels bordering Park Road, Little Conestoga Road, Pottstown Pike, and Graphite Mine Road, as depicted on the Village Planning Area map. The most heavily traveled road within the Historic Eagle Village is Pottstown Pike (PA Route 100). Graphite Mine Road was constructed to alleviate congestion along Pottstown Pike through the Historic Eagle Village. However, since its completion in 2009, Graphite Mine Road has yet to successfully divert through traffic around the Village. The Township is currently in discussions with

the Pennsylvania Department of Transportation (PennDOT) to realign the designated route for PA Route 100 to Graphite Mine Road to help alleviate traffic through the Village's core.



In 2010, the Township implemented the Access Management Ordinance to provide vehicular access to land development that preserves the transportation system's safety and efficiency. The provisions apply to C-1, C-3, and LI districts and include limitations on the number of driveways, driveway location and spacing, restriction to one roadway access, and specific requirements for driveway design. The regulations apply to new land development and applications for building permits. They are beneficial in limiting direct lot access to main roadways within the VPA, facilitating traffic flow and diverting traffic from the main core.

The Township identified two village streetscape projects in 2014: along Byers Road and a portion of Pottstown Pike. The Pottstown Pike streetscape redesign was completed with street light installation, sidewalk improvements and tree plantings. However, the existing streetscape treatments are not consistent throughout the VPA. There is no clear theme of the appearance or view of streets for the Village beyond that established along Pottstown Pike in the Eagle Village core.

Pedestrian and Bicycle Amenities

The VPA includes a multi-use Township trail, side paths and neighborhood trails that connect the Eagle Village Historic Area and Byers Station Village with nearby residential communities, the Pickering Valley Elementary School, Larkin's Field, Upland Farm Park with a restored barn, Hickory Park and other trail systems. The sidewalks in the Village are currently fragmented (i.e., disconnected from each other), which prevents many residents and visitors from walking throughout the Village. Several marked crosswalks along Pottstown Pike have a button crosswalk system to provide safe crossings to Pottstown Pike. However, those are primarily within the Eagle Village core. Opportunities for safe biking within the VPA are minimal, and bike-supporting amenities are lacking, such as defined bike lanes, signage, and bike racks.

The Township recently completed the Active Transportation Plan that illustrates a network of facilities that allow residents and visitors in the Township to access a broad range of destinations and uses without needing to use an automobile. Furthermore, the study aims to increase the sense of community and recreation opportunities in Upper Uwchlan Township while increasing overall health benefits for its residents.

Clean Energy Systems

The Committee recognizes the potential for future installation of alternative energy systems within the VPA, including rooftop or ground-mounted solar systems, solar panel components and/or electric vehicle (Evs) charging stations. Alternative energy systems, including solar, wind and geothermal, are allowed as an accessory use in all districts, including those parts of the VPA with several limitations, including:

- Solar and wind energy systems are prohibited in all public open spaces, within wastewater reclamation areas, greenways, and riparian buffer areas.
- Wind energy and geothermal systems are prohibited in the C-1 Village District and designated historic districts.
- Wind energy systems are permitted in the LI Limited Industrial District as an

accessory use via conditional use approval.

Currently, no regulations address the installation of rooftop solar systems on historic resources. And there are no regulations for installing Electric Vehicle charging stations in the Township, and none exist within the VPA. Considerations shall be given on encouraging the use of alternative energy systems while retaining the VPA's sense of place and rural character.

Future Development

Several new/pending developments will also influence the Village dynamic:

- Ongoing construction of continuing care facility on Graphite Mine Road north of Byers Road.
- Once entirely constructed, Byers Station Parcel 5C (the commercial uses of which are located within the VPA) will include 172 townhomes, a daycare facility, a drive-through restaurant, and 13,200 square feet of retail space.
- Pending development of 301 Park Rd.
- Pending development on the south side of Byers (164 Byers).

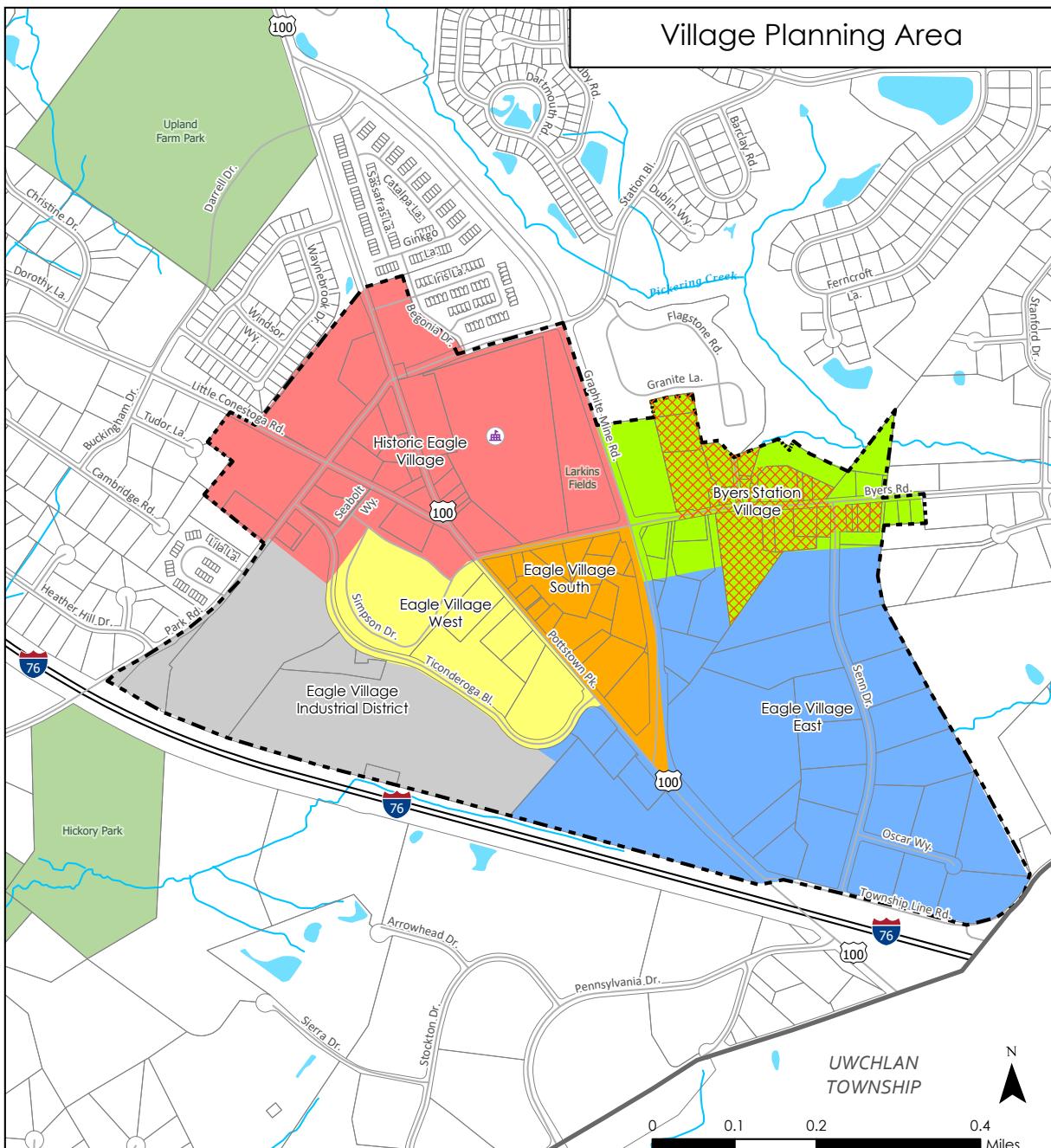


Introduction

Due to the extent and unique characteristics of different geographic areas within the VPA, the Committee identified six sub-areas (see Map 4-1) to evaluate conditions for future growth and/or redevelopment and develop a vision. Each sub-area has distinctive features of how it was developed in the past, including physical location, existing land use, zoning conditions, building patterns and architecture, the extent of historic resources and degree of development potential. The map below represents the six sub-areas:

Main Core Sub-Areas

1. Byers Station Village
2. Historic Eagle Village (*includes the Eagle Village Historic Area*)
3. Eagle Village South
4. Eagle Village East
5. Eagle Village West
6. Eagle Village Industrial District



Village Concept Plan



Upper Uwchlan Township
Chester County, Pennsylvania

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Data Source: Base data from Chester County Geographic Information Services, 2022.



Map created: June 28, 2022
Last Revised: June 10, 2024

- Township boundary
- Adjacent municipalities
- Village Planning Boundary
- Estimated tax parcels
- Lakes, rivers and streams
- Lakes, rivers and streams
- Public Parkland
- Municipal-owned open space
- Roads
- Byers Station Village
- Historic Eagle Village
- Eagle Village East
- Eagle Village West
- Eagle Village South
- Byers Station Historic District
- Eagle Village Industrial District

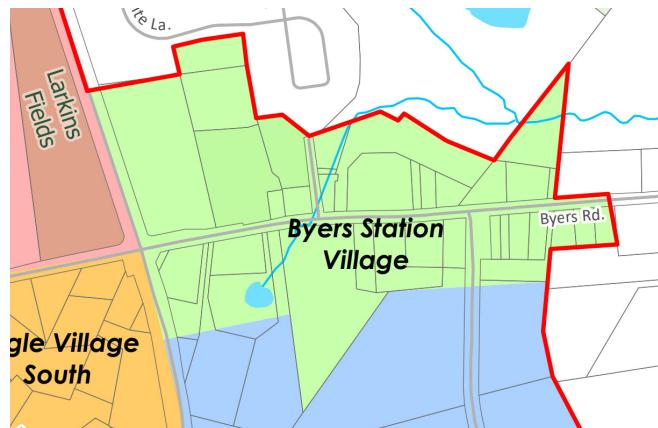
Map 4-1

Byers Station Village

This sub-area is defined predominantly by the officially designated Byers Station Village on Byers Road that is listed on the National Register of Historic Places, and other areas near Byers Road zoned C-1 Village District. There is a low density of buildings and structures within the area, with most classified as historic resources. This sub-area primarily consists of single-family houses and individual medical, professional services, and small-scale retail offices.

The following is envisioned in the Byers Station Village (including the C-1 Village Commercial areas):

1. A small village of single-family and multi-family residences with small-scale office/retail use.
2. Historic resources are preserved, maintained and used to the extent practicable or through adaptive reuse for apartments, offices, services, or retail.
3. Limited light redevelopment and limited infill development, as well as additions or modifications to existing structures, are consistent with and maintain the historic architecture and character of the Byers Station Village (i.e., Victorian façade).
4. Enhanced streetscape along Byers Road, including, but not limited to, street trees, lights, and sidewalks.
5. Signage unique to the Byers Station Village, including interpretive signage.
6. A replica of the now demolished, c. 1870s train station located north of Byers Road, known as Byers Station.
7. Pedestrian-oriented streets encourage walking and bicycle use.
8. Walking connection between Byers Station Village and the Eagle Village.

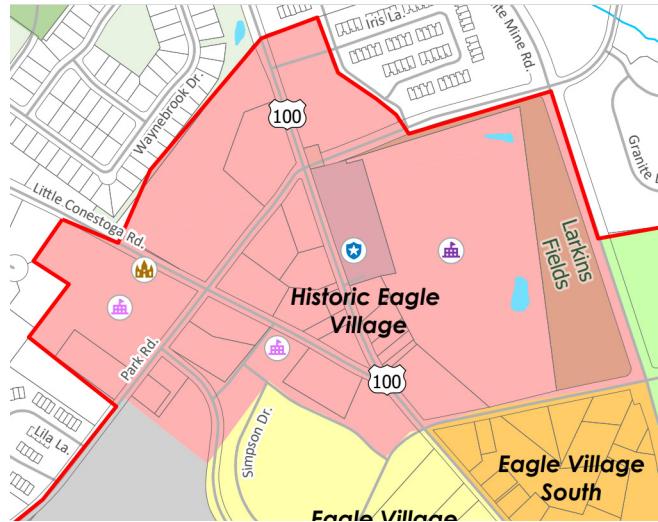


Historic Eagle Village

This sub-area, the core of the Historic Eagle Village, is defined by a cluster of retail uses centered within the boundaries of Park Road, Little Conestoga Road, Byers Road, Pottstown Pike and adjacent areas. There is a high concentration of existing structures in this area that consists of historic resources and contemporary structures. Pedestrian connections have been improved, but sidewalk gaps still exist.

The following is envisioned in the Historic Eagle Village:

1. A walkable village that also encourages bicycle and e-bike use.
2. Historic resources that are preserved, maintained, and used as originally intended or through adaptive reuse.
3. Small-scale businesses that attract and encourage visitors to walk and stay in the Eagle Village, including, but not limited to, restaurants, ice cream parlors, spas, antique stores, hardware stores, bookstores, artists and crafts people.
4. Enhanced streetscape along Pottstown Pike, Little Conestoga Boulevard, Byers Road and Park Road, including street trees, lights, sidewalks, site furniture, signage, banners and seasonal planters.
5. A completed sidewalk connection between the Wawa and Starbucks.
6. Limited light redevelopment and limited light infill development, as well as additions to existing structures, which are consistent with and maintain the historic architecture and character of the Historic Eagle Village.
7. Civic space creation—critical to establishing a village core into which all other sub-areas are connected, becomes a central gathering place where Township activity is centered.
8. Coordinated vehicular access points to limit pedestrian crossings of driveways.
9. Adequate parking at the rear of the buildings and shared or open parking where applicable.

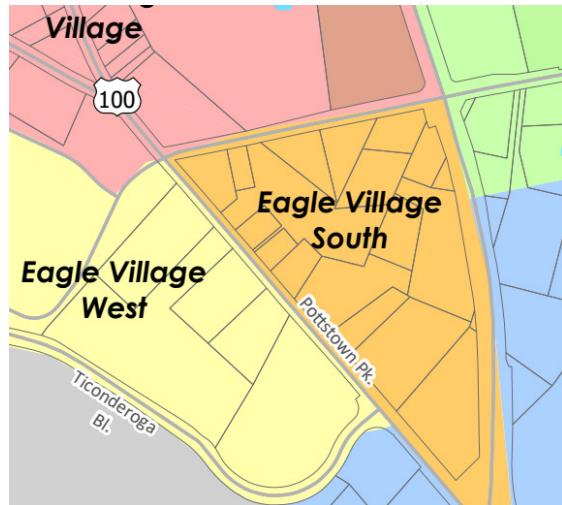


Eagle Village South

This sub-area is bordered by Pottstown Pike to the west, Byers Road to the north and Graphite Mine Road to the east. There is a high concentration of existing structures in this area along Pottstown Pike and Byers Road, consisting of six historic resources and more contemporary structures.

The following is envisioned in the Eagle Village South sub-area:

1. Function as a gateway to the Historic Eagle Village and continuation of the Historic Eagle Village that is walkable and encourages bicycle use, consistent with the view of Pottstown Pike as a small town “Main Street.”
2. Historic resources that are preserved, maintained, and used as originally intended or through adaptive reuse.
3. Small-scale office and retail businesses that attract and encourage visitors to walk and spend time in Historic Eagle Village.
4. Limited light development, redevelopment and infill, as well as additions and modifications to existing structures that are consistent with and maintain the historic architecture and character of the Historic Eagle Village.
5. Larger building footprints potentially along Graphite Mine Road.
6. Controlled access to Graphite Mine Road.
7. Residential uses above commercial/retail; shared parking only.
8. Coordinated access among uses.
9. Parking at the rear of the building.
10. Rear alley for coordinated access.
11. Consistent streetscape treatment along Pottstown Pike, Byers Road, and Graphite Mine Road to strengthen the view of the area as a gateway to and continuation of Historic Eagle Village.

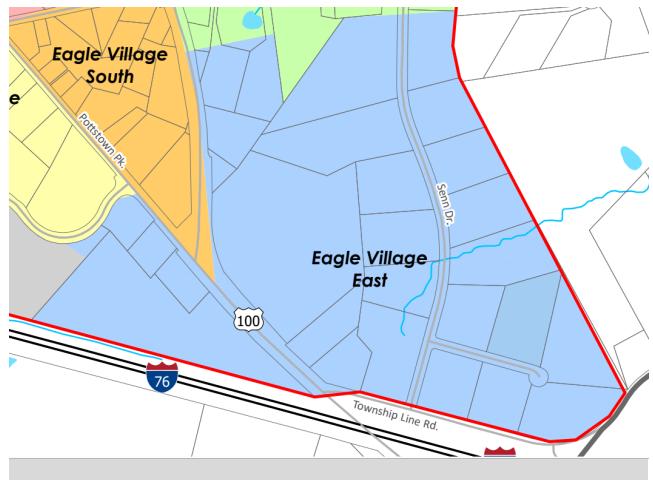


Eagle Village East

This sub-area is defined by the Pennsylvania Turnpike to the south of Ticonderoga Boulevard, north across Pottstown Pike to the east side of Graphite Mine Road. There is a low concentration of existing structures in this area. Two existing structures are historic resources, and the others are more contemporary.

The following is envisioned in the Eagle Village East:

1. Single-story office park or commercial/retail space includes residences.
2. Capitalize on scenic views to the north overlooking the Historic Eagle Village without damaging views, the historic contexts and/or historic resources within the Historic Eagle Village, Byers Station Village and supporting landscapes.
3. Centralized parking plazas or parking structures consistent with and maintain the historic architecture and character of the Historic Eagle Village.
4. Pedestrian-oriented buildings with interconnected sidewalks, public plazas, and pocket parks.
5. Pedestrian connections to Byers Station Village and the Historic Eagle Village shops, restaurants, and services.
6. Landscape buffer along Graphite Mine Road.
7. Landscape screening to buffer Byers Station Village.
8. Coordinated access among uses; rear parking areas; rear alleys.
9. Strong pedestrian orientation around the perimeter and into Eagle Village South and Byers Station Village.
10. Consistent streetscape treatment along Pottstown Pike and Graphite Mine Road.

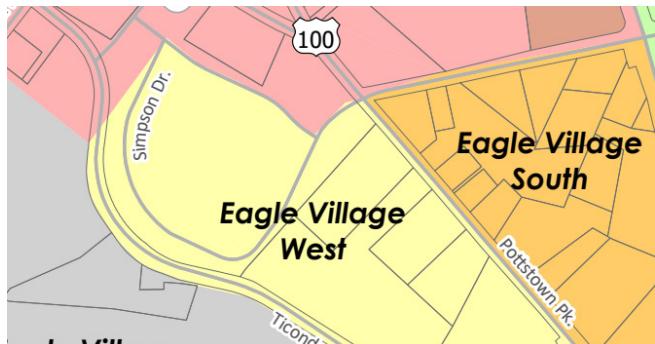


Eagle Village West

This sub-area includes lands east of Ticonderoga Boulevard, south to the Wawa and east to Pottstown Pike. There is a relatively low density of existing structures in this area, and they are primarily contemporary commercial structures with one historic structure, #167.

The following is envisioned in Eagle Village West:

1. The part of this sub-area along Pottstown Pike serves as a gateway to the Historic Eagle Village and a continuation of the Historic Eagle Village, which is walkable and encourages bicycle use.
2. Other parts of this sub-area are walkable areas that encourage bicycle use within easy walking distance to shops, restaurants, groceries, trails, Hickory Park, and Marsh Creek State Park.
3. Limited light infill development, redevelopment and development, as well as additions and modifications to existing structures that are consistent with and maintain the historic architecture and character of the Historic Eagle Village.
4. Small shops, live/work units and retail businesses attract and encourage visitors to walk and spend time in Eagle Village West and Historic Eagle Village.
5. Residential uses above commercial/retail.
6. Civic and open spaces that are accessible from Pottstown Pike.
7. Enhanced streetscape, street trees, lighting, and sidewalks along Pottstown Pike, all consistent with streetscape in Historic Eagle Village.
8. Integrated vehicular and pedestrian access between the Eaglepointe Village shopping center and future infill uses to reduce local traffic on Pottstown Pike and Ticonderoga Boulevard.

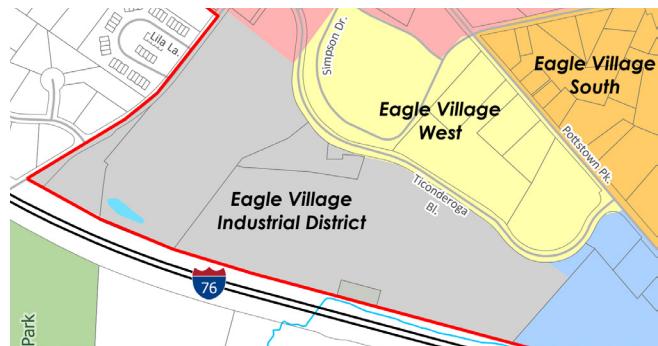


Eagle Village Industrial District

This sub-area north of the PA Turnpike is bordered by Ticonderoga Boulevard to the north.

The following is envisioned in the Eagle Village Industrial District sub-area:

1. To reduce industrial and residential traffic through the Historic Eagle Village, consider through development or redevelopment scenario an extension of Township Line Road north to Park Road (would parallel the Turnpike and provide a second means of access to industrial sites).
2. Two new connector roads between Ticonderoga Boulevard and the Township Line Road extension.
3. Community/active recreation center with multi-use outdoor public space.
4. Some commercial with office buildings, limited light industrial that are compatible and complementarily to existing land uses and character of surrounding sub-areas.
5. High visibility from the Turnpike, the orientation of buildings towards the Turnpike.
6. Landscape screening/buffer between LI District and other Eagle Village sub-areas.





To achieve the visions identified for the Historic Eagle Village and Byers Station Village, the following are primary recommendations:

- Obtain greater historic resource protection within the Village Planning Area by implementing the recommendations within the Historic Resource Protection Plan contained in the Township's Comprehensive Plan (2024).
- Evaluate the adoption of Act 167 Historic District Ordinance (i.e., establishing a HARB) for the Byers Station Village.
- Plan events to showcase the Historic Eagle Village and Byers Station Village's architecture and help promote the Historic Eagle Village by coordinating

with residents, businesses, and service groups, such as the Lions. Additionally, schedule events to attract visitors to the Historic Eagle Village, including but not limited to walking tours, fall and holiday festivals, July 4th parades and artist/craft fairs.

- Evaluate ways to attract more businesses to the Historic Eagle Village that encourages more foot traffic.
- Review the parking ordinance to determine the need to adjust/reduce/increase the Township zoning ordinance's parking requirements in the VPA to encourage pedestrian movement in and around the Village Planning Area to attract visitors to the Historic Eagle Village and the

Byers Station Village. This may include evaluating shared parking opportunities within the Village Planning Area or its immediate surroundings.

- Enhance the architectural design guidelines within the C-1 and C-3 Zoning Districts to address streetscape improvements, pedestrian facilities, civic spaces and other appropriate features.
- Incorporate by reference the Village Design Guidelines in the C-1 and C-3 Zoning Districts and the Township's Subdivision and Land Development Ordinance.
- Review the sign ordinance for its suitability in the C-1 and C-3 Zoning Districts.
- Review the zoning and SLDO lighting ordinance to ensure consistency with the goals of the Village Concept Plan and Village Design Guidelines.
- Review the zoning and SLDO street design ordinance (i.e., benches, trash receptacles) to ensure consistency with the goals of the Village Concept Plan and Village Design Guidelines.
- Review the zoning and SLDO signage ordinance to ensure consistency with the goals of the Village Concept Plan and Village Design Guidelines.
- Evaluate options to install village gateways that enhance sense of place, reduce traffic speeds, and provide landscaping and green stormwater opportunities.
- Along with PennDOT, evaluate the feasibility of redesignating Graphite Mine Road as State Route 100 to divert traffic from the Historic Eagle Village. Consider appropriate traffic studies to better understand existing traffic flows in and adjacent to the VPA. This recommendation must be implemented to make the vision for the Historic Eagle Village possible. Without this change, the vision for the Historic Eagle Village is severely limited.
- Consider adopting the Official Map to add planned (new) roads and trails. Showing these planned facilities does not mean that the Township can take the right-of-way when needed. However, showing them on the Official Map informs landowners of the Township's desired village street network and allows for appropriate Township/developer negotiations at the time of land development application.
- Adopt a Complete Streets ordinance consistent with the Chester County Planning Commission model ordinance, after review and revisions to customize the model to reflect the VPA.
- Consider updating the C-1 and C-3 Districts to provide incentives for applicants to construct residential dwellings above commercial/retail/office space or establish free-standing residential dwellings.
- Consider adding a Village Overlay zoning district that conforms to the C-1 and C-3 District boundaries that could be applied when landowners/developers seek greater use options and development flexibility. Village Design Guidelines are codified in the Village Overlay to reflect village planning objectives.
- Encourage alternative modes of travel to and from the Historic Eagle Village and Byers Station Village by requiring active transportation facilities.
- Intersection improvements at the north and south of the Historic Eagle Village recommended in the Village Transportation Plan should be revisited and updated as necessary.
- Consider opportunities to implement green stormwater infrastructure and improve landscaping to help provide visual separation between the VPA and surrounding landscapes.
- Consider installing solar light structures along trails for increased accessibility and visibility at night.

- Conduct parking study within the VPA to evaluate opportunities for additional parking and/or shared or open parking.

Implementation Strategies

The Village Concept Plan attempts to unify how the Eagle Village Historic Area and Byers Station Village can develop without detracting from or diminishing the area's visual and historic integrity and continue to meet other planning objectives. Supportive participation by both public and private sectors will be essential to realizing many implementation strategies described below.

Village Concept Plan Implementation Task Force

To ensure that recommendations developed in this Plan are being implemented, the development of a dedicated Task Force to oversee the progress is recommended. Said Task Force can be appointed to meet on an as-needed basis to discuss the next steps and identify specific priorities to move forward. In addition, providing feedback on pending subdivision and land developments and any proposals within the VPA can be a dedicated task for that committee.

Official Map

An Official Map is a “land use ordinance” with the map as a primary component of an official map ordinance that provides for planned future public lands and facilities, including streets, trails, parks and open spaces where there is a municipal interest in acquiring lands for public purposes sometime in the future. An Official Map is a legal document adopted in accordance with the Pennsylvania Municipalities Planning Code.

An Official Map can be used to provide for future pedestrian expansions, designate bikeway routes and multi-use trails, reserve land for pocket parks and civic spaces and historical and archaeologically significant areas. Identifying these planned facilities on the map does not mean that the Township can take the right-of-way when needed. However, it informs landowners of the Township's desired village layout and allows for appropriate Township/developer negotiations at the time of land development

application. Landowners should be incentivized to comply with the Official Map so that the Township's infrastructure needs can be satisfied without burdening existing Township residents and businesses. That can be achieved by the following:

1. Adopt an Official Map and ordinance to include planned roads, pedestrian extensions, trails and bike routes and other civic spaces recommended in the Upper Uwchlan Township Comprehensive Plan (2024), Active Transportation Plan (2023), Village Transportation Plan (2015), and other relevant plans.

Village Overlay Zoning District

New zoning tools may be appropriate to help realize the Village's vision, such as creating a Village Overlay zoning district applicable to areas that do not include designated historic buildings or structures which fall under the historic preservation provisions in the Township code. This will allow for an aesthetic transition between historic structures and buildings that are not designated historic by Upper Uwchlan Township.

An overlay is an effective regulatory tool that creates a particular zoning district with unique area and bulk regulations, design guidelines and provisions supportive of the identified vision for the area. The extent of the overlay district can be pre-mapped to a specific geographic area, for example, the Historic Eagle Village and the Byers Station Village or the entire Village Planning Area.

A Village Overlay can forward historic preservation efforts by providing additional provisions of the lot and bulk regulations and additional uses, protecting remaining open space, enhancing provisions for screening, landscaping, and lighting, signage, implementing traffic calming and encouraging consistent signage along the streetscape and throughout the village area. This can be achieved by the following:

1. Designate the Village Overlay District to provide consistency among zoning regulations within the Village Planning Area.

Historic Resources Protection Standards

Obtain greater historic resource protection within the Village by adding appropriate provisions to the zoning ordinance that will provide landowners and developers with incentives for the adaptive reuse of historic buildings and prevent historic resource demolition. Historic resources protection standards can be applied in two ways: through changes to the existing historic preservation regulations in zoning and subdivision and land development provisions (Act 247) and/or by designating a local historic district (Act 167). A historic district is certified by the Pennsylvania Historical and Museum Commission with standards administered by the Township with the assistance and review of a local Historical Architectural Review Board (HARB). While historic preservation regulations in zoning are used to limit alterations and demolition of historic resources, provisions of historic districts allow for the preservation of character and architecture of historic areas for existing structures and new construction. Variations of these standards can be used to ensure the continuous distinctive character of both the Historic Eagle Village and Byers Station Village while allowing for the reuse of historic resources to fit the needs of the existing population. That can be achieved by the following:

1. Establish the designated historic district, for example, the existing Byers Station Village boundary, with specific standards to be administered by the appointed HARB.
2. Strengthen historic resource provisions that apply to all historic resources within the Township, including those in the Village Planning Area.

Zoning and Subdivision and Land Development Ordinance Provisions

The current provisions of the C-1 Village District allow for mixed-use when approved as conditional use. However, there may not be sufficient incentives to encourage new residential units in the village. A balance of residential and non-residential uses will enhance the vitality of the Village, as Village residents will bring activity to the village's businesses, sidewalks, and civic

spaces, enhancing the sense of place and village community. Additionally, the architectural design guidelines are only included in zoning provisions within the C-1 District and the C-3 District (in more limited circumstances), which limits the extent of potential improvements to align with the historic character of the entire Historic Eagle Village and the Byers Station Village.

That can be achieved by the following:

1. Update underlying zoning language within the Village Planning Area to codify the Village Design Guidelines, including streetscape improvements, pedestrian facilities and civic spaces, and provide incentives to use design guidelines.
2. Evaluate the existing street lighting requirements for opportunities for improvements to encourage more walking and use of civic spaces.
3. Consider updating landscape requirements and tree planting regulations to encourage landscape buffers and landscaping of parking areas.
4. Include by reference the Village Design Guidelines in the C-1 and C-3 Zoning Districts and in the Township's Subdivision and Land Development Ordinance.
5. Consider provisions for the installation of electric vehicle charging stations in designated parking areas.
6. Evaluate regulations as they pertain to rooftop solar for historic buildings and structures and adjust accordingly.

Village Design Guidelines

The Village Design Guidelines (Guidelines) have been developed to help property owners and Upper Uwchlan Township preserve the historic character of the Township's Village Planning Area (VPA). The Guidelines are designed for use within the VPA as may be required by Township Ordinances, however, property owners are encouraged to use the Guidelines voluntarily throughout the Township.

The Guidelines are intended to provide proper-

ty owners and Township officials with a “go to” illustrated manual for preserving, rehabilitating, and enlarging historic resources, designing new buildings, and making changes to street-scapes and historic settings within the VPA. Their purpose is to help ensure changes in the VPA will be compatible with and sensitive to the VPA’s historic resources and settings, particularly in the villages of Eagle and Byers. The Village Design Guidelines can be viewed at the Township Offices or on the Township’s website.

Incentives for Landowners and Developers

The Township’s C-1 and C-3 zoning districts include several commercial, institutional, and residential use options for landowners and developers when developing vacant properties, changing an existing use, or redeveloping a site. In addition, the C-1 District includes specific building and site design standards that apply to all proposed uses, and the C-3 District applies those standards for uses subject to conditional use approval.

Sufficient incentives must be offered in the C-1 and C-3 Districts to gain landowner participation in realizing the Township’s vision for the Historic Eagle Village and Byers Station Village. For example, obtaining landowner/developer cooperation in a public street extension or new street construction may require providing additional incentives within the Zoning Code.

Pedestrian and Bicycle Accessibility Improvements

The Village’s success will depend upon a safe network of supporting roads and pedestrian facilities. Using green infrastructure (i.e., street trees, green streets) is important for aesthetics and the comfort of pedestrians, as trees provide shade and visual relief from hard surfaces. Good pedestrian connectivity would include a civic space network to invite public use and informal gathering areas outside shops and businesses. Visitors from nearby residential developments will be encouraged to walk to the village and leave their cars at home. In the future, using structured parking for village use may also be financially feasible. Nevertheless, a well-designed network of sidewalks and civic spaces will be needed, as well as opportunities

to access public transportation and revisions to the Township’s parking requirements.

That can be achieved by the following:

1. Adopt a Complete Streets ordinance consistent with the Chester County Planning Commission model ordinance, after review and revisions to customize the model to reflect the VPA.
2. Complete village streetscape improvement projects to encourage walking, reduce localized traffic, support the Village’s character and feel, and provide additional recreational benefits for the community. Consider installing pedestrian amenities, such as benches, trash and pet waste receptacles, street lighting and additional landscaping along the sidewalks and trails.
3. Consider the location for installing Emergency Call Box systems within the village planning area for safety.
4. Consider various lighting options for pedestrian safety along sidewalks and/or trails.
5. Consider the installation of trail markers and wayfinding signage within the village planning area to guide residents and visitors in the right direction.

Circulation Improvements

It is likely that multi-family residential uses, as well as apartments on upper stories of commercial buildings, could be proposed for the Village in the future. Such development would be consistent with the Village plan, but there would likely be insufficient surface parking to accommodate this build-out scenario. A vibrant village will depend on an appropriate mix of residential, commercial, light industrial, and institutional uses, which may not be easily achieved based on the current parking requirements of the Zoning Ordinance.

The Township is interested in limiting direct lot access to Graphite Mine Road to facilitate traffic flow and divert traffic from the Historic Eagle Village. A goal of the Township is to have Graphite Mine Road formally designated by PennDOT as Pottstown Pike to reduce traffic through the

Historic Eagle Village further. Reduced traffic volumes would allow the former Pottstown Pike to again function as the Historic Eagle Village's traditional "main street."

1. Work with PennDOT to formally redesignate Graphite Mine Road as State Route 100 to divert traffic away from the Historic Eagle Village's "main street."
2. Review the parking ordinance to determine the need to adjust/reduce/increase the Township zoning ordinance's parking requirements in the Village and to encourage greater pedestrian movement in and around the Village.
3. Ensure that the zoning ordinance allows structured parking in select areas.
4. Encourage alternative modes of travel to and from the Village by requiring active transportation facilities.
5. Revisit the need for intersection improvements at the north and south of the Historic Eagle Village recommended in the Village Transportation Plan. Consider village gateway treatments at key village entry points.

Heritage Interpretation Plan

Considering the abundance and variety of historic resources within the Village Planning Area, it is beneficial to provide consistency when interpreting local history and community culture for residents and visitors. The development of the Heritage Interpretation Plan for the Village Planning Area is beneficial to educate residents, attract visitors and encourage walking. The Plan generally includes locations of significant historic and cultural sites, potential improvements (i.e., interpretive signage, public amenities), potential physical connections to other sites and points of interest and steps to develop and implement a walking/biking tour within the area.

Environmental Resilience Planning Initiatives

Climate resilience is the ability to adapt to and withstand the impacts of climate change. For a

successful future into 2035 and beyond, the Village Planning Area recommendations consider clean energy initiatives and implementation of green infrastructure. Regarding redevelopment in the Villages, the recommendations suggest implementing alternate energy solutions for new buildings or additions to include solar or LED lighting. This can be expanded to the Village Planning Area sidewalks and crosswalks with lighting, stoplights and charging stations within parking lots for electric vehicles. As this technology continues to develop, climate-related recommendations could expand, requiring further action from the Township. These climate resilient recommendations can be achieved by the following.

1. Complete a Local Climate Action Plan (LCAP) to inventory greenhouse gas emissions while assessing response to climate-related events or vulnerabilities.
2. Promote alternative energy solutions in new and redevelopment projects (i.e., solar panels).
3. Incentives for businesses/landlords to install solar.
4. Installation of EV charging stations.
5. Implementation of Green Stormwater Infrastructure (GSI) practices throughout the Villages.
6. Installation of solar light structures along trails for increased accessibility and visibility at night.
7. Installation of recycling stations within the Village Districts.

Chapter 6 Reference Documents



Reference Documents

Chester County Comprehensive Plan,
Landscapes3 (2018)

Chester County Planning Commission

Rural Center Landscape Design Guide
(2022)

Chester County Planning Commission

Village Planning Handbook (1993)

Chester County Planning Commission

Smart Growth Online

Smart Growth Network

Upper Uwchlan Township
Comprehensive Plan (2024)

Upper Uwchlan Township

Village Design Guidelines

Upper Uwchlan Township

Village of Eagle and Byers Station
Concept Plan (2010)

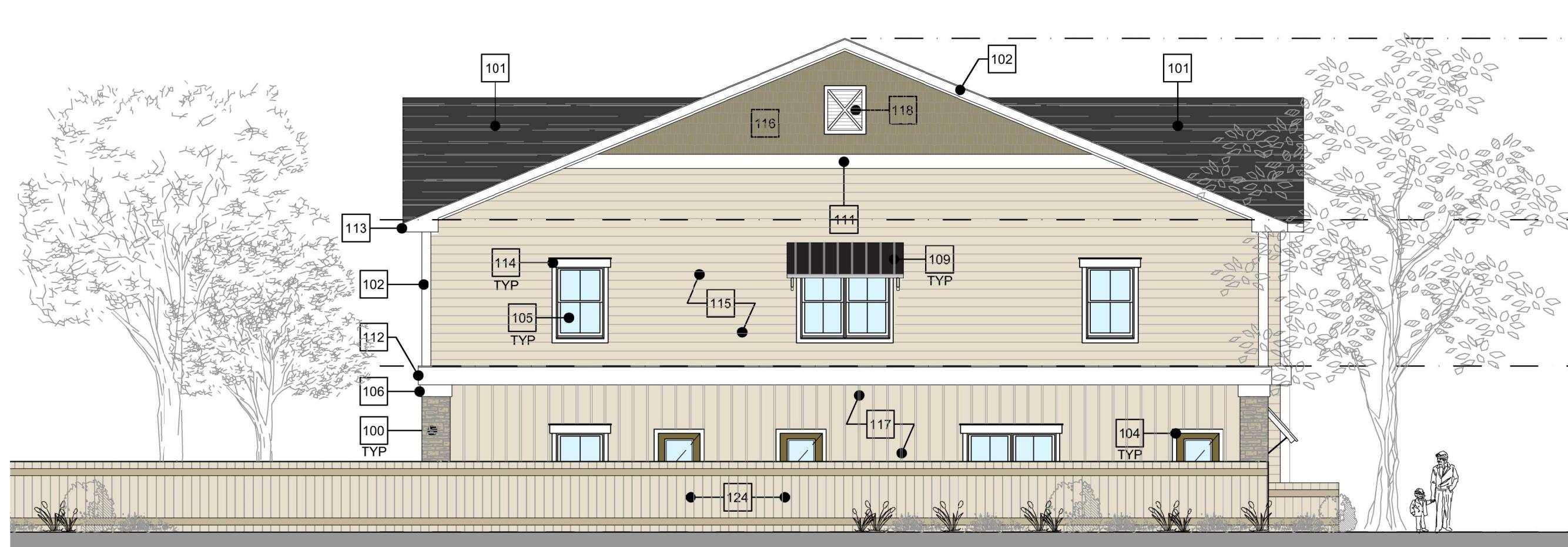
Upper Uwchlan Township

Upper Uwchlan Zoning Ordinance



1 Elevation

SCALE: $\frac{1}{8}$ " = 1'-0"



4 Elevation

SCALE: $\frac{1}{8}$ " = 1'-0"



2 Elevation

SCALE: $\frac{1}{8}$ " = 1'-0"

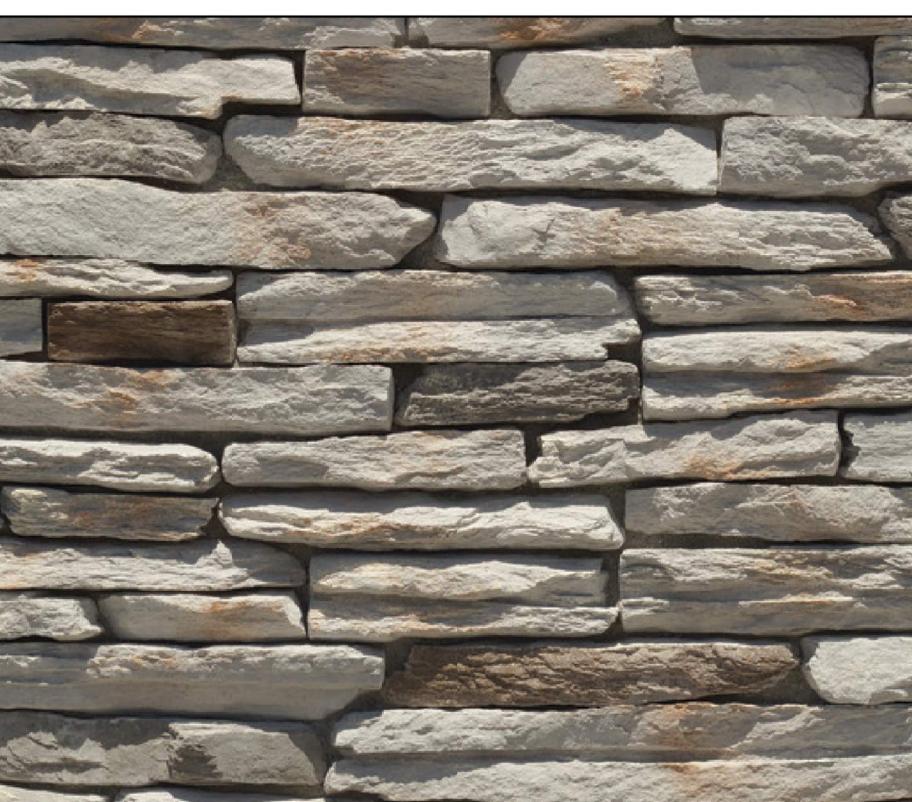


3 Elevation

SCALE: $\frac{1}{8}$ " = 1'-0"

EXTERIOR FINISH MATERIALS SCHEDULE				
TAG	MATERIAL	MANUFACT'R	COLOR / NO.	NOTES
100	THIN STONE	STONE GRAFT	LAUREL CAVERN LEDGE ASHER'	DRYSTACK
101	PRE-ENG ROOF TRUSS W/ ASPHALT SHINGLES	GAF TIMBERLINE HDZ	CHARCOAL	-
102	8" AZEK RAKE BOARD & CORNER TRIM	AZEK	WHITE	SMOOTH
103	PRECAST SILL	T.B.D.	BROWN	
104	EXTERIOR DOOR	AS SCHEDULED	TO MATCH ADJACENT	AS SCHEDULED
105	VINYL DOUBLE HUNG WINDOW SYSTEM	ANDERSEN	WHITE 400 SERIES	REFER TO DETAILS IN THIS DRAWING SET
106	PRECAST / PRE-MFR. BASE OR CAP		NATURAL	MORTAR COLOR TO MATCH BASE OR CAP
107	ALUM. DOWNSPOUT	SELECTED BY OWNER	MATCH CORNICE	PVC BOOT TO STORM WATER SYSTEM
108	ALUM. CONT. GUTTER	SELECTED BY OWNER	MATCH CORNICE	-
109	PRE-MANUFACTURED ALUM. METAL AWNING		BLACK	AS APPROVED BY OWNER. PROVIDE SUPPORT BRACKETS
110	PRE-MANUFACTURED ZEE BOX	ATAS INTERNL, INC.	BLACK	INSTALL PER MFR'S DETAILS AND INSTRUCTIONS MANUAL
111	12" AZEK BAND TRIM	AZEK	WHITE	SMOOTH
112	16" AZEK BAND TRIM	AZEK	WHITE	SMOOTH
113	10" AZEK FASCIA TRIM	AZEK	WHITE	SMOOTH
114	4" AZEK JAMB / SILL TRIM 8" AZEK HEAD TRIM	AZEK	WHITE	SMOOTH
115	HORIZONTAL SIDING	CERTAINTEED 'CERTAPLANK'	HERRINGBONE	SINGLE 7" LAP SIDING
116	STRAIGHT-CUT SIDING	CERTAINTEED CEDAR IMPRESSIONS 'PERFECTION'	NATURAL CLAY	'STRAIGHT EDGE' DOUBLE 7"
117	BOARD & BATTEN SIDING	CERTAINTEED	HERRINGBONE	8" VERTICAL SIDING PANELS
118	DECORATIVE VINYL VENT	CERTAINTEED	WHITE	-
119	FRT PLYWOOD AT SIGNAGE		-	PROVIDE BEHIND EXTERIOR SIGNAGE
120	SIGNAGE		-	BY OWNER / TENANT
121	BEAMS & BEAM END TRIM	AZEK	WHITE	SMOOTH
122	FIBERGLASS COLUMN SURROUND (TAPERED)	BUILDER'S STANDARD	WHITE	PROVIDE MATCHING BASES & CAPITALS (SMOOTH)
123	STANDING SEAM METAL ROOF	ATAS	BLACK	PREFINISHED
124	6" VINYL FENCE PRIVACY PLANK BOARDS	CERTAINTEED	SAND (TAN)	HEIGHT AS DETERMINED BY OWNER / TENANT
125	GOOSE NECK LIGHTING	T.B.D.	BLACK	BY OWNER / TENANT
126	SCONCE LIGHTING	T.B.D.	BLACK	BY OWNER / TENANT
127	DECORATIVE VINYL GABLE TRIM	T.B.D.	WHITE	BY OWNER / TENANT

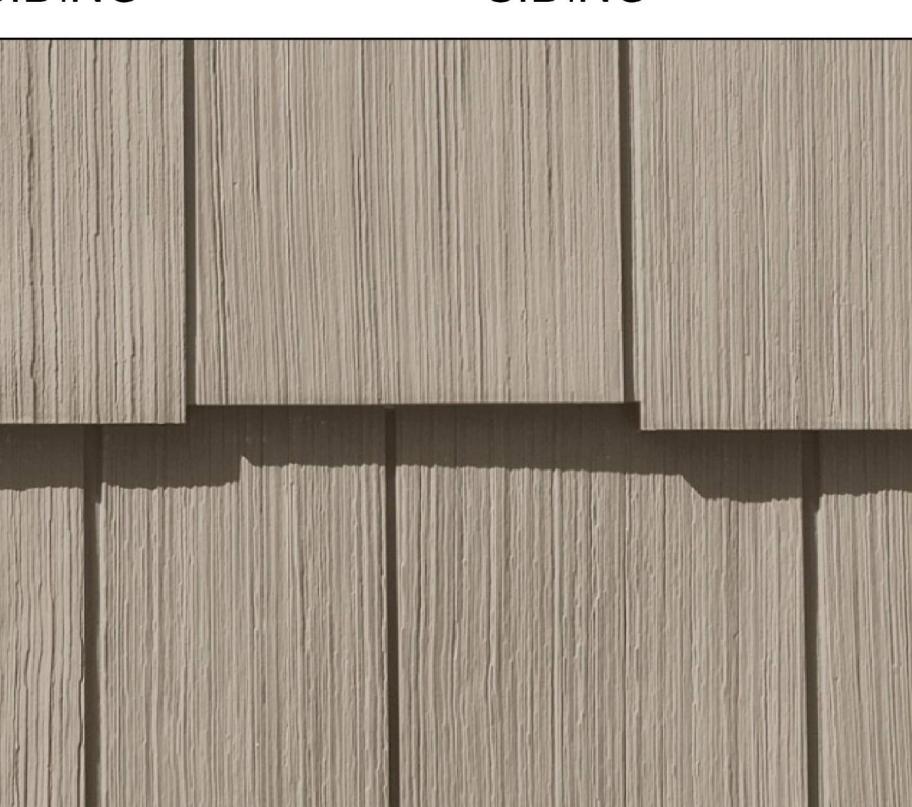
NOTE: ALL NEW CMU ON THIS PROJECT SHALL BE 'DRY BLOCK' TYPE CMU & ALL MORTAR TO HAVE WATERPROOFING ADDITIVE AGENTS.



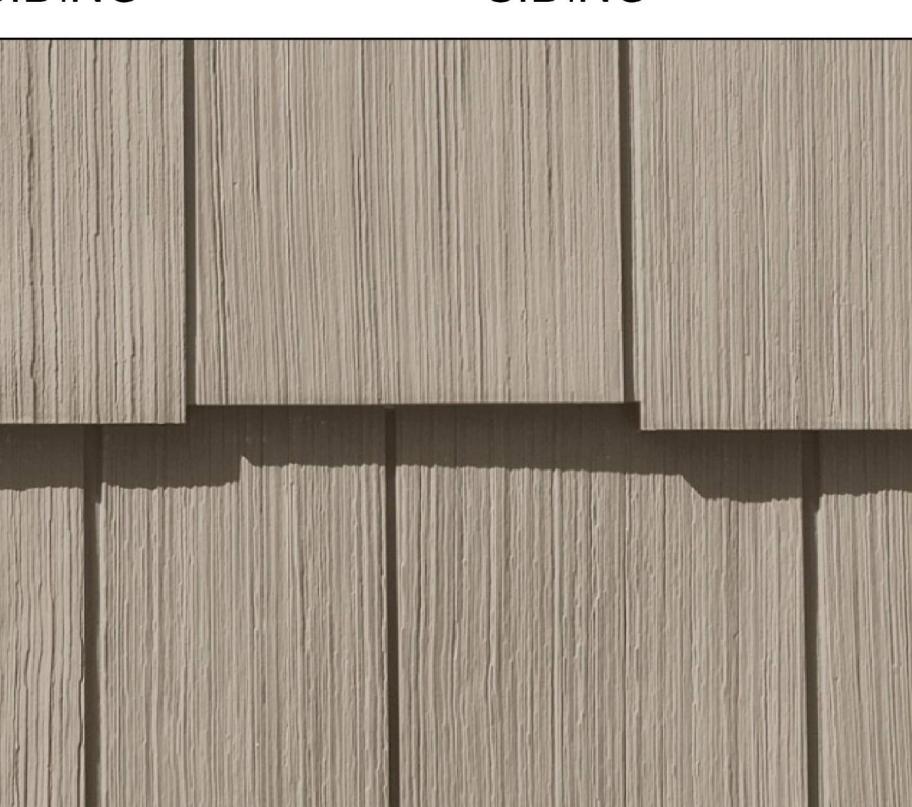
THIN STONE



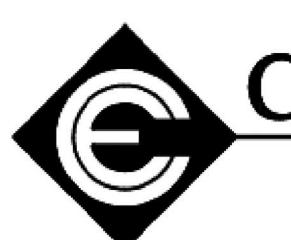
HORIZONTAL
SIDING

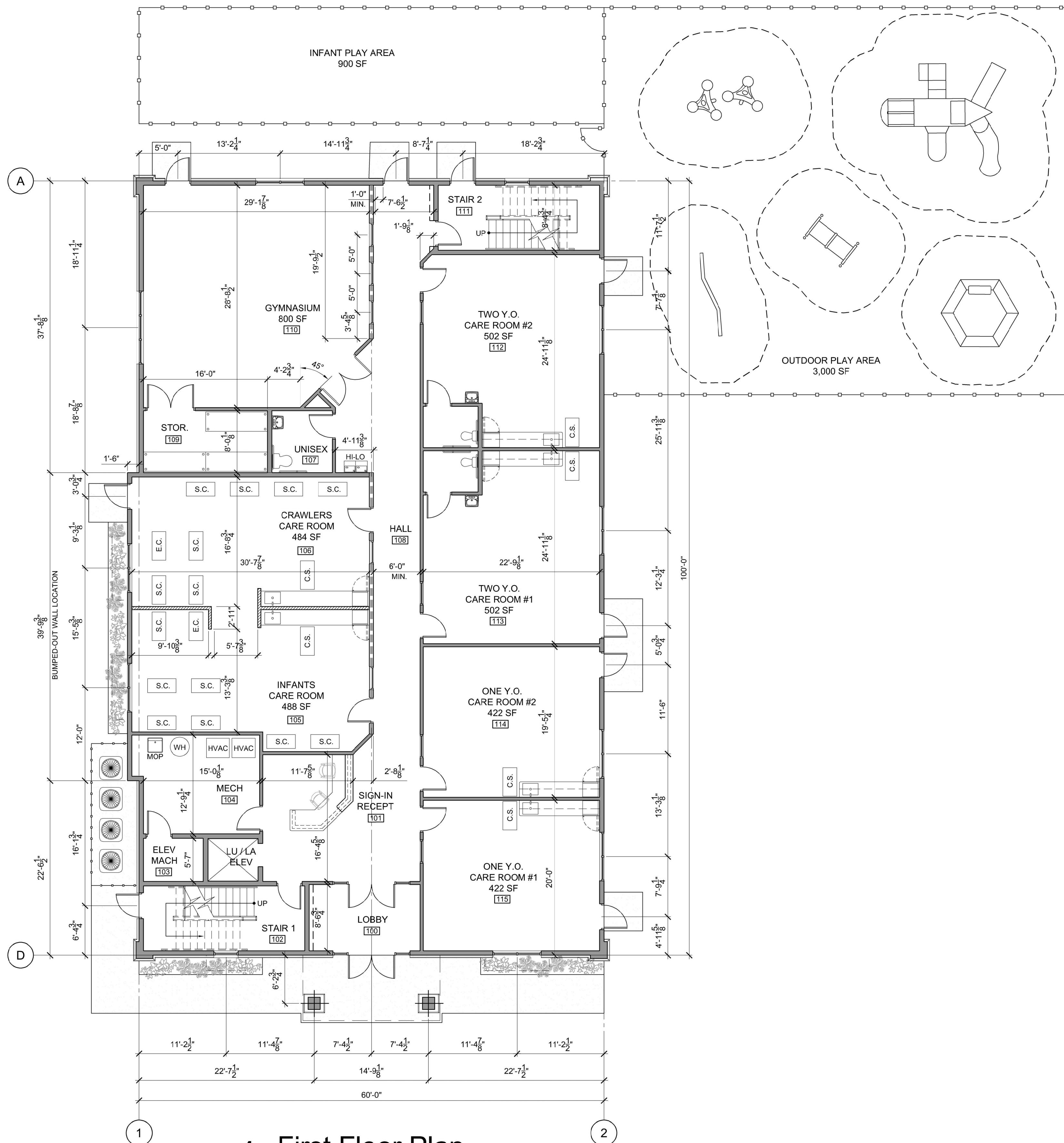


BOARD & BATTEN
SIDING



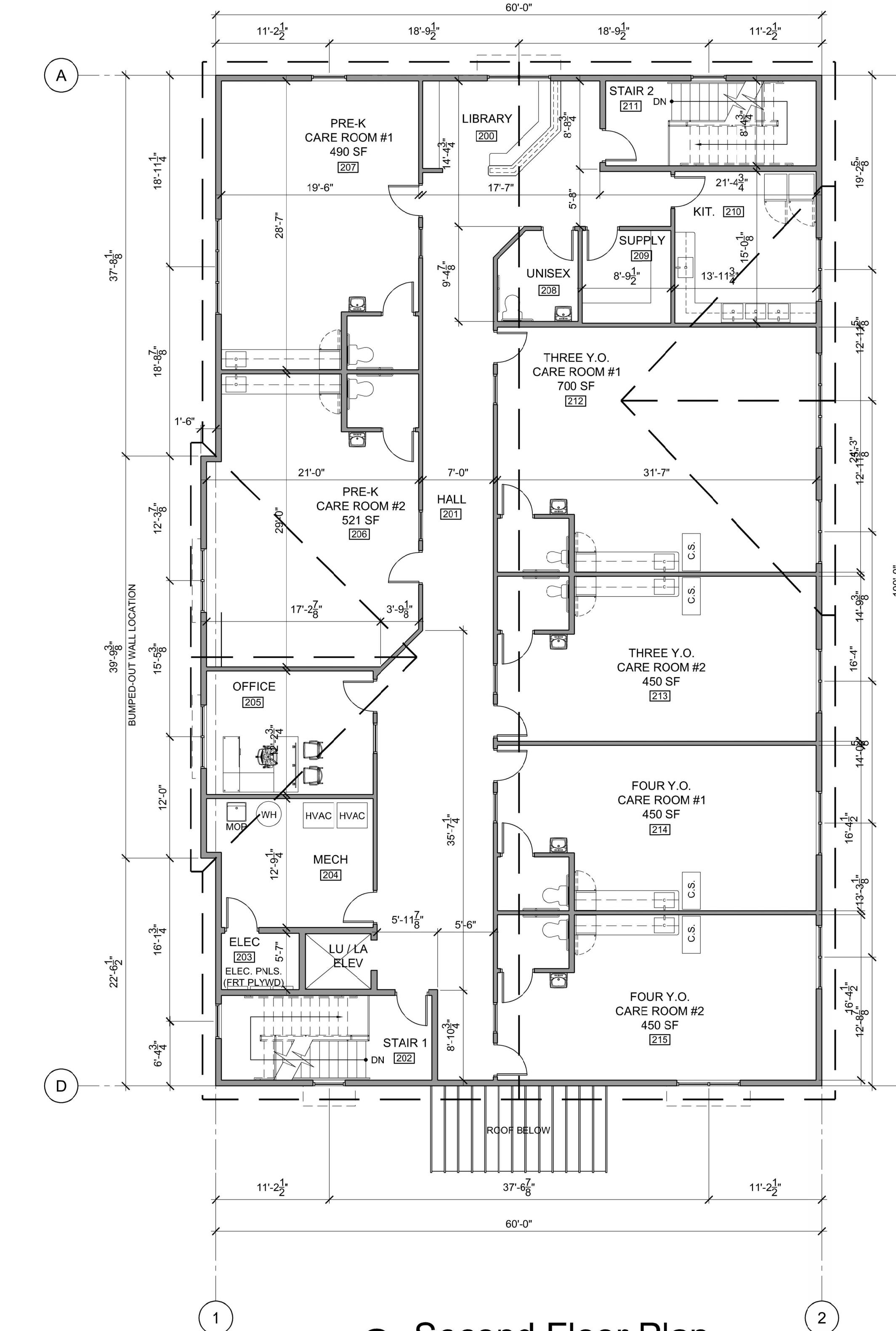
STRAIGHT-CUT SIDING





1 First Floor Plan

SCALE: $\frac{1}{8}$ " = 1'-0"



2 Second Floor Plan

SCALE: $\frac{1}{8}$ " = 1'-0"



COMMONWEALTH ENGINEERS, INC.

114 EAST LANCASTER AVENUE ~ SECOND FLOOR
DOWNTOWN, PA 19335
TELEPHONE: (610) 518-9003 ~ FAX: (610) 518-9004

LETTER OF TRANSMITTAL

To: **Upper Uwchlan Township**

140 Pottstown Pike

Chester Springs, PA 19425

Phone: 610 458 9400

Via Email & UPS

Date: **07/26/24** Job No.: **202317**

Attention:

241 Park Road (Daycare)
Preliminary/Final Land Development Plans

COPIES	DATE	NO.	DESCRIPTION
4	07/26/24	9	Response Letter to Gilmore & Associates, Inc. Review Letter of 7/3/24
4	07/03/24	1	Lot Consolidation Legal Description
4	07/26/24	10	Conditional Use Impact Statement
4	07/26/24	16	Plans (11" x 17")
9	07/26/24	16	Preliminary/Final Land Development Plan (Full Size) Highlighted for Township Engineer (1)

FOR REVIEW/APPROVAL

AS REQUESTED

If you have any questions, please don't hesitate to contact me.

Thank you,

SIGNED:

Michael Cowen, EIT

If enclosures are not as noted, kindly notify Commonwealth Engineers, Inc. at once



COMMONWEALTH ENGINEERS, INC.

114 East Lancaster Avenue ~ Second Floor ~ Downingtown, PA 19335
Telephone: (610) 518-9003 ~ Fax: (610) 518-9004 ~ comeng@cei-1.com

July 26, 2024

Mr. Tony Scheivert
Township Manager
Upper Uwchlan Township
140 Pottstown Pike
Chester Spring, PA 19425

Re: 241 Park Road (Daycare Facility)
Preliminary Land Development

Dear Mr. Scheivert:

The following revisions are offered in response to the review letter dated July 3, 2024, issued by David N. Leh, P.E., Vice President of Gilmore & Associates, Inc., Township Engineer, for the above-referenced plan submission. Township Engineer review comments are shown in *italics*; new Township Engineer review comments are shown in ***bold italics***. Commonwealth Engineers, Inc., responses shown in **bold**.

II. TOWNSHIP ENGINEER COMMENTS-CONDITIONAL USE DECISION & ORDER

1. *Condition No. 10 required evaluation of the adequacy of the structural wall along Ticonderoga Blvd.; we note a boulder retaining wall is now proposed. Additional information should be provided regarding the size of the boulders. Further, protective barriers in the parking lot near the outdoor plan area were to be installed; none are shown on the plans. This shall be addressed.*
 1. Two bollards have been added to the plans to protect the play area from vehicles using the parking area. (See Sheet 2 of 16)
 2. *The detail for the proposed Privacy Fence should be provided to the Historical Commission for review to satisfy Condition No. 13.*
 2. Color Architectural renderings have been sent to the township for distribution.
 3. *Architectural Plans should be provided which demonstrate Condition No. 15 will be satisfied.*
 3. Color Architectural renderings have been sent to the township for distribution.

III. ZONING ORDINANCE-GILMORE & ASSOCIATES

1. *(CU) §200-33.B.(3) – A daycare center is permitted as a principal use when authorized by conditional use by the Board of Supervisors. The applicant has received conditional use approval. The Conditional Use Approval provided on the plans shall be revised to include the Order and the Conditions of Approval (verbatim).*

PROFESSIONAL CIVIL ENGINEERING



COMMONWEALTH ENGINEERS, INC.

TO: Mr. Tony Scheivert, Upper Uwchlan Township
RE: 241 Park Road (Daycare)
DATE: July 26, 2024
PAGE: 2

1. The Conditional Use Order and Conditions have been added to the plans. (See Sheet 2 of 16)
2. §200-36.B.(1)(d) – All facilities and equipment for heating/air conditioning, trash collection and compaction, and other structural elements not in keeping with historical architectural themes shall be concealed architecturally or otherwise screened from view from any public right-of-way or public space. The applicant shall clarify where HVAC equipment is located to determine if screening is required.

HVAC equipment is now shown on the plan with a 4-foot-high privacy fence and seven (7) deciduous shrubs. Plans should be revised to include a mix of evergreens to screen the mechanical equipment so that it is not visible from Park Road, to meet the conditions of the approval and to demonstrate compliance with the ordinance.

2. The location of the HVAC units has been added to the plans. A 4-foot-high privacy fence and landscape screening is proposed around the units. The Plans and Privacy Fence Detail have been revised. All privacy fencing is to be 4 feet high. (See Sheets 2 and 8 of 16)
3. §200-73.B.(2), and §200-74. – Please indicate where delivery vehicles are intended to park. The response letter states that delivery trucks are limited to local carriers who will utilize the proposed parking spaces. We would recommend one parking space be designated as a “Delivery Space.”

The response letter states that delivery trucks (to will be standard size delivery vans) will be able to use the child loading and unloading spaces that have been added to the plans. Confirm whether the spaces indicated with the “L” are for child drop-off/pick-up.

3. Delivery trucks will be able to use the child loading and unloading spaces that have been added to the plans, as they will be standard size delivery vans. The parking spaces marked with an “L” are intended to be for child pickup/drop-off only. This designation has been added to the Legend. (See Sheet 2 of 16)

IV. SUBDIVISION & LAND DEVELOPMENT ORDINANCE – GILMORE & ASSOCIATES

1. (RW) §162-7.B.(2) – A waiver is being requested to allow preliminary and final plan submission. Due to the scope of the project, we have no objection to this request.
 1. Noted.
2. §162-8.F – The applicant shall execute a land development agreement, to be approved by the Township, pending the review of the Township Solicitor, before the final plan is recorded.
2. Pending, Township has provided us with templates of the agreements.
3. §162-8.G – The applicant shall deposit, with the Township, financial security in an amount sufficient to cover the cost of all improvements. A cost estimate, signed and sealed by a Professional Engineer, shall be provided for review.



COMMONWEALTH ENGINEERS, INC.

TO: Mr. Tony Scheivert, Upper Uwchlan Township
RE: 241 Park Road (Daycare)
DATE: July 26, 2024
PAGE: 3

3. Noted. Prior to plan recording a cost estimate signed and sealed by a Professional Engineer shall be provided to the Township Engineer for review and approval.
4. §162-9.B.(1)(b) – Provide a legal description for the consolidated lot.

A legal description has been provided. The distance from the POB to the first course should be verified and labeled on the plans. Course #2 distance, along FedEx lands, does not match the plans; the existing right-of-way distance along this line should also be labeled on the plans. The courses along Park Road are not labeled on Sheet 2, and the POB is incorrectly identified in Course #2. Finally, the tract boundary shown on the plans (i.e., dark outline) does not match the consolidated legal description and shall be revised once dedication of the right-of-way has been determined.

4. The closure report has been added to the plans (See Sheet 1 of 16) and the legal description is attached herewith.
5. (RW) §162-9.H.(1)(b) and §162-9.H.(5). – We defer to the Township's Historical Commission whether an Historic Impact Statement is required. A waiver from the Historic Impact Statement is being requested.
5. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.
6. §162-9.H.(4). – All of our previous comments regarding the fiscal impact statement have been adequately addressed in the engineers response letter. However, the impact statement itself should be updated with this information and submitted.

The response letter states the impact statement has been updated with the fiscal impact statement previously submitted to the township. However, no updated statement was submitted. Please provide.

6. The impact statement has been updated with the fiscal impact statement previously submitted to the township.
7. §162-47.a(4) – Provide concrete monuments at changes of direction of the ultimate right-of-way as well as at intersections with property lines.

7. Concrete monuments have been added to the plans along the ultimate right-of-way. (See Sheet 1 of 16)
8. (RW) §162-55.B.(7) Replacement trees are required for each tree greater than six inches dbh removed. Based on the size and number of trees proposed to be removed, either four (4) replacement trees (at 4-6.5" caliper) or twenty-three (23) replacement trees (at 3-3.5" caliper) are required to be planted and are in addition to other required landscaping. The applicant is requesting a waiver from this requirement.
8. Upon discussion with Adrienne Blank and per John Hosbach's review of the existing trees on site, it was determined that only one existing tree that will be removed is a viable tree for replacement requirements. This tree would require four (4), 3-3.5-inch caliper replacement trees be planted. A



COMMONWEALTH ENGINEERS, INC.

TO: Mr. Tony Scheivert, Upper Uwchlan Township
RE: 241 Park Road (Daycare)
DATE: July 26, 2024
PAGE: 4

waiver is being requested from providing the required four (4) replacement trees. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.

9. (RW) §162-57.C.(7). - *The interior of each parking area shall have at least one shade tree for every five parking spaces. Based on 39 proposed spaces eight (8) shade trees are required but only four (4) are proposed. A waiver has been requested from this requirement.*
9. Based on 39 proposed parking spaces, eight (8) shade trees are required to be planted. A total of 5 parking lot trees are being proposed. A waiver to allow five (5) shade trees be planted instead of the required eight (8) shade trees. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.
10. (RW) §162-57.D.(1) - *Per 1,000 SF of gross building area two (2) deciduous trees, one (1) evergreen tree and eight (8) shrubs are required to be planted and are in addition to any required replacement plantings due to woodland disturbance. Based on the 6,000 SF proposed building 12 deciduous trees, 6 evergreen trees and 48 shrubs are required. The applicant is requesting a waiver from providing 12 deciduous trees and 6 evergreen trees. (48 shrubs are provided.)*
10. Based on the 6,000 SF proposed building, 12 deciduous trees, 6 evergreen trees and 48 shrubs are required. Four (4) deciduous/ornamental trees are proposed. A waiver to allow four (4) deciduous/ornamental trees to be planted instead of the required number of trees. The 48 required shrubs are being provided. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.
11. (RW) §162-57.D.(1) - *Per 100 linear feet of existing tract boundary where not coincident with existing road frontage, 1 deciduous tree, 2 evergreen trees, and 8 shrubs are required. Based on the eastern and southern property lines linear footage, 4 deciduous trees, 8 evergreen trees and 32 shrubs are required. The applicant is requesting a partial waiver from providing 3 evergreen trees (5 are provided) and 2 shrubs (30 are provided). The applicant is requesting a waiver from this requirement.*
11. Four (4) deciduous trees, eight (8) evergreen trees and thirty-two (32) shrubs are required. Six (6) evergreen trees are proposed to be planted. A waiver to allow (6) evergreen trees and Thirty (30) shrubs to be planted instead of the required number of plantings. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.
12. §162-58.C(4)(b) - *Plans shall clarify that the Base Detail provided on Sheet 11 of 15 does not apply to the historic fixtures along Park Road. Additional detail/information shall be provided for those pole bases.*
12. A note has been added to the plans indicating that the pole base on Sheet 10 of 16 (LP-2) is for parking lights only.
13. §162-58.D(1)(a) - *Site plan shall show all lighting including building and architectural lighting. The plan does not indicate any additional lighting beyond the proposed area lights. The applicant shall verify that no additional architectural lighting is proposed.*



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Architectural lighting appears to be proposed in the form of wall mounted gooseneck fixtures as well as sconce fixtures. Cut sheets and information regarding architectural lighting shall be included in the overall site lighting plan.

13. Gooseneck light cutsheet has been added to the plans (See Sheet 12 of 16 (LP-4)).
14. §162-58.D(1)(c) – Plan shall include fixtures and pole cut sheets. Fixture cut sheets shall include architectural fixtures.
14. The fixture and pole cut sheets have been added to the plans. (See LP-3 Sheet 11 of 16)
15. §162-58.D(3)(o) – Wall mount fixtures shall not be used to provide area lighting unless it can be demonstrated that pole mounted lights are not possible. A majority of the site lighting appears to be accomplished by utilizing seven (7) wall mounted fixtures mounted at an unknown mounting height. Pole mounted fixtures shall be utilized.

The number of wall mounted lights has increased to 9 fixtures. We question the need for wall mounted site light fixtures at doorways which appear to be only utilized for emergency egress. If the doorways are only for emergency egress, they would only need to comply with code requirements for emergency egress and not site lighting requirements. Applicant shall clarify if outdoor areas surrounding the building are anticipated to be used after dark. It appears that lighting of the walkway and parking areas may be able to be accomplished utilizing pole mounted fixtures rather than the proposed wall mounted fixtures.

15. As discussed with the township engineer wall mount lights on the north and west sides of the building have been removed. Wall mount lights have only been proposed on the parking lot side of the building. (See LP-3 Sheet 11 of 16)

V. STORMWATER MANAGEMENT ORDINANCE-GILMORE & ASSOCIATES

1. §152-110. – The applicant shall post financial security to the municipality for the timely installation and proper construction of all stormwater management facilities as required by the approved SWM site plan and this chapter.
1. Pending.
2. §152-306.J(3) – Setbacks from property lines and rights-of-way for all infiltration practices are to be consistent with accessory structures in the Zoning Ordinance, for the applicable zoning district. Note that the Zoning Ordinance states that no accessory structure shall be located within the front yard, nor within 10-feet of any rear or side lot line. SB-2 is located within the front yard setback from Ticonderoga Blvd. The applicant is requesting a waiver from this requirement.
2. The Planning Commission at the July 11, 2024 meeting has recommended the waiver.
3. §152-402.F.(2) and §152-701.D.(1), (2) and F(1) and (2), and §152-703.A. – An Operation and Maintenance Agreement shall be prepared and submitted to the municipality for review and approval. The Stormwater Management Plans and the agreement shall be recorded.



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3. Pending, we have been provided templates by the Township.

VI. GENERAL COMMENTS-GILMORE & ASSOCIATES

1. A preliminary design shall be provided for the retaining walls to verify that any required tie-backs will not conflict with the adjacent FedEx property.

We believe, at a minimum, a temporary construction easement will be required for construction of the proposed wall along the southeast property line as there is only approximately 4' between the back of curb and property line.

The Boulder Landscape Wall Detail (Sheet 8 of 15) should be revised to include approximate size and type of boulders proposed.

There are areas where the wall height appears it may exceed 4'. It shall be confirmed this is not the case or in the contrary, additional design information should be provided.

1. A detail for the rock landscape walls has been added to the plans. (See Sheet 8 of 16)
2. Existing features to be removed (TBR) should not be shown on any proposed conditions plans. Currently existing features TBR are shown on most proposed conditions plan sheets. *The existing signs shown in the entrance drive and in curbing (on Park Road) should be shown as to be relocated on the existing conditions plan and the relocated signs shown on all proposed plans.*
2. The existing sign to be relocated has been labeled on the Existing Conditions Plan and removed from any proposed conditions plans. (See Sheet 3 of 16)
3. Two (2) SL3 light fixture locations appear to conflict with the proposed retaining wall. Locations shall be coordinated. *The designer has indicated that there is adequate space. Please provide a detail as to how this will be constructed.*
3. As discussed with Chris Green a note has been added to the plans stating that rock landscape wall will be adjusted, where necessary, to avoid conflict with light pole base(s). (See LP-3 Sheet 11 of 16)
4. All signs to be relocated should be labeled on the Existing Conditions Plan, and their existing locations not shown on any proposed conditions plans.
4. All signs to be relocated have been labeled on the Existing Conditions Plan and removed from any proposed conditions plans. (See Sheet 3 of 16)
5. Remove the sight distance lines from all proposed conditions plans (except Sheet 14); remove the ISO candle values from the Landscaping Plan (Sheet 0); show the "Limit of Abacus Surveying Boundary" on the existing conditions plan only.



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5. The sight distance lines have been removed from all proposed conditions plans except for Sheet 15 of 16
6. *Verify the width of vertical slates in the Vinyl Privacy Fence Detail on Sheet 8 (Notes #5 width 72 inches vs. 10 ft on detail).*
6. Note #5 has been removed from the detail. The 10-foot dimension is for the distance between fence posts.
7. *Mounting Height for site lighting fixtures is noted as 16'; however, the pole selected is 15' and the poles will be mounted to a 30" high base. Clarification shall be provided.*
7. A note has been added to the plans stating that the light pole shall be cut to provide mounting height of 16' inclusive of 30" pole base height. (See LP-2, Sheet 10 of 16)

VII. TOWNSHIP TRAFFIC CONSULTANT COMMENTS-BOWMAN

1. *Conditional Use Decision and Order Condition 7 – If on-site parking causes traffic to back-up into Park Road, then the applicant must implement revised pick-up/drop-off procedures with staggered, assigned times, or other modifications, to mitigate the traffic congestion. These measures must be approved by the Board of Supervisors and our office.*
 1. No response required.
2. *Conditional Use Decision and Order Condition 8 – The following comments pertain to the proposed curb ramp designs at the intersection of Ticonderoga Boulevard and Park Road shown on Sheet 4:*
 - a. *The curb ramp grading plans should be revised so the depressed portion of the curb ramp adjacent to the roadway is flush with the adjacent road pavement for both curb ramps.*
 - a. The curb ramps have been revised and the depressed portion of the curb ramp adjacent to the roadway is now flush with the adjacent road pavement. (See Sheet 4 of 16)
 - b. *Typically, two separate curb ramps should be provided on the southeast corner of the Park Road/Ticonderoga Blvd. intersection (one for the pedestrian crossing of Park Road and one for the pedestrian crossing of Ticonderoga Blvd.). However, in this situation, this is a challenge, as this would impact the location of the decorative crosswalks on the south and east legs of the intersection. Therefore, in order to allow a single curb ramp, the ramp should be revised as a Type 2 ramp, and spot elevations should be provided in the pavement area adjacent to the curb ramp to show a level turning area is provided within the existing crosswalk area.*
 - b. The curb ramps have been revised. (See Sheet 4 of 16)



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- c. *The plans appear to show the same elevation for the top of curb and bottom of curb elevations at the top of the southern rolled flare at the northeast Ticonderoga Blvd. curb ramp.*
- c. The proposed spot elevation in this location as been revised. (See Sheet 4 of 16)
- 3. *Conditional Use Decision and Order Condition 9 – Satisfied. The plans show sidewalk along Park Road, which is consistent with the Conditional Use Plan.*
- 3. Noted.
- 4. *Conditional Use Decision and Order Condition 11 – Partially Satisfied. The plans include the proposed roadway improvements, including widening the northbound Park Road travel lane, and grading improvements to the south of the access to provide adequate sight distance. However, please note the following:*
 - a. *Provide half-width mill and overlay of Park Road along the site frontage to the double yellow centerline. This should be reflected on the Park Road widening detail on Sheet 4.*
 - a. As discussed with the township engineer a half-width mill and overlay will not be required. The Park Road Widening detail has been revising to specify a two foot from edge of pavement mill and overlay. (See Sheet 4 of 16)
 - b. *The Park Road widening Detail provided on Sheet 4 should be revised to show new, full depth pavement replacement within the full width of the two-foot saw cut area.*
 - b. As discussed with the township engineer a half-width mill and overlay will not be required. The Park Road Widening detail has been revising to specify a two foot from edge of pavement mill and overlay. (See Sheet 4 of 16)
 - 5. *ZO §200-75.H(3) – The sight line for left-turn entering vehicles looking ahead should be revised to extend to a point 324 feet west of the centerline of the proposed driveway in the center of the eastbound Park Road travel lane.*
 - 5. The sight line for left-turn entering vehicles looking ahead has been revised and is now measured from a point in the south bound lane, 35-feet east of the centerline of the proposed driveway to a point in the center of the north bound lane, 324 feet west of the centerline of the proposed driveway. (See Sheet 15 of 16)
 - 6. *Chapter 79-8.C and Conditional Use Decision and Order Condition 17 – The proposed redevelopment is located in the Township's Act 209 Transportation Service Area, and as such, this development is subject to the Townships Transportation Impact Fee of \$2,334 per weekday afternoon peak hour trip. As such, based on the trip generation calculations provided by the applicant's traffic engineer, the site will generate 34 new weekday afternoon peak hour trips. Furthermore, the existing home on the site would generate one new weekday afternoon peak hour trip, which can be*



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credited against the total Transportation Impact Fee for the site. As such, the number of trips subject to the Township's Transportation Impact Fee is 33 and the resultant Transportation Impact Fee is \$77,022.

6. Noted.

VIII. TOWNSHIP PLANNING CONSULTANT COMMENTS: BRANDYWINE CONSERVANCY

In response to these submitted plans and the applicant's response letter, the Brandywine Conservancy is satisfied that the applicant has addressed or resolved our prior comments included in our review letters dated January 3, 2023, and April 2, 2024. Currently, the conservancy has no additional comments on the application.

IX. TOWNSHIP SEWER CONSULTANT COMMENTS: ARRO CONSULTING, INC.

1. *The necessary financial security shall be posted with the Township, which shall be in a form and amount acceptable to the Township.*
 1. **Financial security shall be posted in the form and amount acceptable to the township prior to plan recording.**

We hope that the above plan revisions satisfy your requirements. If you have any questions or need further clarification of the revisions we have made, please feel free to contact me.

Sincerely,

A handwritten signature in blue ink, appearing to read "Michael Cowen".

Michael Cowen, EIT
Project Manager



COMMONWEALTH ENGINEERS, INC.

114 East Lancaster Avenue ~ Second Floor ~ Downingtown, PA 19335
Telephone: (610) 518-9003 ~ Fax: (610) 518-9004 ~ comeng@cei-1.com

LOT CONSOLIDATION LEGAL DESCRIPTION

TAX ID/PARCEL NO. 3204 0038 0800 (32-4-38.8) and;
TAX ID/PARCEL NO. 3204 0037 0000 (32-4-37)

Project #202317
241 Park Road
July 3, 2024

ALL THAT CERTAIN lot or piece of ground situate in the Township of Upper Uwchlan, County of Chester and Commonwealth of Pennsylvania, bounded and described according to a plan entitled "Preliminary/Final Land Development Plan for 241 Park Road" and prepared by Commonwealth Engineers, Inc. Downingtown, PA., dated December 4, 2023, last revised May 24, 2024 and recorded in Chester County as Plan # _____, as follows, to wit:

BEGINNING AT A POINT a mag spike set in the cartway of the southeasterly intersection of Park Road and Ticonderoga Boulevard; thence, leaving said cartway and passing through and along the southeasterly right-of-way of Ticonderoga Boulevard, being 60 feet wide, South 51 degrees 02 minutes 54 seconds East, a distance of 161.73 feet to a 1" iron pin found;

thence, leaving said right-of-way and traveling along lands N/F of FedEx National LTL, Inc. the following two (2) courses and distances:

- (1) South 37 degrees 24 minutes 27 seconds West, a distance of 219.67 feet to a 1" bent iron pin found;
- (2) North 51 degrees 15 minutes 28 seconds West, a distance of 162.81 feet to a 1" iron pin found;

thence leaving said lands and traveling through the easterly right-of-way of Park Road, North 37 degrees 41 minutes 23 seconds East, a distance of 220.24 feet to the **POINT AND PLACE OF BEGINNING**.

Containing 0.819 Acres, more or less.

BEING THE SAME PREMISES which 241 Park Avenue, LLC, a Pennsylvania limited liability company by Deed dated June 7, 2019 and recorded June 12, 2019 in the Office of the Recorder of Deeds in and for the County of Chester, Commonwealth of Pennsylvania, in Deed Book #9939, Page #1141, granted and conveyed unto 241 Park Road, LLC, a Pennsylvania limited liability company.



COMMONWEALTH ENGINEERS, INC.

114 East Lancaster Avenue ~ Second Floor ~ Downingtown, PA 19335
Telephone: (610) 518-9003 ~ Fax: (610) 518-9004 ~ comeng@cei-1.com

CONDITIONAL USE IMPACT STATEMENT FOR 241 PARK ROAD July 26, 2024

Client Name:	Planebrook Partners, LLC
Property:	3204 0037 0000 located at 241 Park Road 3204 0038 0800 located at 235 Park Road
Zoning District:	C1 Village District
Nature of Development:	Daycare Facility
Combined Gross Acreage:	0.819 +/- acres
Current Sewer/Water Service:	On lot water and sewer unused by vacant house.
Proposed Sewer/Water Service:	Public Water & Public Sewer

The property consists of two parcels:

- (a) 235 Park Road, which is more fully described as Chester County UPI No. 32-4-38.8 is undeveloped. The rear of the property abuts the Federal Express facility on Ticonderoga Blvd.; and
- (b) 241 Park Road, which is more fully described as Chester County UPI No. 32-4-37, which is developed with single-family dwelling and outbuilding, which have been unused for years.

Planebrook Partners, LLC plans to combine two parcels totaling 0.819 acres (gross) (together, the "Property") and construct a two-story, 6,000 sf (12,000 total gross floor area) daycare facility, access drive from Park Road, play area (approximately 3,000 sf), parking lot and associated stormwater management infrastructure.

The facility will operate Monday through Friday during normal business hours with an anticipated capacity of 120-125 children and 20 employees.

The proposed project is permitted with Conditional Use Approval per Section 200-33.B(3) of the Upper Uwchlan Township Zoning Ordinance. The applicant is also pursuing a modification of parking requirements pursuant to the Township Zoning Ordinance Section 200-73.K. As a daycare facility is an permitted use, it is not contrary to public health safety or welfare, nor will it lower the level of service of the roads in the area.

External access locations and interior traffic circulation will be designed in accordance the Township ordinance to allow adequate and safe use by the users as well as compliance for use of emergency vehicles.

Approval from Aqua Pennsylvania for firefighting purposes and water use by the facility will be obtained, as well as sewer approvals from the Township.

Sidewalk is proposed along Park Road to improve pedestrian traffic and access.



The landscaping and lighting will be designed per the Township Ordinance. The adjoining properties include a Federal Express Depot (abutting the project parcels on two sides), the Windsor Baptist Church/Windsor Christian Academy with recreation area, across from the project area, which contains both school and church buildings and a dentist office across Ticonderoga Road, therefore noise would not be a factor for the proposed daycare facility.

A Fiscal Impact Analysis has been attached.

EH CREATIVE SERVICES LLC
16 MANOR ROAD
PAOLI, PA 19301
610.322.7154
erik@erikhetzel.com

Memorandum

To: Victor Kelly Jr., PE

From: Erik Hetzel, AICP/PP, LEED AP

Date: April 14, 2024

Re: Fiscal Impact Analysis – Proposed Daycare Facility, 241 Park Road

Planebrook Partners, LLC is proposing to construct a 12,000-square-foot daycare facility on the property located at 241 Park Road in Upper Uwchlan Township, Chester County, Pennsylvania. Along with site improvements that will generate additional real estate tax revenue for the Township, the applicant expects to create approximately 18 jobs at the site. It is projected that the proposed development will result in beneficial, net-positive annual fiscal impacts to both Upper Uwchlan Township and the Downingtown Area School District, as described in Table 1.

Table 1
Summary of Annual Fiscal Impacts

	Tax Revenues	Expenditures	Net Fiscal Impact
Upper Uwchlan Township	\$2,159	(\$1,784)	\$375
Downingtown Area School District	\$38,141	\$0	\$38,141
TOTAL	\$40,301	(\$1,784)	\$38,517

Fiscal impacts presented in this analysis were estimated using a methodology developed by the Rutgers University Center for Urban Policy Research, as originally described in The New Practitioner's Guide to Fiscal Analysis¹ and further developed in a

¹ Burchell, Robert W., David Listokin, et al. *The New Practitioner's Guide to Fiscal Impact Analysis*, New Brunswick, NJ: Rutgers, The State University of New Jersey, 1985.

later publication by the same authors entitled Development Impact Assessment Handbook².

Revenue Impacts

Real Estate Property Tax - At project completion, the portion of the building dedicated to the proposed use will have a total market value of approximately \$3.5 million. The market value of the improved property was provided by the Applicant based on estimated construction value for this project and their knowledge and experience of comparable properties similarly developed. This market value translates to an assessed value of approximately \$1,258,993. The assessment calculation is based on the current (2023-2024) Chester County common-level ratio of 2.78, which estimates assessed value at approximately 35.97% of market value. The Township levies the real estate tax at the rate of 1.121 mills, which includes the Fire Hydrant millage due to the project's location being within 780 feet of the fire hydrant at Lila Lane and Park Road. The real estate tax will generate approximately \$1,411 annually to the Township. The School District millage rate is currently 29.558 mills, which applied the assessment described above, will generate approximately \$37,213 in real estate taxes to the School District annually.

Earned Income Tax (EIT) Revenues – Workers who reside in Upper Uwchlan Township pay the EIT at the rate of 1.0%, with 0.5% going to each the Township and School District. The EIT is also levied at the rate of 1.0% on non-residents who do not pay the EIT in their home jurisdictions with the full amount of the tax going only to the Township. The applicant estimates that the proposed use will create 18 jobs at this location with an estimated mean annual wage of \$29,920 per employee, based on data obtained from the U.S. Bureau of Labor Statistics specific to the Philadelphia metropolitan region³. We cannot project where these employees will reside, nor whether they will be paying the EIT to their home jurisdictions. Hence, we conservatively assume that 25% of the workers (or, 5 of the total 18 workers) will be eligible to pay the EIT as Township residents. Under the foregoing assumptions, the EIT will generate revenues totaling \$748 to the Township and \$748 to the School District.

² Burchell, Robert W., David Listokin, et al. *Development Impact Assessment Handbook*. Washington, D.C.: ULI-the Urban Land Institute, 1994.

³ Occupation title "Childcare Workers" from U.S. Bureau of Labor Statistics, *Occupational Employment and Wage Statistics, May 2022 Metropolitan and Nonmetropolitan Area Occupational Employment and Wage Estimates, Philadelphia-Camden-Wilmington, PA-NJ-DE-MD* <https://www.bls.gov/oes/current/oes_37980.htm#39-0000>

Regional Economic Impacts

The proposed development will have a beneficial economic “ripple effect” in the local economy, as the new employees use goods and services in and around Upper Uwchlan Township. In addition, the building improvements will provide construction jobs and result in construction-related consumption expenditures in the local and regional economies. These impacts are not reflected in the summary table included in this memorandum but will provide economic benefits over and above the annual revenues described therein.

Cost Analysis

Annual Township expenditures attributable to the proposed development were projected using the Per Capita Multiplier Method described in the Development Impact Assessment Handbook. In calculating the per capita expenditure value for the Township, the methodology uses information from the current (2023) Township budget and accounts for the fact that costs are divided differently among serving both residential and non-residential portions of the Township, based on the actual mix of land uses provided by the County Board of Assessment. This analysis estimates annual per capita costs at \$99.11 per employee. Overall, it is estimated that the proposed development will result in Township costs totaling approximately \$1,784 annually, which is essentially offset by revenues totaling \$2,050 annually from the tax sources described previously.

The analysis included expenses from the Township budget for services most likely to be impacted by the proposed development. These expense items are described in Table 2.

Table 2
Upper Uwchlan Township Budget
General Fund Expenses

Expenditure Line Item	Budget Amount	Amount Used in Analysis	
Tax Collection	\$12,900	100%	\$12,900
Legal	\$60,000	100%	\$60,000
Police Expenses	\$2,977,950	100%	\$2,977,950
Fire & Ambulance	\$504,770	100%	\$504,770
Codes Administration	\$391,089	10%	\$39,109
Emergency Operations/Other	\$65,024	100%	\$65,024
Public Works	\$1,177,346	100%	\$1,177,346
TOTAL	\$5,189,079		\$4,837,099

The per capita multiplier method differentiates how services are distributed among residential and nonresidential portions of Upper Uwchlan Township on the basis of a ratio of residential to nonresidential uses as described in County assessment data for the Township, using a blended average of total assessments and number of parcels for residential and nonresidential land use classifications. Assessment information for Upper Uwchlan Township was obtained from the Chester County Board of Assessment for this analysis. Details of the cost calculations are described in Table 3 on the next page.

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Table 3
Per-Capita Cost Calculation Methodology

A	Number of residential parcels in Township	4,180
B	Number of nonresidential parcels in Township	245
C	TOTAL PARCELS (A + B)	4,425
D	Percentage of nonresidential parcels in Township (B / C)	5.54%
E	Total residential assessment in Township	\$965,722,915
F	Total nonresidential assessment in Township	\$171,473,350
G	TOTAL ASSESSMENT (E + F)	\$1,137,196,265
H	Percentage of nonresidential assessment in Township (F / G)	15.08%
I	Blended average of parcels and assessment, Nonresidential percentage of Township ((D + H) / 2))	10.31%
J	Cost for services likely to impacted by new development	\$4,837,099
K	Cost of Township services allocated to nonresidential uses (I x J)	\$498,592
L	Total employment (number of employees working in Township)	5,031
M	Per capita cost per employee working in Township (K / L)	\$99.10
N	Projected employees working at proposed development	18
O	Township costs attributable to proposed development (M x N)	\$1,783.87

NOTES:

1. Parcel and assessment information obtained from Chester County Board of Assessment.
2. Township cost information from Upper Uwchlan Township Budget, 2023.
3. Township employment estimate from Delaware Valley Regional Planning Commission, Municipal and County-Level Population and Employment Forecasts, 2015-2050
(<https://www.dvRPC.org/webmaps/popempforecasts/>)
4. Projected number of new employees working at proposed daycare facility estimated by applicant/operator.

The proposed development of this non-residential use will not generate any school-aged children and hence, will not result in additional costs for the School District.

Public Services Impacts

Erik Hetzel (the author of this report) met with Township Manager Tony Scheivert on February 5, 2024 to discuss potential public service demands from the proposed development. Mr. Scheivert noted that, as with any new development there will likely be some additional need for public services; however, based on the cost analysis described herein, demand from the proposed daycare development is not anticipated to exceed existing capacity for such Township functions as administration, public works, police, fire and emergency services, and parks and recreation.

Published demand factors are available for Police, Fire and Emergency Medical Services (EMS) personnel, vehicles, and facilities. These factors and the resulting estimated demands for each service are presented in Table 4 on the next page. Planning standards for these calculations are from the Development Impact Assessment Handbook⁴ and the Pennsylvania Department of Health, Bureau of Emergency Medical Services⁵ based on a non-residential use with a daytime population of approximately 143 persons (18 employees and 125 students). Personnel, vehicles, and facilities for Police and Fire are based on the noted planning standards per 1,000 population per year. EMS planning standards for personnel and vehicles are per 30,000 population per year, and the planning standard for EMS calls is per 1,000 population per year.

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⁴ Burchell, Robert W., David Listokin, et al. *Development Impact Assessment Handbook*. Washington, D.C.: ULI-the Urban Land Institute, 1994.

⁵ 2021 EMS Data Report, 2022

<https://www.health.pa.gov/topics/Documents/EMS/2021%20EMS%20Data%20Report.pdf>

Table 4
Public Safety Demand Factors and
Projected Demand from Proposed Development

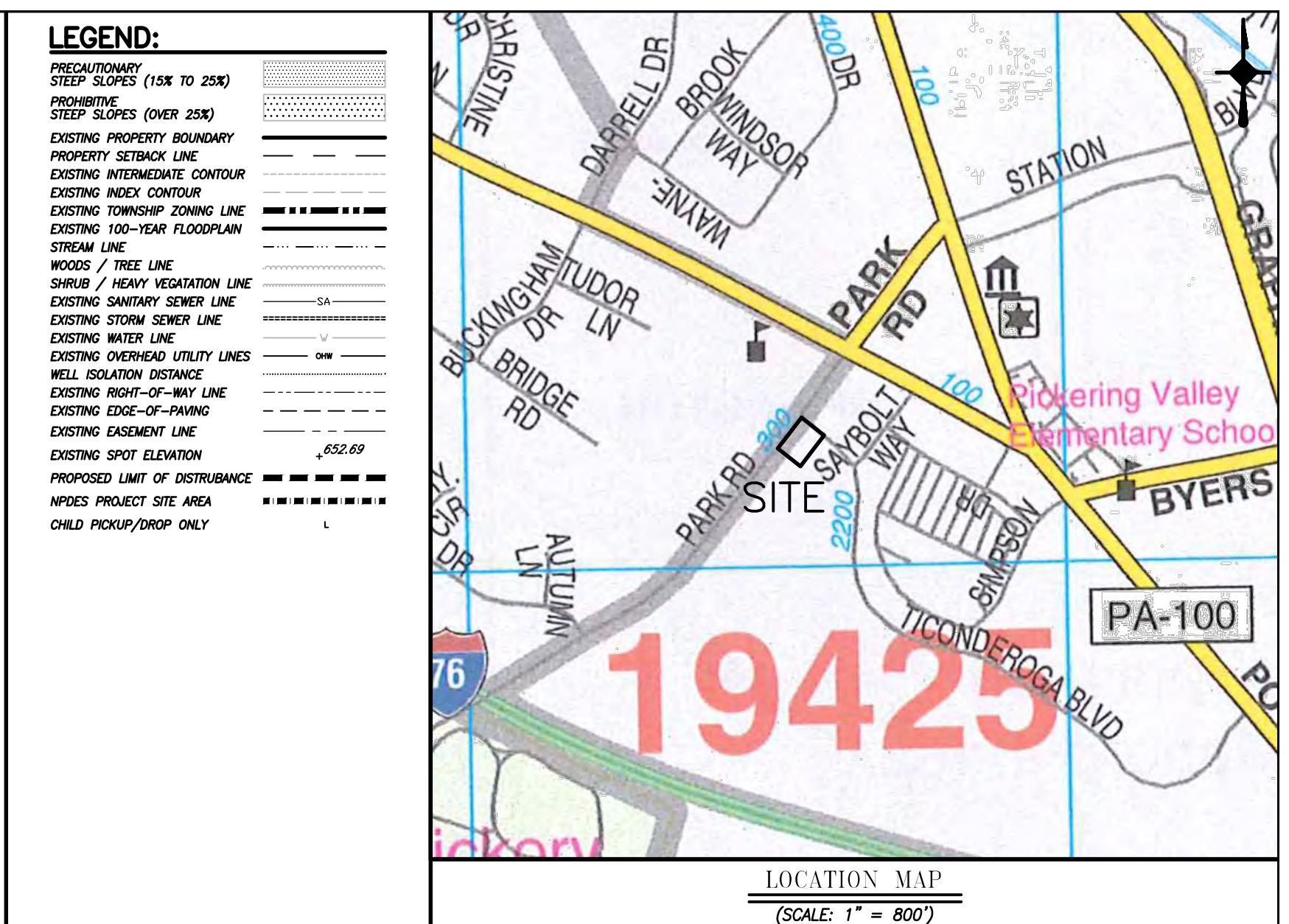
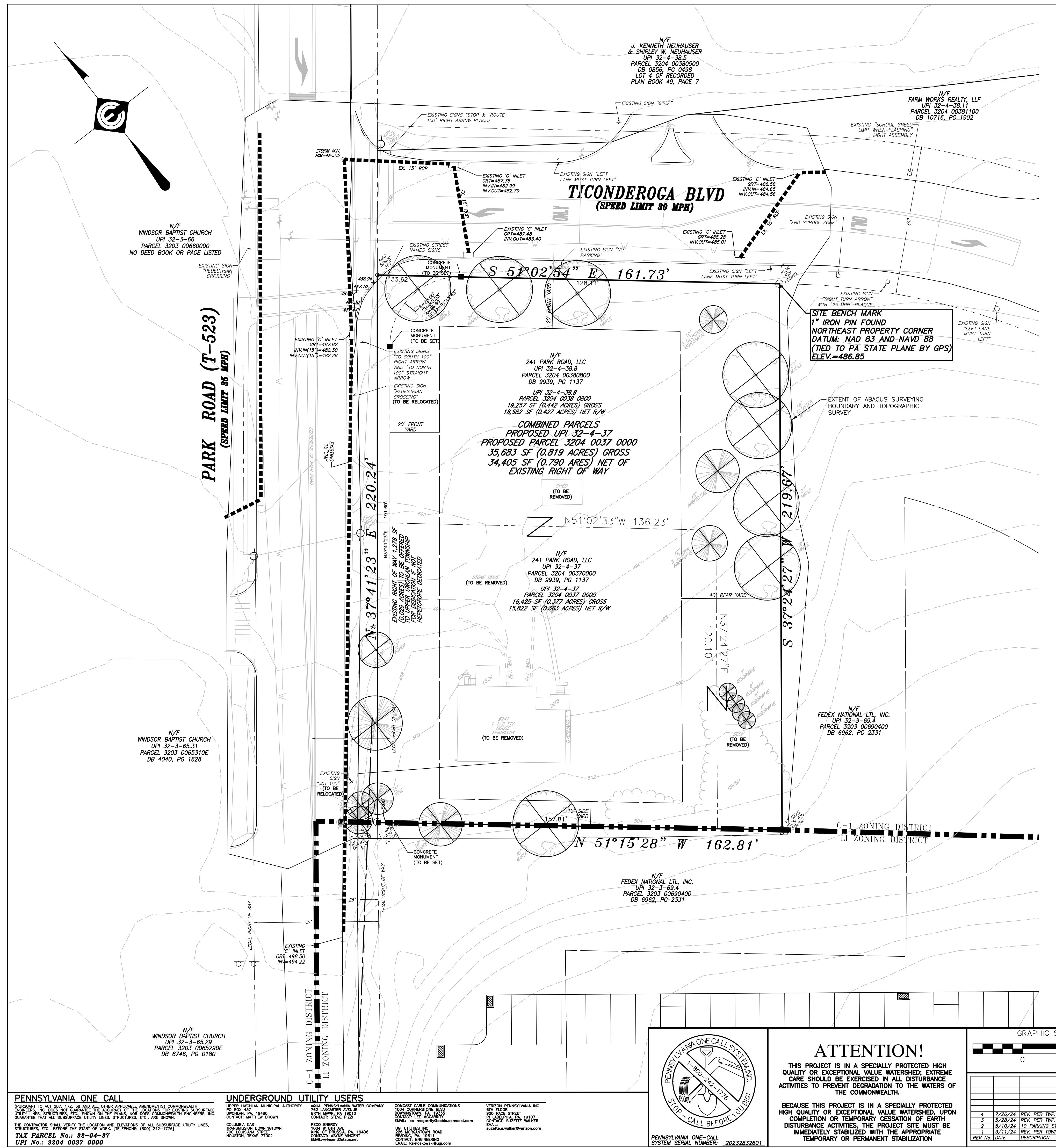
	Planning Standard	Projected Demand
<i>Police</i>		
Personnel	0.50	0.07
Vehicles	0.15	0.02
Facilities (square feet)	50	7.15
<i>Fire</i>		
Personnel	0.41	0.06
Vehicles	0.05	0.01
Facilities (square feet)	62.5	8.94
<i>EMS</i>		
Personnel	1.03	0.0049
Vehicles	0.25	0.0012
Calls (per year)	31.13	4.45

Conclusions and Summary

In conclusion, the net positive fiscal impact in terms of projected revenues over costs for Upper Uwchlan Township is expected to be over \$375 annually at project completion, essentially paying for itself in terms of Township services. The projected net positive fiscal impact on the Downingtown Area School District is projected to be more significant at over \$38,000 per year. The combined net positive fiscal impact for both taxing authorities is estimated at nearly \$39,000 annually. Table 5 on the next page summarizes the project details and fiscal impacts to the Township and School District.

Table 5
Summary of Project Details and Fiscal Impacts

Project Details - 241 Park Road Daycare Facility	
Non-Residential Square Feet	12,000
Market Value of Proposed Development	\$3,500,000
Assessed Value (approx. 35.97% of market value)	\$1,258,993
New Residential Population	0
New School-Aged Children	0
New Employees	18
Annual Wages per Employee	\$29,920
Upper Uwchlan Township Fiscal Impacts	
Real Estate Tax Revenue (1.121 mills)	\$ 1,411
Earned Income Tax Revenue (0.5%)	\$ 748
Total Township Revenues	\$ 2,159
Total Township Expenditures	\$ (1,784)
Net Township Fiscal Impact	\$ 375
Downingtown Area School District Fiscal Impacts	
Real Estate Tax Revenue (29.558 mills)	\$ 37,213
Earned Income Tax Revenue (0.5%)	\$ 748
Local Services Tax (\$10 per resident worker)	\$ 180
Total School District Revenues	\$ 38,141
Total School District Expenditures	\$ -
Net School District Fiscal Impact	\$ 38,141
Total Development-Generated Revenues (Township + School District)	\$ 40,301
Total Development-Generated Expenditures (Township + School District)	\$ (1,784)
Total Net Annual Fiscal Impact (Township + School District)	\$ 38,517



GENERAL NOTES:

RECORD OWNER: 241 PARK ROAD, LLC 413 EXECUTIVE DRIVE LANGHORNE, PA 19047	RECORD OWNER: 241 PARK ROAD, LLC 413 EXECUTIVE DRIVE LANGHORNE, PA 19047	EQUITABLE OWNER /APPLICANT: 241 PR PARTNERS, LLC c/o SCOTT RISBON 114 EAST LANCASTER AVE. 2ND FLOOR DOWNTOWN PA, 19335 (610) 637-1975
SITE ADDRESS: 235 PARK ROAD DOWNTOWN, PA 19335 PARCEL 3204 00380800 UPI 32-4-38.8 DB 9939 PG 1137	SITE ADDRESS: 241 PARK ROAD DOWNTOWN, PA 19335 PARCEL 3204 00370000 UPI 32-4-37 DB 9939 PG 1137	
2. TOTAL COMBINED TRACT AREA: 35,682.5 SF / 0.819 AC (GROSS)		
NUMBER OF EXISTING LOTS.....2 NUMBER OF PROPOSED LOTS.....1 PROPOSED USE.....COMMERCIAL DAYCARE CENTER		
. BOUNDARY AND PARTIAL TOPOGRAPHIC SURVEY PERFORMED BY ABACUS SURVEYING SEPTEMBER 2023. INFORMATION SHOWN WITHIN THE LIMITS OF SURVEY SHOWN ON THE PLANS.		
DATUMS - NAD 83 AND NAVD 88 (TIED TO PA STATE PLANE WITH GPS) BENCH MARK - 1" IRON PIN FOUND NORTHEAST PROPERTY CORNER ELEV - 486.85		
PARTIAL TOPOGRAPHIC INFORMATION SHOWN PER AN AS-BUILT SURVEY OF EAGLEPOINTE; AERIAL PHOTOGRAPHS PREPARED BY PLUM AND ASSOCIATES, DATED DECEMBER 4, 1994; A TOPOGRAPHIC PLAN PREPARED BY CHESTER VALLEY ENGINEERS FOR PICCONE PROPERTY, PLAN DATED APRIL 29, 1992; FIELD SURVEY OF ROUTE 100 AND PARK ROAD PREPARED BY GROSSER AND ERHILICH ASSOCIATES. ALL DATA COMPILED IN A PLAN ENTITLED "LAND DEVELOPMENT PLAN FOR 241 PARK ROAD ASSOCIATES, LLC" PREPARED BY EDWARD B. WALSH & ASSOCIATES, INC., DATED FEBRUARY 18, 2010, AND LAST REVISED SEPTEMBER 29, 2010. DATUM: USGS		
PARTIAL TOPOGRAPHY FROM PENNSYLVANIA SPATIAL DATA ACCESS (PASDA) WEBSITE.		
. UNDERGROUND UTILITIES SHOWN AS APPROXIMATE. NO GUARANTEE IS MADE THAT THE UNDERGROUND UTILITIES ARE ACCURATELY OR COMPLETELY SHOWN HEREON. THE SITE CONTRACTOR MUST VERIFY LOCATION AND DEPTH OF ALL UNDERGROUND UTILITIES AND FACILITIES PRIOR TO BEGINNING CONSTRUCTION.		
. ON-SITE SEWER AND WATER ARE PROPOSED.		
. SOILS CLASSIFICATIONS FROM THE UNITED STATES DEPARTMENT OF AGRICULTURE (USDA) NATURAL RESOURCE CONSERVATION DISTRICT WEB SOIL SURVEY, JUNE 2023.		
. CONCRETE MONUMENTS SHOWN AS THUS " ■ " SHALL BE PLACED WHERE INDICATED. IRON PINS SHOWN AS THUS " ● " SHALL BE PLACED WHERE INDICATED.		
. THE DEVELOPER SHALL CONFORM TO OR CAUSE CONFORMANCE WITH ALL OF THE APPLICABLE REQUIREMENTS OF THIS PLAN AND THE UPPER UWCHLAN TOWNSHIP ZONING ORDINANCE AND SUBDIVISION AND LAND DEVELOPMENT ORDINANCE AS WELL AS OTHER APPLICABLE LOCAL, STATE AND FEDERAL LAWS, STATUTES, REGULATIONS AND ORDINANCES.		
. THE DEVELOPER SHALL NOTIFY UNDERGROUND UTILITY USERS AT LEAST THREE DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION IN ACCORDANCE WITH PA ACT 287 OF 1974, AS AMENDED BY ACT 187 OF 1996 & ALL OTHER APPLICABLE AMENDMENTS, AS WELL AS CONFORM TO APPLICABLE REGULATIONS OF SAID ACTS. UNDERGROUND UTILITIES SHOWN ARE FROM SURFACE EVIDENCE OBSERVABLE IN THE FIELD AND AS MARKED BY THE UTILITY COMPANIES.		
. ALL PROPOSED STORMWATER MANAGEMENT FACILITIES ARE PERMANENT. MAINTENANCE OF PERMANENT STORMWATER MANAGEMENT FACILITIES WILL BE THE RESPONSIBILITY OF THE LOT OWNER(S).		
1. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% PERENNIAL VEGETATIVE COVER AND/OR ANY OTHER PERMANENT, NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER SUBSURFACE MOVEMENTS.		
2. THIS PROJECT IS WHOLLY LOCATED WITHIN THE MARSH CREEK WATERSHED. A WATERSHED DESIGNATED A HIGH QUALITY-TROUT STOCK FISHERY; MIGRATORY FISHERY (HQ-TSF; MF) BY THE COMMONWEALTH OF PENNSYLVANIA.		
3. THE SITE IS WHOLLY LOCATED WITHIN FEMA FLOOD PLAIN ZONE X PER FEMA FLOOD PLAIN MAP 42029C0090G EFFECTIVE SEPTEMBER 29, 2017.		
4. PER A WETLAND EVALUATION CONDUCTED BY WOLF BIOSERVICES, INC. ON OCTOBER 17, 2023, NO WETLANDS ARE PRESENT ON THE SUBJECT PROPERTY.		
5. UPON COMPLETION OF CONSTRUCTION, THE ENTIRE AMOUNT OF TOPSOIL STRIPPED SHALL BE REPLACED ON		

ZONING: C1 VILLAGE DISTRICT

**AREA AND BULK REGULATIONS PER UPPER UWCHLAN TOWNSHIP
ZONING ORDINANCE PART 19 SECTION 27-1903**

	<u>REQUIRED</u>	<u>EXISTING</u>	<u>PROPOSED</u>
MINIMUM NET LOT AREA	10,000 SF (MIN)	35,682 SF	35,682 SF
MINIMUM LOT WIDTH	100 FT (MIN)	100 FT (MIN)	100 FT (MIN)
MINIMUM FRONT YARD	20 FT (MIN)	20 FT (MIN)	20 FT (MIN)
MINIMUM SIDE YARD (EACH)	10 FT (MIN)	10 FT (MIN)	10 FT (MIN)
MINIMUM REAR YARD	40 FT (MIN)	40 FT (MIN)	40 FT (MIN)
MINIMUM RT. 100 SETBACK	50 FT (MIN)	50 FT (MIN)	50 FT (MIN)
MINIMUM GARAGE SETBACK	40 FT (MIN)	40 FT (MIN)	40 FT (MIN)
MAXIMUM BUILDING COVERAGE	30% (MAX)	30% (MAX)	16.81% (6,000 SF)
MAXIMUM LOT COVERAGE	60% (MAX)	60% (MAX)	59.41% (21,199 SF)
MAXIMUM BUILDING HEIGHT	35 FT (MAX)	35 FT (MAX)	35 FT (MAX)

ONING NOTES:

1. NOT MORE THAN 15% OF THE AREA OF A LOT SHALL BE OCCUPIED BY BUILDINGS OR STRUCTURES LESS THAN 1-1/2 STORIES IN HEIGHT.
2. NO ACCESSORY BUILDING OR STRUCTURE SHALL BE LOCATED WITHIN THE FRONT YARD, NOR WITHIN 10 FEET OF ANY SIDE OR REAR LOT LINE.
3. NO INDIVIDUAL BUILDING SHALL CONTAIN MORE THAN 6,000 SQUARE FEET OF TOTAL FLOOR AREA, EXCEPT WHERE INCREASED SQUARE FOOTAGE ABOVE 6,000 IS ENTIRELY WITHIN AN ADDITIONAL STORY OR STORIES ABOVE THE FIRST FLOOR OR WHERE SQUARE FOOTAGE ABOVE 6,000, AND NOT LIMITED TO A 50% INCREASE, IS APPROVED SUBJECT TO CONDITIONAL USE APPROVAL AND IN ACCORDANCE WITH ALL APPLICABLE DESIGN STANDARDS.

IDENTIFICATION:

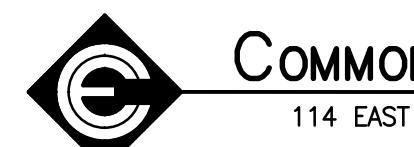
ITE IS COMPRISED OF GLADSTONE GRAVELLY LOAM, 3 TO 8 PERCENT S. GLADSTONE SOILS ARE NOT IDENTIFIED AS HYDRIC SOILS.

DESCRIPTION:	HYDROLOGIC GROUP	DEPTH TO WATER	DEPTH TO BEDROCK
GLADSTONE GRAVELLY LOAM, 3 TO 8 PERCENT SLOPES	B	>80"	60"-80"

TE ANALYSIS & IMPACT/CONSERVATION PLAN

*PRELIMINARY/FINAL LAND DEVELOPMENT PLANS
FOR*

UPPER LIWCHI LAN TOWNSHIP

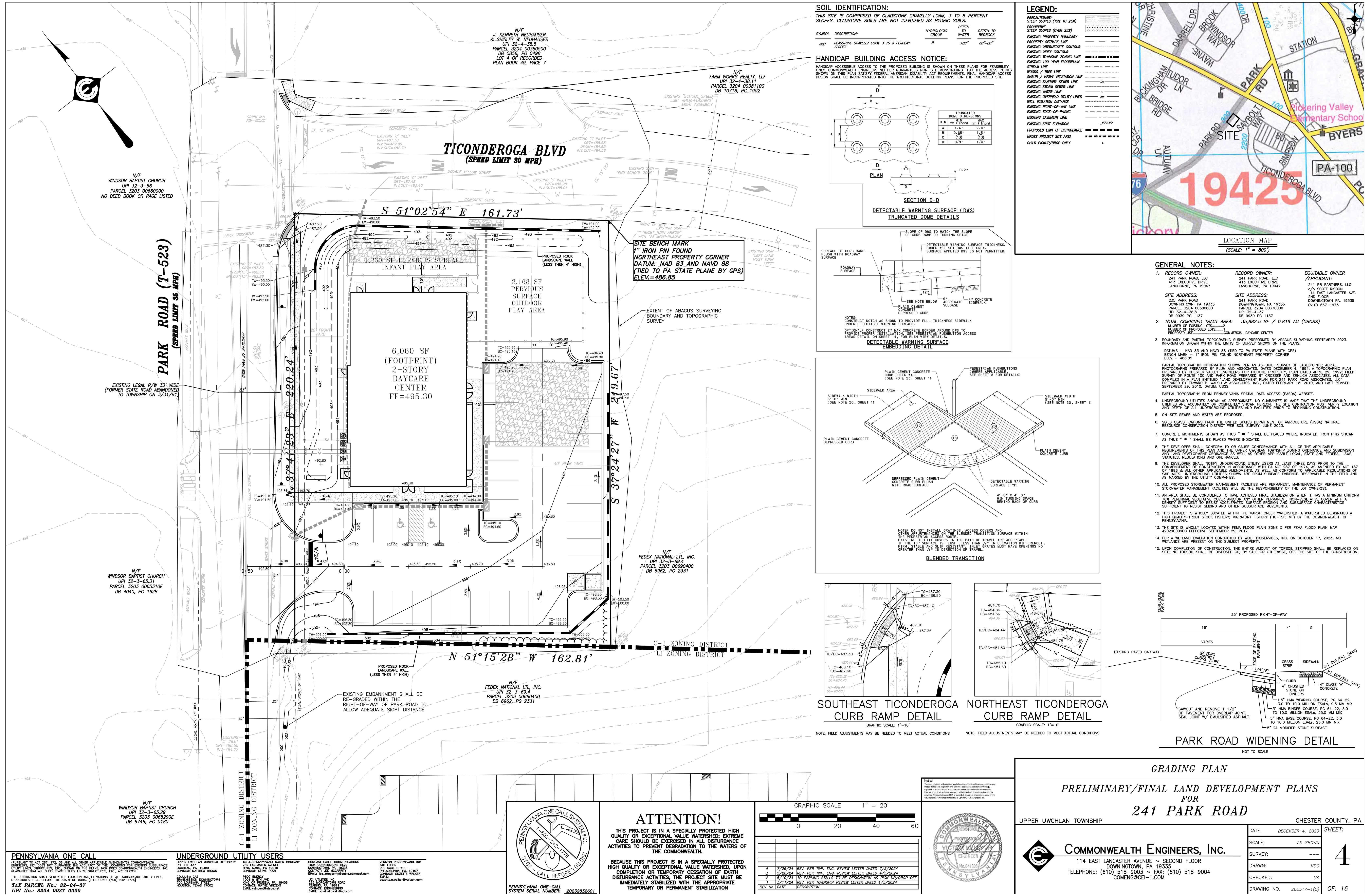


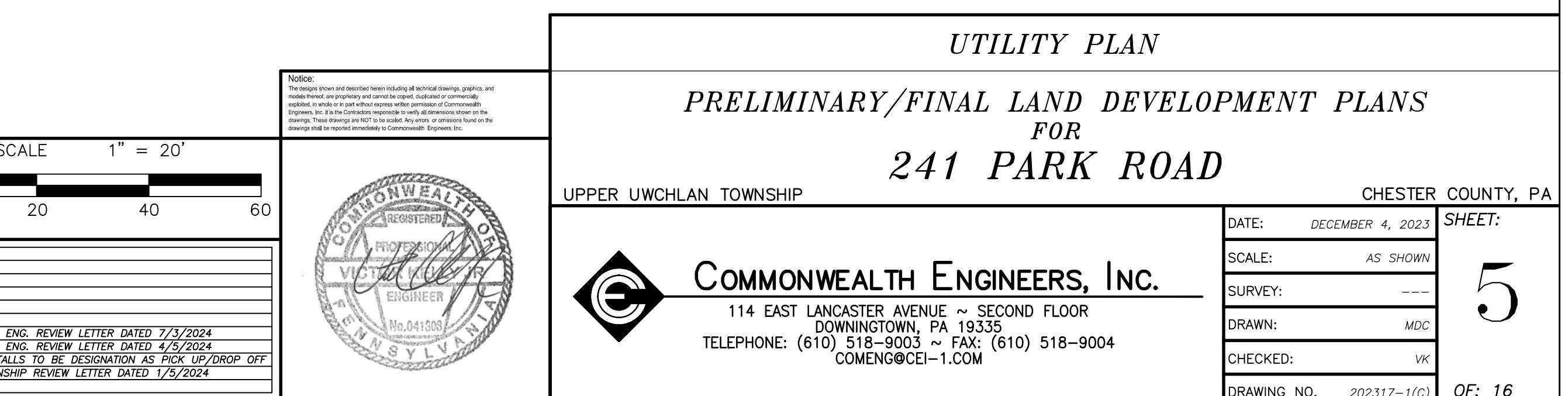
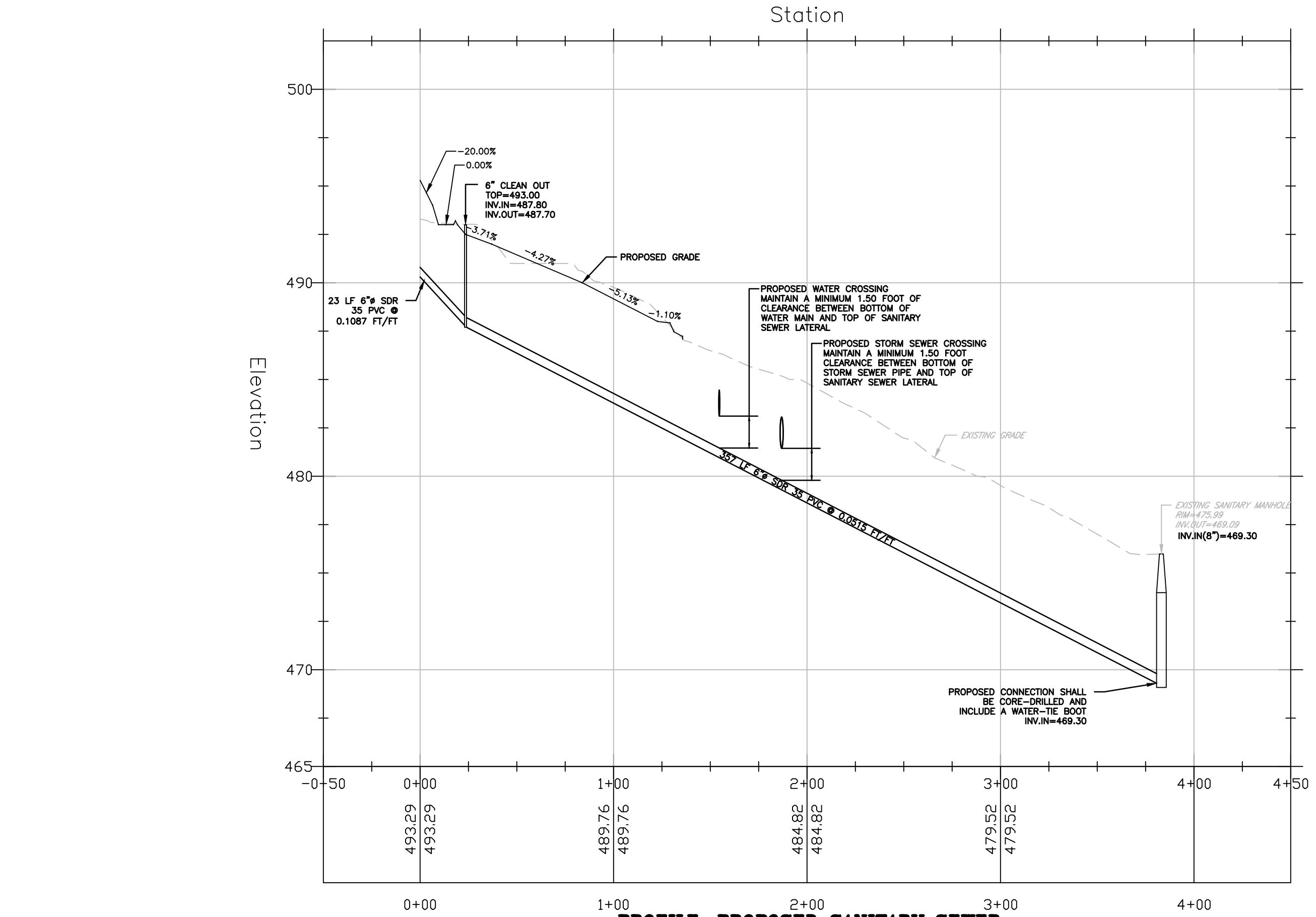
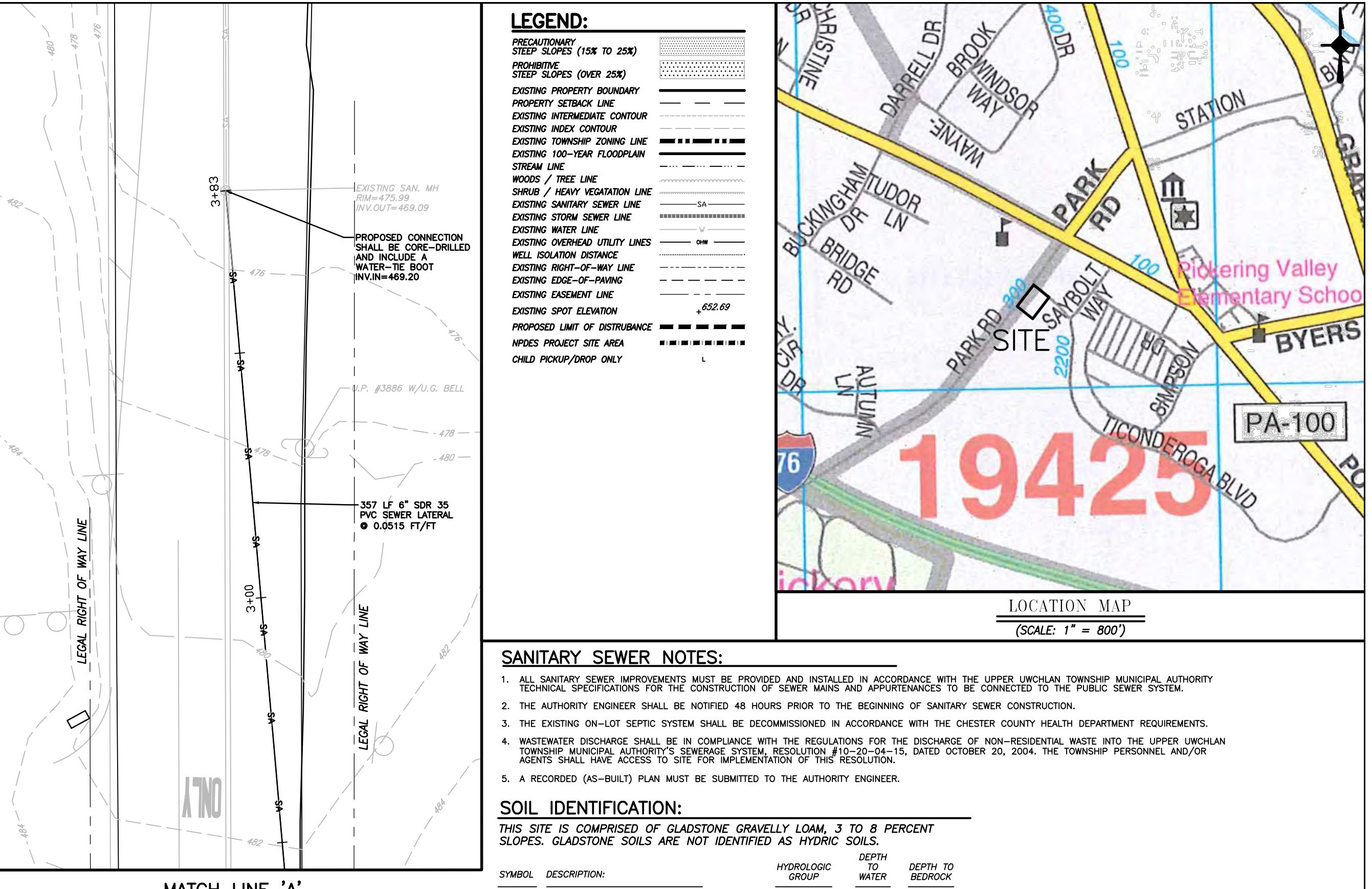
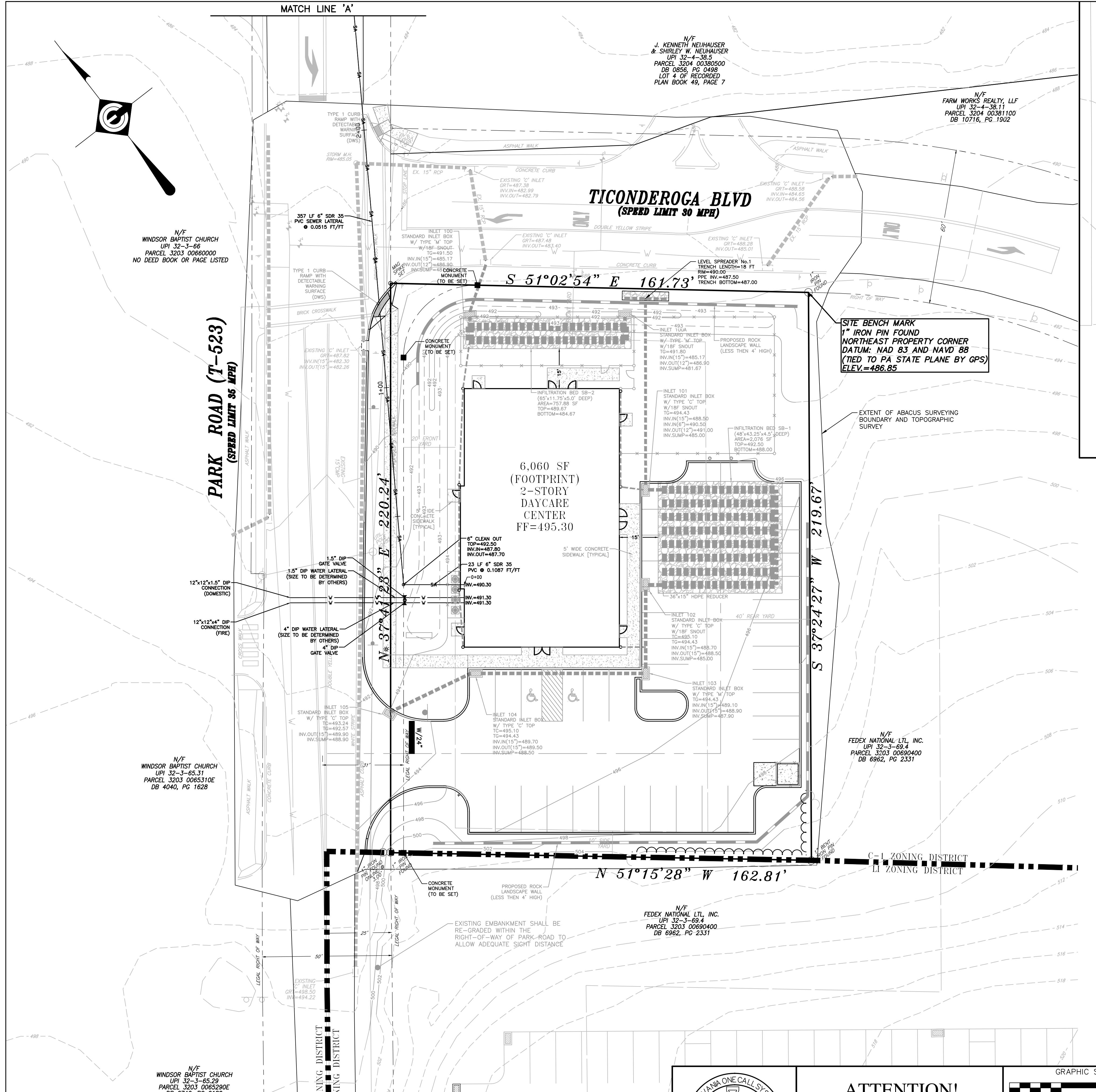
NWEALTH ENGINEERS, INC.

LANCASTER AVENUE ~ SECOND FLOOR

LANCASTER AVENUE ~ SECOND FLOOR
DOWNTOWN, PA 19335
(610) 518-9003 ~ FAX: (610) 518-9004

CHESTER COUNTY, PA	
DECEMBER 4, 2023	
AS SHOWN	
Y:	---
:	MDC
ED:	VK
NG NO.	202317-1(C)
SHEET: 3 OF: 16	





NOTES:

1. ADJUST TO GRADE WITH CONCRETE GRADE RINGS (MAXIMUM VERTICAL ADJUSTMENT 6") SEE FRAME & COVER DETAILS
2. MECHANICALLY VIBRATED PRECAST CONCRETE SHALL CONFORM TO A.S.T.M. SPEC. C-478.
3. IF INCOMING INVERT EXCEEDS OUTGOING INVERT BY GREATER THAN OR EQUAL TO 6", SEE INSIDE SPLASH/INSIDE DROP MANHOLE DETAILS.
4. FOR MANHOLES WHERE TOP OF RIM TO INVERT DISTANCE IS LESS THAN 5'-0", USE FLAT TOP MANHOLE IN LIEU OF CONE TOP.
5. FILL ALL LIFTING HOLES WITH NON-SHRINK, NON-METALLIC GROUT.
6. LARGER DIAMETER MANHOLE REQUIRED ON INSIDE DROP AND INSIDE SPLASH CONNECTION GREATER THAN 12" SEE MANHOLE DETAILS FOR INSIDE SPLASH/INSIDE DROP
7. PROVIDE 2 RINGS OF PREFORMED BUTYL RUBBER SEALING COMPOUND (INSIDE & OUTSIDE) AT ALL MANHOLE JOINTS.
8. COAT EXTERIOR WITH BITUMASTIC, 20-1 DFT (TYP.).

SECTION

		Upper Uwchlan Township Municipal Authority STANDARD DETAIL - SEWER SYSTEM	ARR Suite 200, 321 N. Birdsboro Tel 6 Fax 6
4	4/21		
3	1/13		
2	6/05		
1	11/04		
REVISION	DATE		

**PRECAST MANHOLE
8" THROUGH 24" SEWERS**

DATE: APRIL

DETAIL: 7000S

NOTES:

1. WATERTIGHT FRAME & COVER SHALL BE CAT. NO. 1045ZPT FRAME & 1040APT COVER, BY EAST JORDAN IRON WORKS, DESIGNED FOR AASHTO HS-25 LOADING.
2. ANCHOR BOLT HOLES SHALL BE DRILLED WITH A PERCUSSION OR ROTARY HAMMER DRILL UTILIZING A CARBIDE DRILL BIT. CORE DRILLING, OR ROTATIONAL ONLY DRILLING, OF ANY KIND, IS NOT ALLOWED.
3. FRAME ANCHORS SHALL BE TIGHTENED PRIOR TO CURING OF NON-SHRINK, NON-METALLIC GROUT.

PLAN-COVER PATTERN

SECTION

Upper Uwchlan Township Municipal Authority
STANDARD DETAIL – SEWER SYSTEM

MANHOLE FRAME & COVER (WATERTIGHT)

ARRO

4 **4/21**

3 **1/13**

2 **6/05**

1 **11/04**

REVISION **DATE**

Suite 200, 321 N. Furnace Street, Birdsboro, PA 19508
Tel 610.374.2200
Fax 610.374.2201

DATE: **APRIL 2021**

DETAIL: **7000SD05**

NOTES:

1. THREE INLET PIPES AND ONE OUTLET PIPE MAX INTO MANHOLE.
2. NO LATERALS INTO MANHOLES, EXCEPT BY SPECIAL EXCEPTION FROM TOWNSHIP AUTHORITY.
3. MINIMUM 12" SEPARATION FROM EDGE OF PIPE PENETRATION TO EDGE OF PIPE PENETRATION.
4. MINIMUM CHANNEL DROP THROUGH MANHOLE:
STRAIGHT THROUGH – 0.1 FEET
BEND – 0.2 FEET
5. ALL CHANNELS SHALL BE PRECAST, UNLESS OTHERWISE SPECIFICALLY NOTED OR APPROVED.
6. THESE DETAILS APPLY TO PRECAST CHANNELS IN NEW MANHOLES, AND FIELD-FORMED CHANNELS IN EXISTING MANHOLES.
7. CHANNEL BENCH AT PIPE SHALL MATCH 3/4 CROWN ELEVATION OF PIPE AND RISE 1/2" PER FOOT TO THE MANHOLE WALLS.
8. CHANNELS SHALL MATCH THE CROSS-SECTIONAL DIMENSIONS OF THE PIPES ENTERING AND EXITING THE MANHOLE. SMOOTH TRANSITIONS SHALL BE PROVIDED BETWEEN CHANGES IN PIPE SIZE.

Upper Uwchlan Township Municipal Authority
STANDARD DETAIL – SEWER SYSTEM

REVISION	DATE
4	4/21
3	1/13
2	6/05
1	11/04

ARRO
Suite 200, 321 N. Furnace
Birdsboro, PA 19508
Tel 610.374.5100
Fax 610.374.5101

DATE: APRIL 2021

DETAIL: 7000SD06

PRECAST MANHOLE CHANNELS

SECTION

PIPE BEDDING

NOTES:

1. PROVIDE 6" OF AASHTO NO. 57 STONE BELOW PIPE AND 12" ABOVE PIPE (TYPICAL ENTIRE LENGTH OF LATERAL AND SERVICE LINE)
2. MINIMUM SLOPE = 1/8" PER FT.
3. MINIMUM DEPTH OF COVER = 4 FT
4. PIPE MATERIALS:
LATERAL SEWER - PVC SDR 35 OR DIP CLASS 52 WHEN INSTALLED IN FILL MATERIAL
SERVICE LINE - PVC SDR 35 OR SCH. 40
5. TO CONDUCT AIR TESTING OF SERVICE LINE, INSTALL EXPANDABLE PLUGS AT LOCATIONS SHOWN.
6. NO SEWER SERVICE LINE VENT CAPS SHALL BE INSTALLED WITHIN A 100-YR FLOOD PLAIN OR WITHIN FLOOD PRONE AREAS.
7. NO VENT CAPS OR CLEANOUTS SHALL BE INSTALLED IN DRIVEWAYS OR OTHER PAVED AREAS, UNLESS SPECIFICALLY APPROVED BY UUTMA. VENTS AND CLEANOUTS LOCATED IN PAVED AREAS MUST BE PROVIDED WITH A FRAME AND COVER IN ACCORDANCE WITH VENT & CLEANOUTS IN PAVED AREAS DETAIL.
8. THE LATERAL SEWER SHALL BE INSTALLED TO THE EDGE OF THE SEWER EASEMENT FOR SEWERS LOCATED IN AN EASEMENT OR TO THE STREET RIGHT-OF-WAY AS A MINIMUM, OR TO SUCH POINT, AS REQUIRED, TO CLEAR STREET SIDEWALKS AND UNDERGROUND UTILITIES.
9. CLEANOUTS AND VENTS IN LAWN AREAS MUST BE 6" MINIMUM ABOVE GRADE.

6	4/21	Upper Uwchlan Township Municipal Authority STANDARD DETAIL - SEWER SYSTEM	ARRO Suite 200, 321 N. Furnace St. Birdsboro, PA 19508 Tel 610.374.5285 Fax 610.374.5287
5	9/15		
4	10/14		
3	1/13		
2	6/05		
1	11/04		

REVISION	DATE	LATERAL & SERVICE LINE	
		DATE:	APRIL 2021
		DETAIL:	7000SD17

SECTION

SECTION

NOTES:

1. BACKFILL TRENCHES WITH EXCAVATED MATERIAL CONTAINING NO STONES LARGER THAN FOUR (4) INCHES IN ANY DIMENSION.
2. A MAXIMUM OF 20% OF BACKFILL CAN CONSIST OF STONES (NOTED ABOVE) IF EVENLY DISTRIBUTED THROUGHOUT BACKFILL.
3. BACKFILL MATERIAL SHALL BE FREE OF ORGANIC MATERIAL, REFUSE, AND FROZEN MATERIAL SUBJECT TO LIMITATIONS SPECIFIED.
4. BACKFILL SHALL BE PLACED IN 4" LOOSE LIFTS WITH THE USE OF A MECHANICAL TAMPER AND 8" LOOSE LIFTS WITH THE USE OF A VIBRATORY TAMPER.

**Upper Uwchlan Township
Municipal Authority**
STANDARD DETAIL – SEWER SYSTEM

ARRO
Suite 200, 321 N. Furnace
Birdsboro, PA 19508
Tel 610.374.5000
Fax 610.374.5001

**TRENCH RESTORATION
LAWN/FIELD AREAS**

DATE: APRIL 2021

DETAIL: 7000SD29

4	4/21
3	1/13
2	6/05
1	11/04
REVISION	DATE

NOTES:

1. FOR CLEANOUTS LOCATED IN YARDS, CAPS TO REMAIN ABOVE GRADE AT ALL TIMES DURING & AFTER INSTALLATION.
2. CLEANOUTS LOCATED IN PAVED AREAS MUST BE PROVIDED WITH FRAME AND COVER SIMILAR TO CAPS SHOWN ON DETAIL 7000SD19.

SECTION

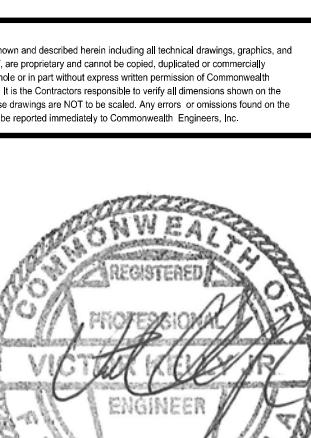
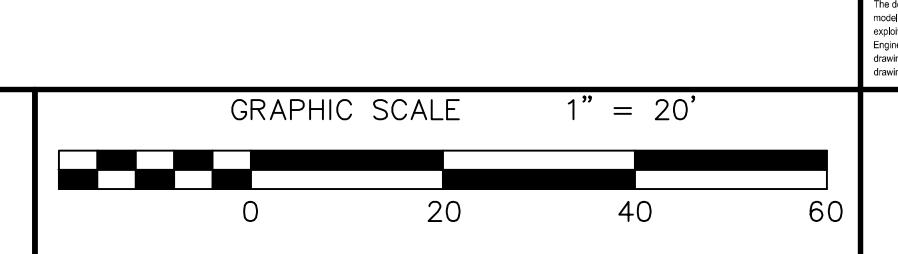
4	4/21	Upper Uwchlan Township Municipal Authority STANDARD DETAIL - SEWER SYSTEM	ARRC Suite 200, 321 N. Furnace St. Birdsboro, PA 19508 Tel 610.374.5285 Fax 610.374.5287
3	1/13		
2	6/05		
1	11/04		
REVISION	DATE	SERVICE LINE CLEANOUT & VENT	DATE: APRIL 2021
			DETAIL: 7000SD18



ATTENTION!

PROJECT IS IN A SPECIALLY PROTECTED HIGH
Y OR EXCEPTIONAL VALUE WATERSHED; EXTREME
E SHOULD BE EXERCISED IN ALL DISTURBANCE
ES TO PREVENT DEGRADATION TO THE WATERS OF
THE COMMONWEALTH.

SE THIS PROJECT IS IN A SPECIALLY PROTECTED
QUALITY OR EXCEPTIONAL VALUE WATERSHED, UPON
COMPLETION OR TEMPORARY CESSION OF EARTH
MOVEMENT ACTIVITIES, THE PROJECT SITE MUST BE
IMMEDIATELY STABILIZED WITH THE APPROPRIATE
TEMPORARY OR PERMANENT STABILIZATION



SANITARY SEWER DETAILS SHEET

FINAL LAND DEVELOPMENT

FINAL LAND DEVELOPMENT FOR

UPPER LIWCHI LAN TOWNSHIP

COMMONWEALTH ENGINEERS, INC.

114 EAST LANCASTER AVENUE ~ SECOND FLOOR
DOWNTOWNS PA 19335

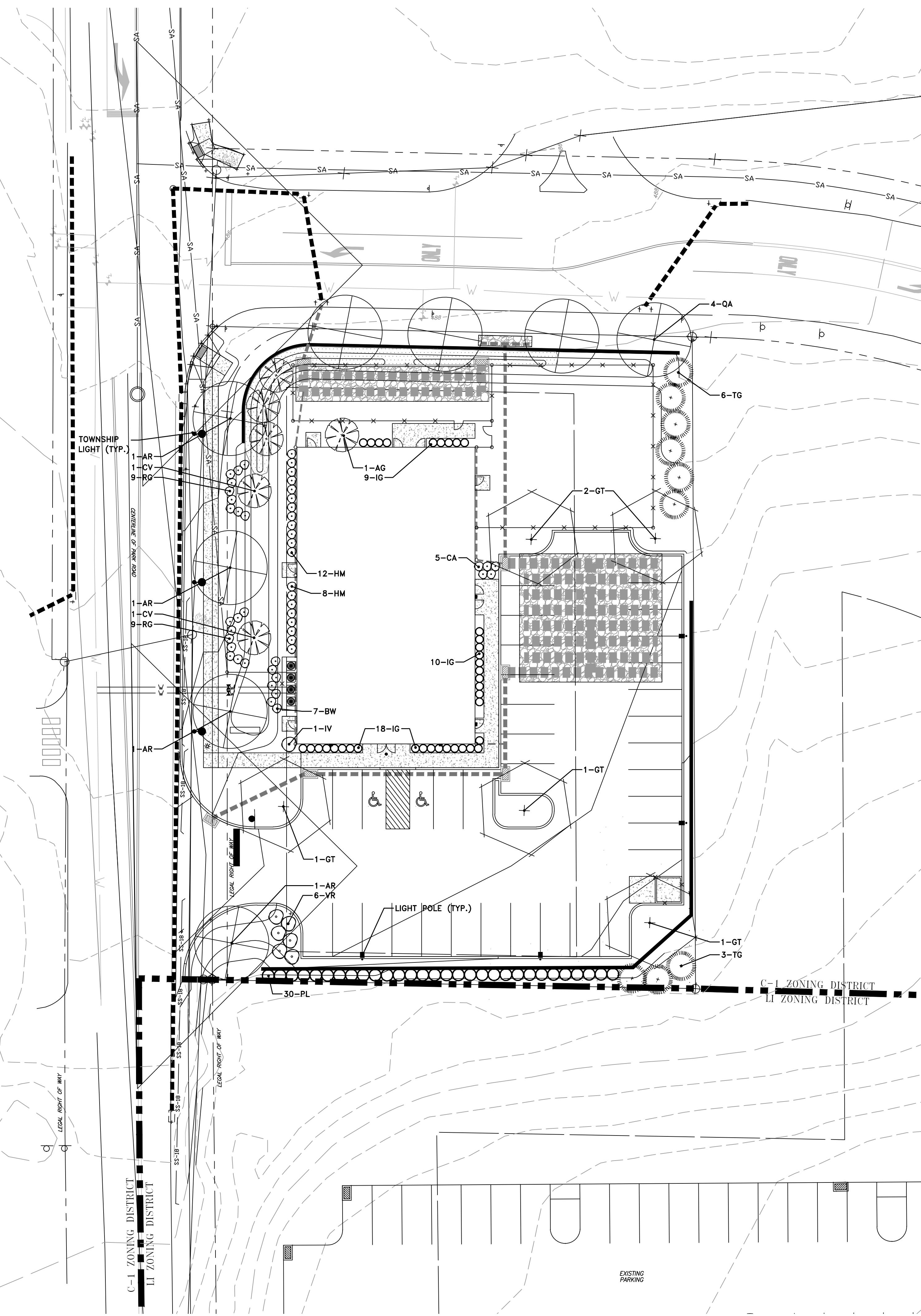
DOWNTOWN, PA 19335
TELEPHONE: (610) 518-9003 ~ FAX: (610) 518-9004
COMMERCIAL.COM

COMENG@CEI-1.COM

10 of 10 pages

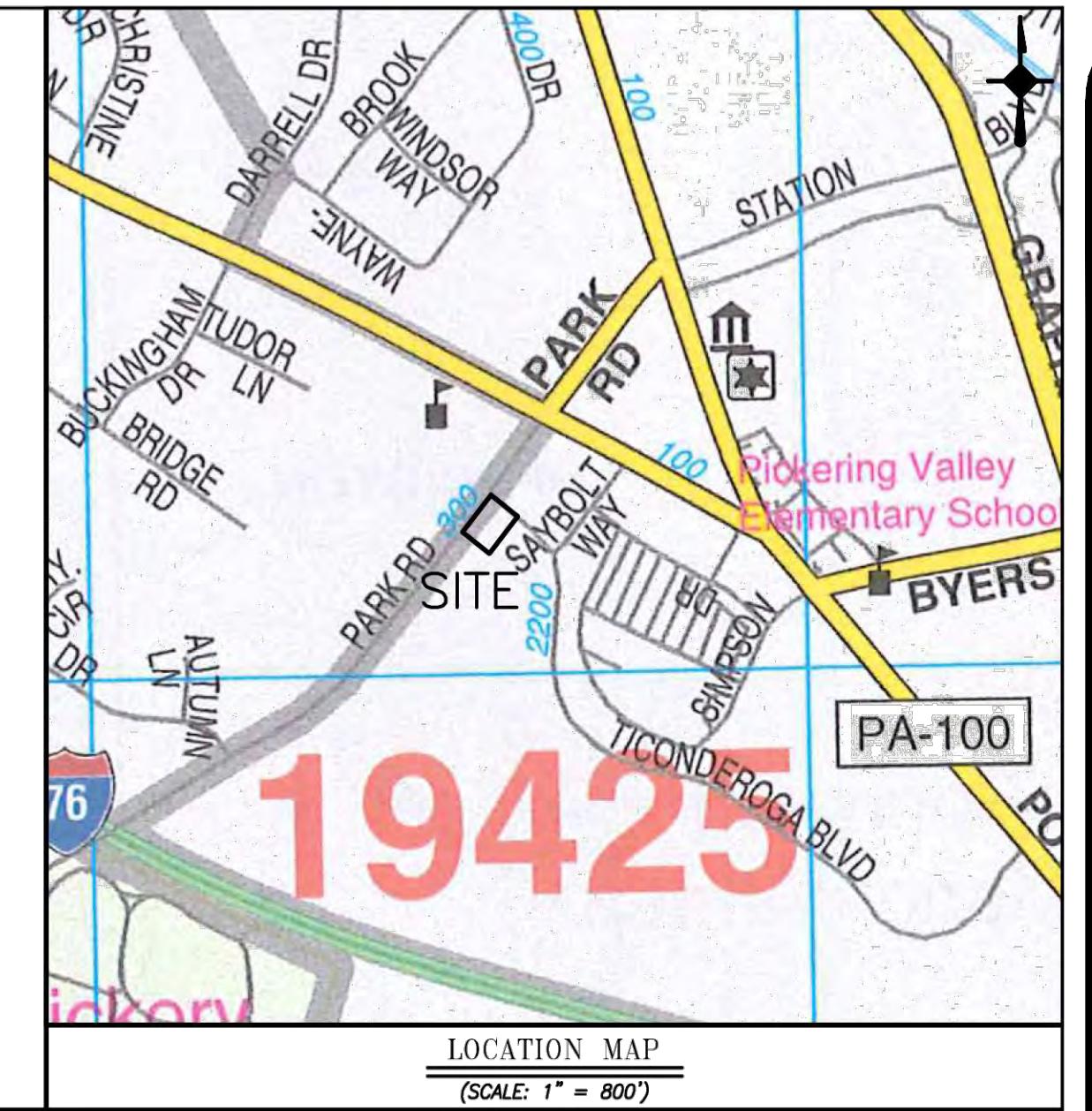
CHESTER COUNTY, PA	
E:	DECEMBER 4, 2023
LE:	AS SHOWN
RVEY:	---
WN:	MDC
CKED:	VK
WING NO.	202317-1(C)
SHEET: 6 OF: 16	

Landscape Plan



LEGEND:

- PRECAUTIONARY STEEP SLOPES (15% TO 25%)
- PROHIBITED STEEP SLOPES (OVER 25%)
- EXISTING PROPERTY BOUNDARY
- PROPERTY SETBACK LINE
- EXISTING INTERMEDIATE CONTOUR
- EXISTING INDEX CONTOUR
- EXISTING FLOODPLAIN LINE
- EXISTING 100-YEAR FLOODPLAIN
- STREAM LINE
- WOODS / TREE LINE
- SHRUB / HEDGE / GROVING LINE
- EXISTING STORM SEWER LINE
- EXISTING WATER LINE
- EXISTING OVERHEAD UTILITY LINES
- WELL ISOLATION LINE
- LEGAL RIGHT-OF-WAY LINE
- EXISTING EDGE-OF-PIVING
- EXISTING SPOT ELEVATION
- PROPOSED LIMIT OF DISTURBANCE
- NPDES PROJECT SITE AREA



PLANT SCHEDULE								
Plan Symbol	Quantity	Botanical Name	Common Name	Min. Planting Caliper	Min. Planting Spread	Min. Planting Height	Remarks	Comments
Deciduous Trees								
AR	4	Acer rubrum 'Bowhall'	Bowhall Red Maple	3-3.5"	-	14'-16'	B&B	Full, straight leader
GT	5	Gleditsia triacanthos var. inermis	Thornless Honeylocust	3-3.5"	-	14'-16'	B&B	Full, straight leader
QA	4	Quercus alba	White Oak	3-3.5"	-	14'-16'	B&B	Full, straight leader
Evergreen Tree								
TG	9	Thuja 'Green Giant'	Green Giant Arborvitae	-	-	7'-8'	B&B	Symmetrical, branched to ground
Ornamental Trees								
AG	4	Amelanchier x grandiflora 'Autumn Brilliance'	Autumn Brilliance Serviceberry	-	-	8"	B&B	Multi-stem, min. 5 stems
CV	2	Chionanthus virginicus	Fringe Tree	-	-	8"	B&B	Multi-stem, min. 5 stems
Shrubs **								
BU	7	Buxus 'Winter Gem'	Winter Gem Boxwood	-	-	24-30"	CONT	Heavy, full specimen
CA	5	Citharexylum 'Ruby Spic'	Ruby Spic Summersweet	-	-	24-30"	CONT	Heavy, full specimen
HM	20	Hydrangea macrophylla 'Endless Summer Bloomstruck'	15 Bloomstruck Hydrangea	-	-	24-30"	CONT	Heavy, full specimen
IG	37	Ilex glabra 'Gem Box'	Gem Box Hollyberry	-	-	24-30"	CONT	Heavy, full specimen
IV	1	Ilex virginica	Sweetspire	-	-	24-30"	CONT	Heavy, full specimen
PL	30	Prunus laurocerasus 'Schipkaensis'	Skip Laurel	-	-	36"	CONT	Heavy, full specimen
RG	18	Rhus 'Gro Low'	Gro Low Sumac	-	18-24"	*	CONT	Heavy, full specimen
VR	6	Viburnum rhytidophyllum	Leatherleaf Viburnum	-	-	24-30"	CONT	Heavy, full specimen

** Plant in continuous mulch bed (2-3" depth) until fully established.

* Spreading variety

SWALE AREA AT NORTHWEST PROPERTY CORNER
SEED IN ERNST SEED MIX (ERNMX-181-1); RAIN GARDEN GRASS MIX
SEEDING RATE IS 15 LBS. PER ACRE WITH A COVER CROP

SWALE AREA = 84 SF

(340/43560) X 15 = .30 LBS. ERNMX-181-1

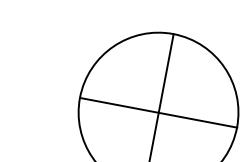
FOR AREA THAT DRAINS WITHIN 24 HOURS OF A RAIN EVENT CHOOSE ONE OF THE FOLLOWING COVER CROPS:

OATS (JAN. THRU JULY); 30 LBS./ACRE

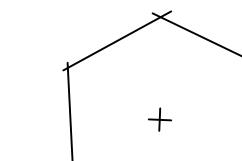
JAPANESE MILLET (MAY THROUGH AUG.); 10 LBS./ACRE

GRAIN RYE (AUG. THRU DEC.); 30 LBS./ACRE

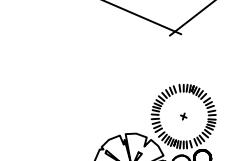
LANDSCAPE LEGEND



Street tree



Parking lot landscape



Minimum Required Planting

See Sheet LP-2 for Compliance Chart & Landscape Details

General Notes																
																
<table border="1"> <tr> <td>4</td> <td>PER TWP COMMENTS</td> <td>7/26/24</td> </tr> <tr> <td>3</td> <td>PER TWP COMMENTS</td> <td>6/25/24</td> </tr> <tr> <td>2</td> <td>PER TWP COMMENTS</td> <td>5/28/24</td> </tr> <tr> <td>1</td> <td>PER TWP COMMENTS</td> <td>3/7/24</td> </tr> <tr> <td>No.</td> <td>Revision/Issue</td> <td>Date</td> </tr> </table>		4	PER TWP COMMENTS	7/26/24	3	PER TWP COMMENTS	6/25/24	2	PER TWP COMMENTS	5/28/24	1	PER TWP COMMENTS	3/7/24	No.	Revision/Issue	Date
4	PER TWP COMMENTS	7/26/24														
3	PER TWP COMMENTS	6/25/24														
2	PER TWP COMMENTS	5/28/24														
1	PER TWP COMMENTS	3/7/24														
No.	Revision/Issue	Date														
Firm Name and Address																
 InFocus Planning EFFECTIVE. EFFICIENT. LAND DEVELOPMENT SERVICES 1121 N. BETHLEHEM PIKE SUITE 60 #206 SPRING HOUSE, PA 19476 P: 215-759-2540 www.infocusplanning.com																
Project Name and Address																
241 PARK ROAD UPPER UWCHLAN TOWNSHIP CHESTER COUNTY, PA																
Project	Sheet															
InFocus_23-69	LP-1	9 of 16														
Date	11/27/23															
Scale	As Noted															

Landscape & Lighting Details

General Notes	
	
Firm Name and Address	
InFocus Planning EFFECTIVE. EFFICIENT. LAND DEVELOPMENT SERVICES 1121 N. BETHLEHEM PIKE SUITE 60 #206 SPRING HOUSE, PA 19477 P: 215-759-2540 www.infocusplanning.com	
Project Name and Address	
241 PARK ROAD UPPER UWCHLAN TOWNSHIP CHESTER COUNTY, PA	
Sheet	
10 of 16	

GENERAL LANDSCAPE NOTES

1. All plant material shall meet the standards of the American Standard for Nursery Stock published by the Americanfort (2014), or most recent edition, and the height, spread and/or caliper for trees and shrubs listed in Table Section 300-47, Upper Moreland Recommended Plant List.

2. All plant material shall be installed in accordance with the planting practices stated in Chapter 3 of Tree Maintenance by P.P. Pirone (Fifth or most recent edition).

3. All stakes are to be installed for a period of 12 months and are to be removed prior to the End of Guarantee. Stakes for deciduous trees are to be vertical and three (3) stakes are to be provided for all trees.

4. All planter islands are to be crowned to a height of eight (8) inches above the average top of curb height.

5. A permanent seeding specification is on the Erosion Control Details (Sheet 8 of 14), refer to this sheet for details.

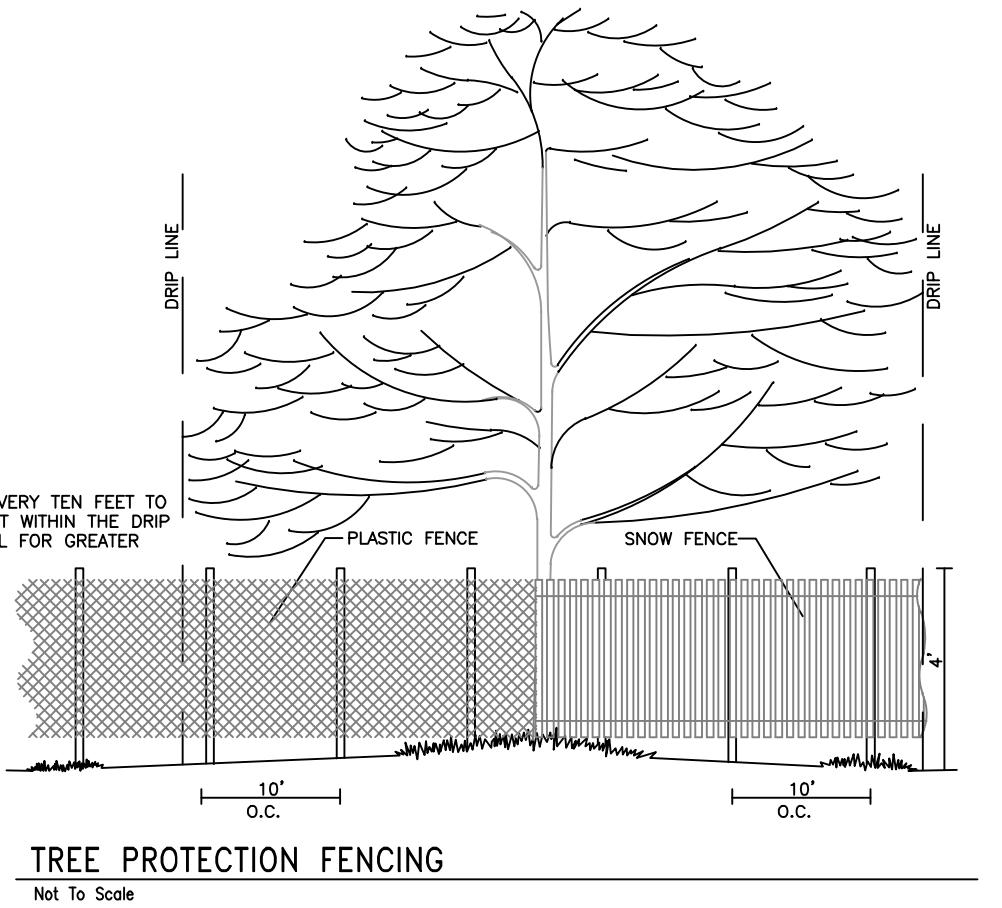
6. The Project Landscape Architect is to review all plant substitutions and submit them to the Township Landscape Architect for review prior to installation.

7. All plant material shall be guaranteed for 18 months from the day of final approval of the landscape installation by the Township Landscape Architect or the Township Engineer. Any plant material 25% or more of which is dead shall be considered dead. A tree shall be considered dead when the main leader has died or 25% of the crown is dead. Any dead plant material shall be replaced and installed according to the approved planting practices.

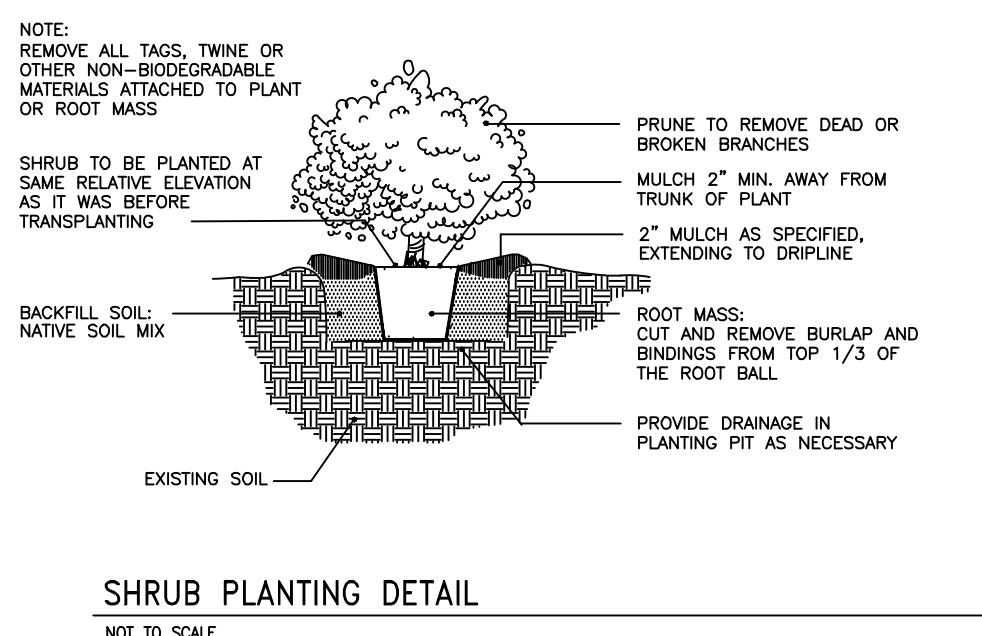
8. The Applicant shall contact the Township in writing to request a final inspection for acceptance at the end of the guarantee period. These inspections will be performed when plant materials are in full leaf only. (May 1 through November 15). All guarantee escrow funds will be released upon acceptance at the end of the guarantee period. The guarantee period will be extended until 30 days after the receipt of the request letter following May 1. Should the end of the guarantee period occur after November 15, the guarantee period shall be extended to May 15.

9. The Township reserves the right to require additional landscape buffer plantings, following substantial completion of construction, should vegetation to be preserved not be preserved or not otherwise be as represented on the Final Landscape Plan(s).

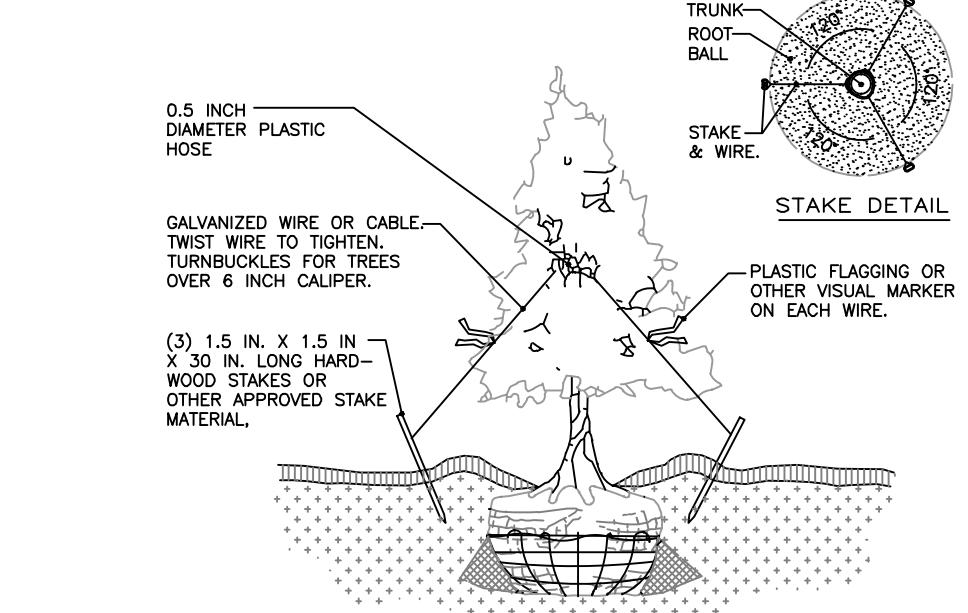
10. All required plant material shall be planted prior to the issuance of a use and occupancy permit.



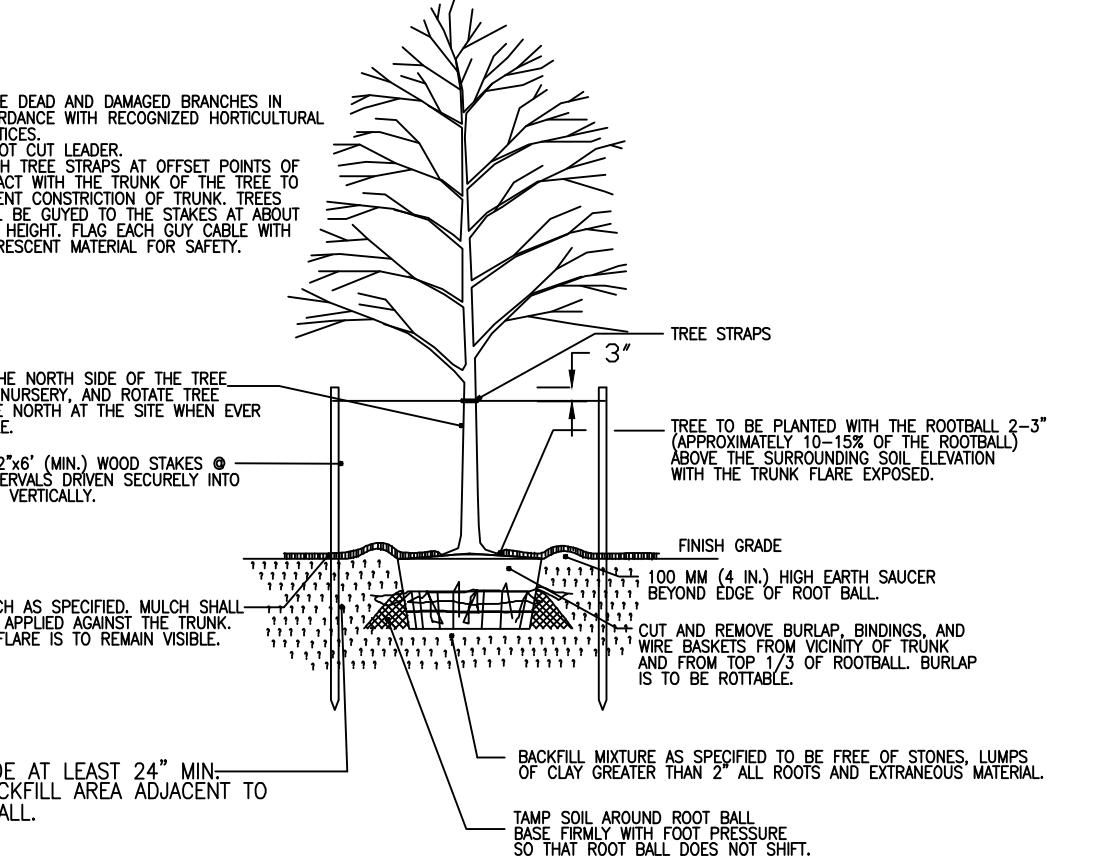
TREE PROTECTION FENCING
Not to Scale



SHRUB PLANTING DETAIL
Not to Scale



EVERGREEN TREE STAKING DETAIL
Not to Scale



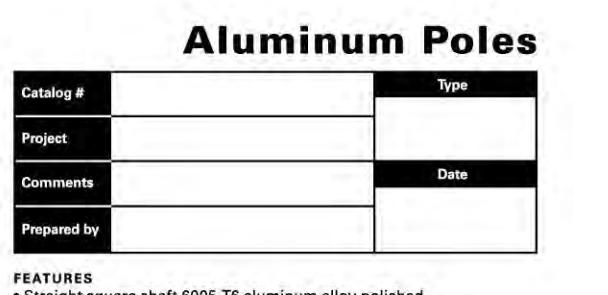
ORNAMENTAL AND SHADE TREE PLANTING/ STAKING DETAIL
Not to Scale

LANDSCAPE REQUIREMENTS CHART - UPPER UWCHLAN TOWNSHIP

Ordinance Item	Requirement	Plan Proposed
S.O. 162-55 Replacement tree standards	For each tree greater than 6" dbh removed, replacement trees shall be planted. Refer to Tree Inventory chart for calculation Total of 4 Replacement trees are required.	0 Shade trees [W1]
S.O. 162-57.C.(6) Street trees	Any subdivision or land development shall provide street trees along the entire length of any public street. Spacing between trees shall be no greater than 50'.* Ticonderoga Blvd.: 162' 162/50 = 4 trees	4 Shade tree
	Park Rd.: 220' - 34' (driveway opening) = 186' 186/50 = 4 trees	4 Shade trees
S.O. 162-57.C.(7)	All off-street parking areas (except for single-family residences) shall be landscaped with trees and shrubs of varying species. One shade tree for every 5 parking spaces 39 parking spaces / 5 = 8 shade trees	5 Shade trees [W2]
S.O. 162-57.D. Minimum planting standards	Per 1,000 sf gross area (ground floor only): 2 deciduous trees, 1 evergreen tree, 8 shrubs Building: 6,000 sf (6000/1000) x 8 = 12 deciduous trees 6000/1000 = 6 evergreen trees (6000/1000) x 8 = 48 shrubs	4 Ornamental trees [W3] 4 Evergreen trees [W3] 48 Shrubs
	Per 2,000 sf off-street parking or loading area (excluding driveways less than 18 feet wide): 1 deciduous tree, 1 evergreen tree, 4 shrubs Parking/ Loading: 13,685 sf 13685/2000 = 7 deciduous trees 13685/2000 = 7 evergreen trees (13685/2000) x 4 = 28 shrubs	0 Deciduous trees [W3] 0 Evergreen trees [W3] 28 Shrubs
	Per 100 linear feet of new & existing public or private road frontage: 2 deciduous trees, 1 evergreen tree, 5 shrubs Along Park Rd. & Ticonderoga Blvd: 327' (327/100) x 2 = 7 deciduous trees 327/100 = 4 evergreen trees (327/100) x 5 = 178 shrubs	2 Ornamental trees [W3] 3 Evergreen trees [W3] 18 Shrubs [W3] + proposed wall
	Per 100 linear feet of existing tract boundary, where not coincident with existing or proposed road frontage: 1 deciduous tree, 2 evergreen trees, 8 shrubs Eastern & Southern property lines: 395' 395/100 = 4 deciduous trees (395/100) x 2 = 8 evergreen trees (395/100) x 8 = 32 shrubs	0 Deciduous trees [W3] 0 Evergreen trees [W3] 30 Shrubs [W3] + proposed wall
	Total Landscape Proposed	13 Shade trees 6 Ornamental trees 9 Evergreen trees 124 Shrubs

* Street trees shall be at least 14-16' in height and 3-3.5" caliper in size.

[W1],[W2],[W3] Waiver requested due to spatial constraints.



Aluminum Poles

Catalog #	Type
Project	
Comments	Date

Prepared by

Not to Scale

Initial Delivered Luminaire

CPY250F LED Canopy/Soffit Luminaire

Rev Date 7/5/2023

Product Description

The CPY250F LED Canopy/Soffit Luminaire has an extremely low profile constructed of rugged cast aluminum. It can be surface mounted easily below the canopy deck and can be pendant mounted. Direct mounting is available for recessed canopies, soffit, and flat panel applications. It is available in 120V or 110V 120w-glass-lens.

Prepared by

Not to Scale

Accessories

Flat-mounted

Direct Mount Luminaire

Canopy Mount, surface mount, recessed mount, pendant mount

CPY250F - For use with Canopy Mount, 120V

CPY250F - For use with Direct Mount, 120V

CPY250F - For use with Recessed Mount, 120V

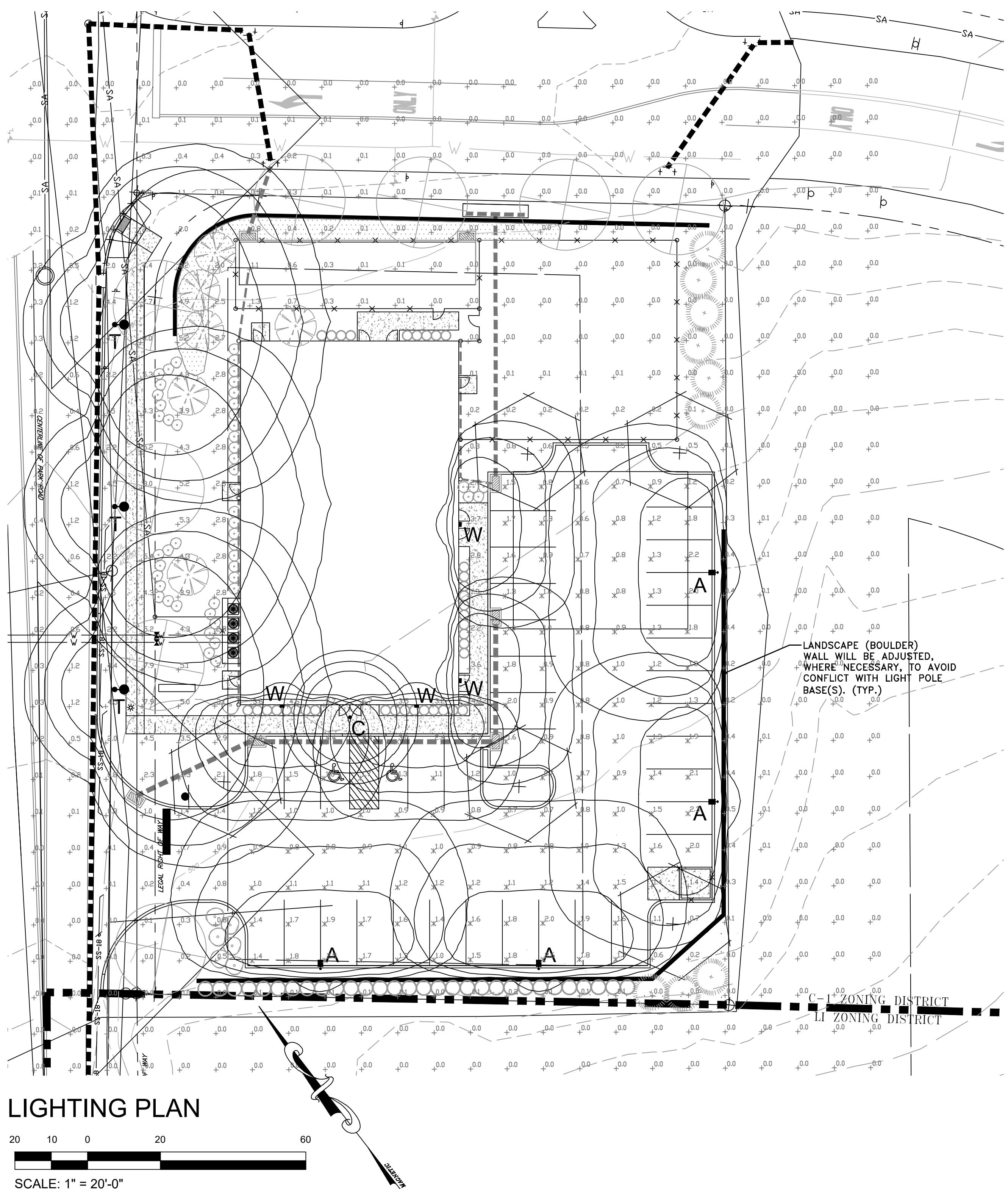
CPY250F - For use with Pendant Mount, 120V

CPY250F - For use with Flat Mount, 120V

CPY250F - For use with Drop Lens, 120V

CPY250F - For use with Drop Lens, 110V

Lighting Plan



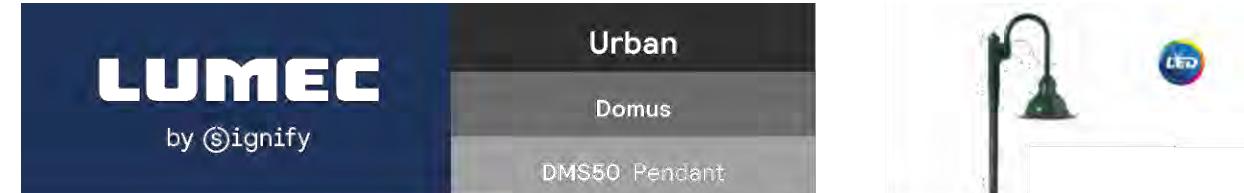
LUMINAIRE SCHEDULE						
Symbol	Label	Qty	Catalog Number	Description	File	Lumens per Lamp
■	A	4	GALN-SA1A-730-U-SL4-HSS	Cooper Lighting Solutions - McGraw-Edison Galleon Area and Wall Mount Luminaire (1) 70 CRI, 3000K, 615mA Light, 16 LEDs and Type IV Spill Light, Eliminator Optics with House Side Shield	GALN-SA1A-730-U-SL4-HSS_3466.lumens.ies	Absolute 1.0 33.0 16'
■	W	4	CCW-SA-1A-730-U-SL4-HSS	Cooper Lighting Solutions - Clear Curve Wall Pack MOUNT Light Source Luminaire @350mA w/ SL4 distribution lens and House Side Shield	CCW-SA-1A-730-U-SL4-HSS_2057lumens.ies	Absolute 1.0 18.0 10'
●	C	1	NPCPY250-C-2L-30K-F-UL-xx-xx-xxx	CYP250 Canopy Luminaire, 2L Lumens Package, 3000K, 80 CRI, Flat Lens, 120-277V	CYP250-C-2L-30K-F-UL-xx-xx-xxx_PL16375-001A.ies	Absolute 1.0 13.5 10'
●	T	3	DMS55-80W48LED3K-G3-LE4S	Domus LED pendant - large (DMS55, 48 LED's, 3000K CCT, type LE4S Optic, Sag Glass Lens	DMS55-80W48LED3K-LE4S.ies	Absolute 1.0 80.0 15'

NOTES:
LIGHTS SHALL BE EXTINGUISHED BETWEEN 11PM AND DAWN, OR TO ONLY HAVE LIGHTING ON WHEN MOTION IS DETECTED.
TIME CLOCKS SHALL HAVE ASTRONOMIC DIALS THAT TRACK DAYLIGHT HOURS SHIFTS, AND CAPACITOR OR BATTERY BACKUP TO PRESERVE CLOCK SETTINGS IN THE EVENT OF A POWER OUTAGE.

STATISTICS						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Main Parking Lot	✖	1.4 fc	5.4 fc	0.6 fc	9.0:1	2.3:1
Internal Sidewalk	◇	3.7 fc	8.3 fc	1.3 fc	6.4:1	2.8:1
Main Entrance	□	7.3 fc	8.3 fc	5.9 fc	1.4:1	1.2:1

ARCHITECTURAL GOOSENECK LIGHT NOT INCLUDED IN SCHEDULE OR PHOTOMETRIC INFORMATION.
TO BE CONFIRMED WITH ARCHITECT.

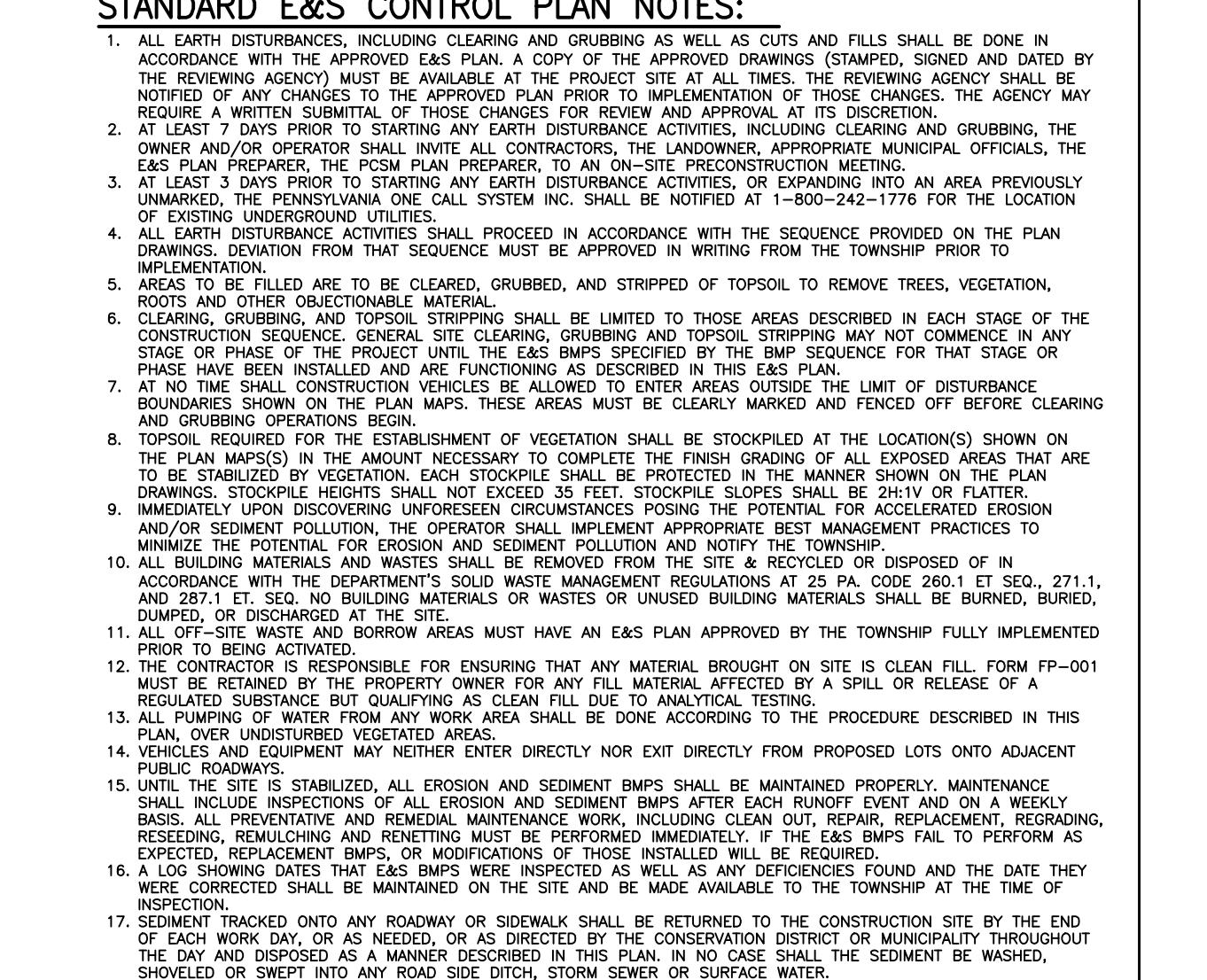
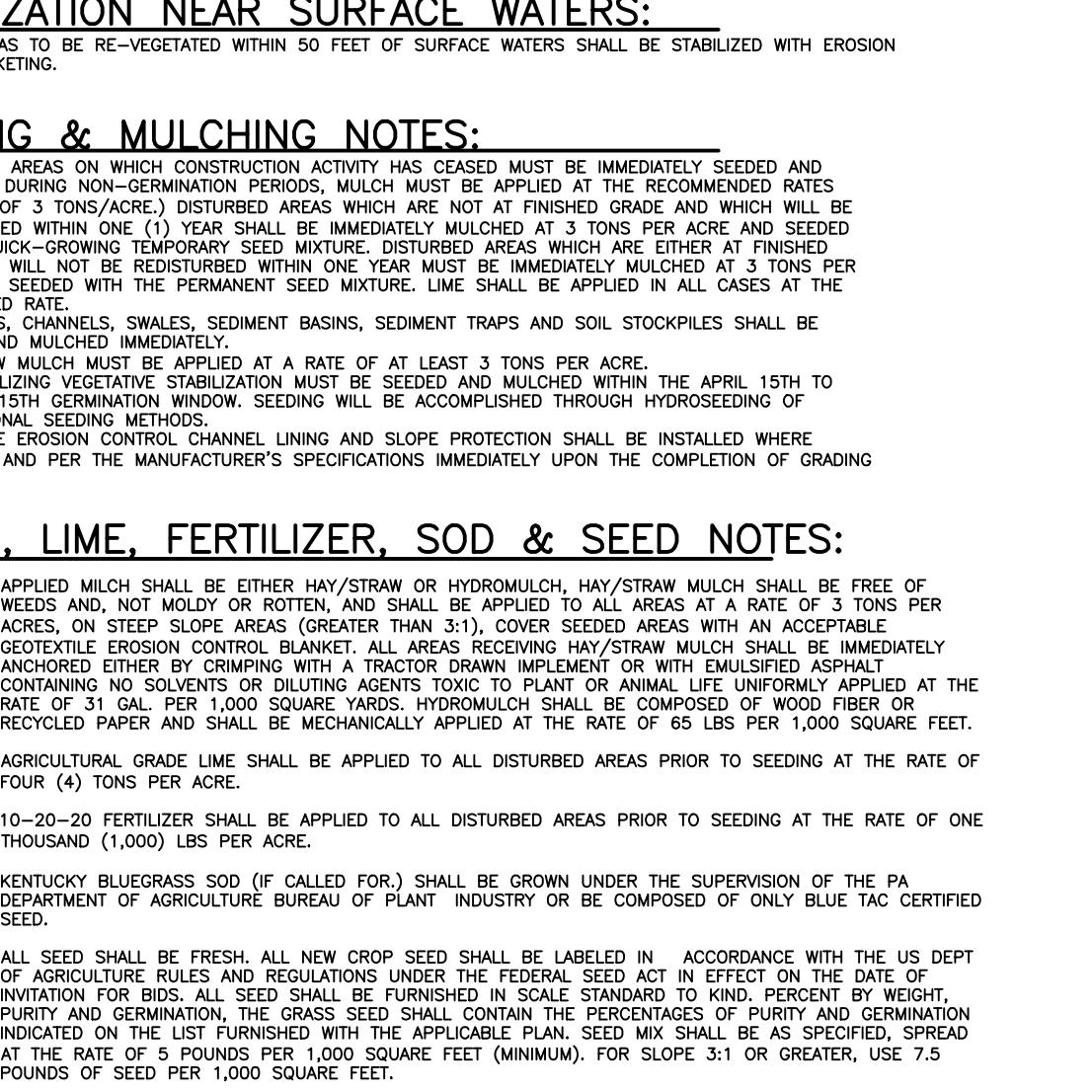
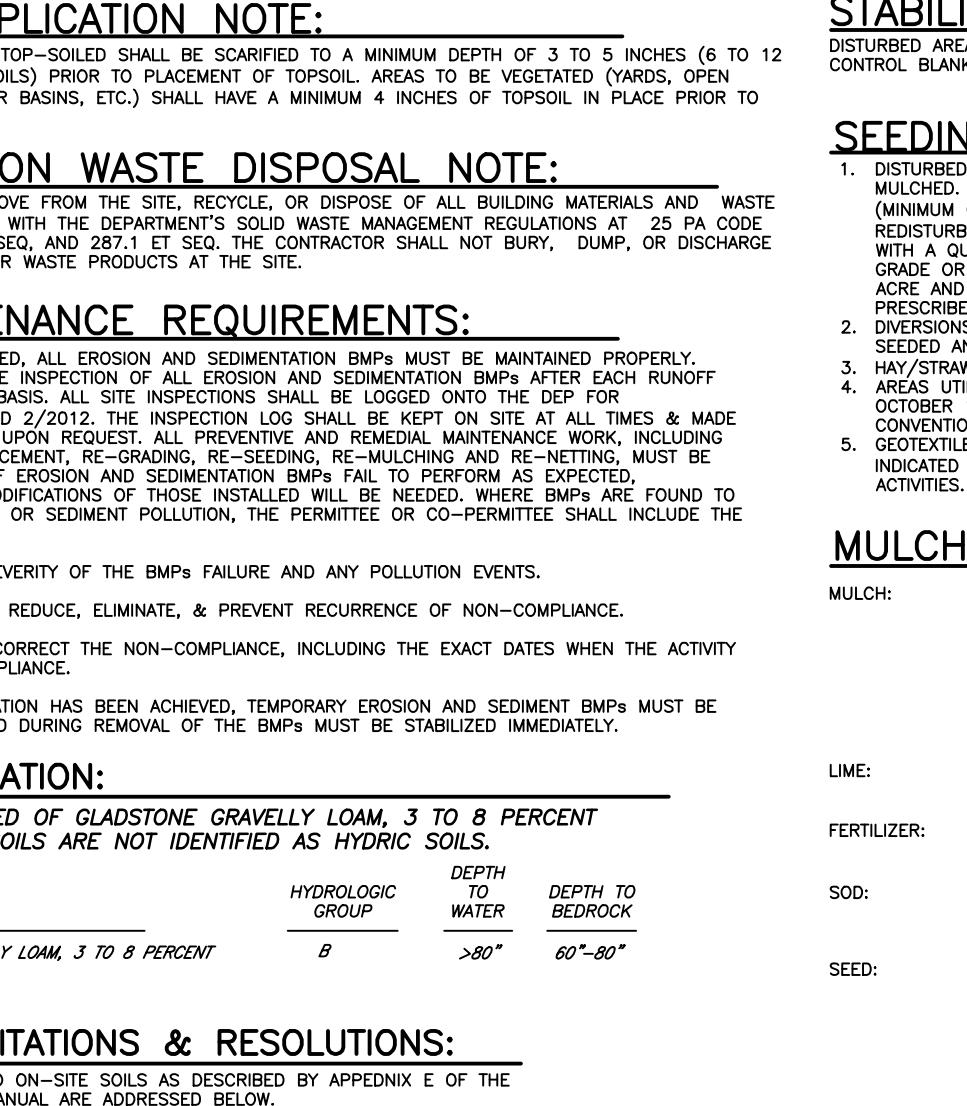
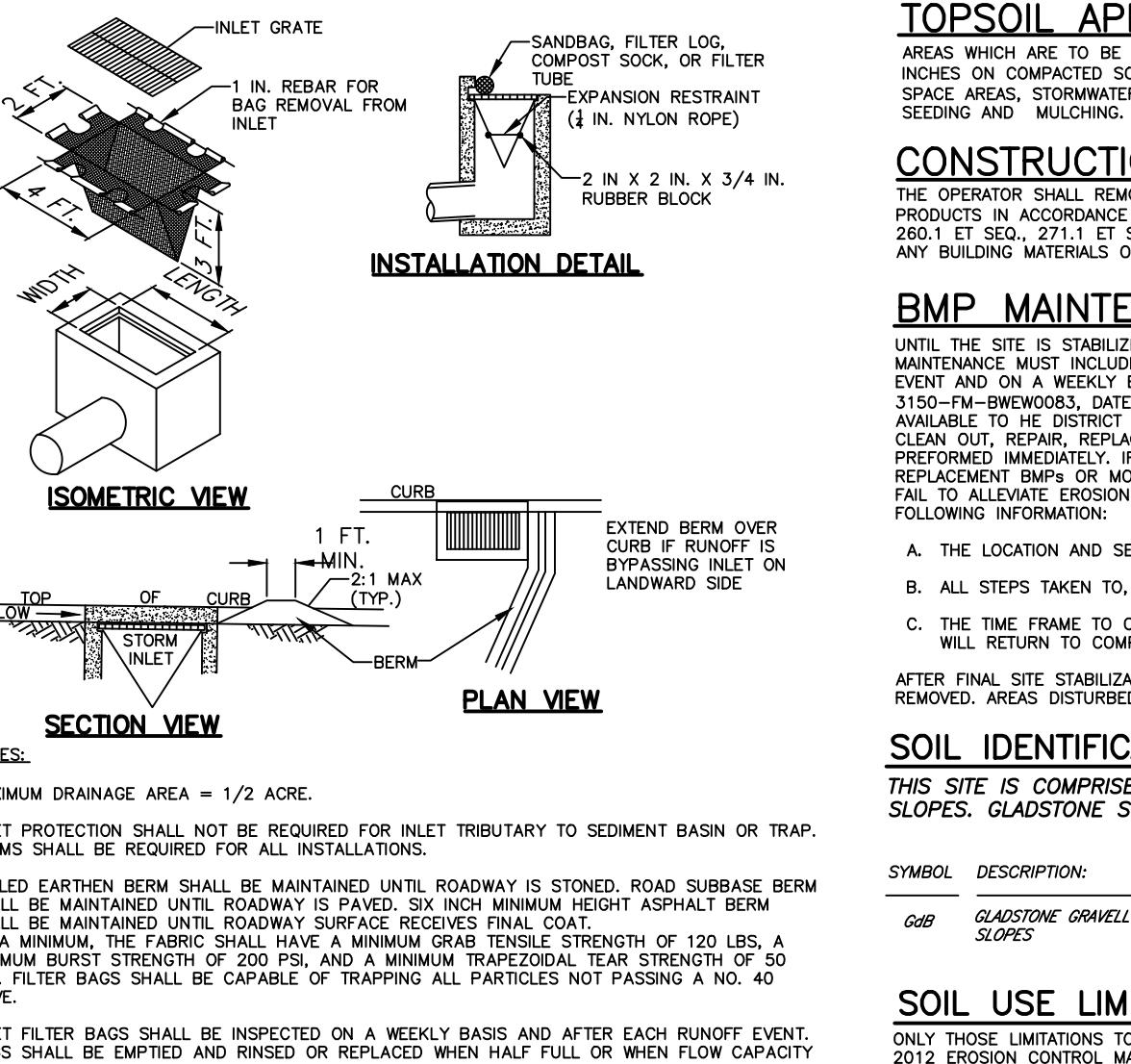
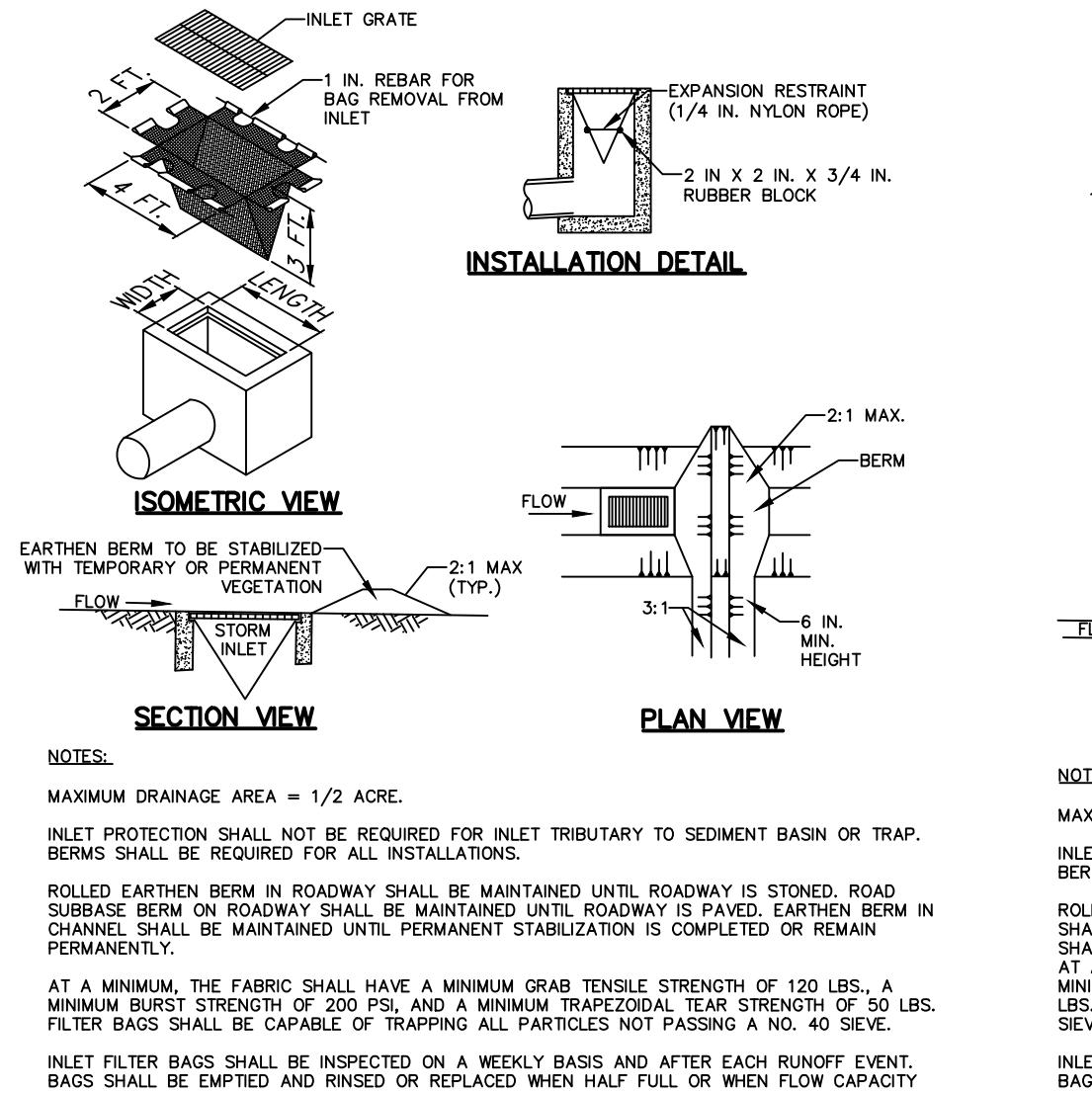
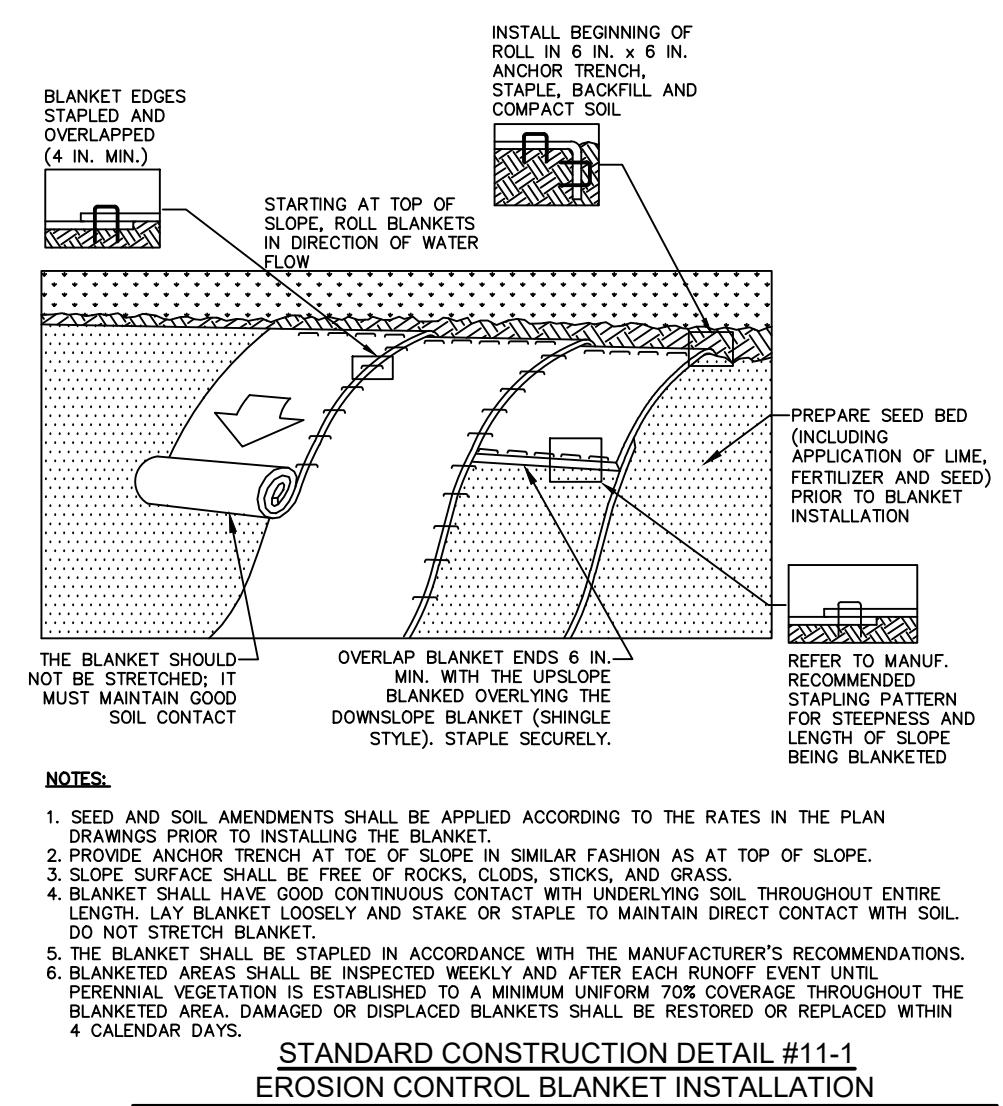
McGraw-Edison GALN Galleon II									
Project	Catalog #	Type	Prepared by	Notes	Date	Light Engine	Color Temperature	Voltage	Distribution
McGraw-Edison GALN Galleon II									
Ordering Information									
SAMPLE NUMBER: GALN-SAC-740-U-T4FT-GM									
Product Features									
• Light Engine: 100W LED, 3000K, 5000K, 6500K, 8000K, 9000K, 10000K, 12000K, 14000K, 16000K, 18000K, 20000K, 22000K, 24000K, 26000K, 28000K, 30000K, 32000K, 34000K, 36000K, 38000K, 40000K, 42000K, 44000K, 46000K, 48000K, 50000K, 52000K, 54000K, 56000K, 58000K, 60000K, 62000K, 64000K, 66000K, 68000K, 70000K, 72000K, 74000K, 76000K, 78000K, 80000K, 82000K, 84000K, 86000K, 88000K, 90000K, 92000K, 94000K, 96000K, 98000K, 100000K, 102000K, 104000K, 106000K, 108000K, 110000K, 112000K, 114000K, 116000K, 118000K, 120000K, 122000K, 124000K, 126000K, 128000K, 130000K, 132000K, 134000K, 136000K, 138000K, 140000K, 142000K, 144000K, 146000K, 148000K, 150000K, 152000K, 154000K, 156000K, 158000K, 160000K, 162000K, 164000K, 166000K, 168000K, 170000K, 172000K, 174000K, 176000K, 178000K, 180000K, 182000K, 184000K, 186000K, 188000K, 190000K, 192000K, 194000K, 196000K, 198000K, 200000K, 202000K, 204000K, 206000K, 208000K, 210000K, 212000K, 214000K, 216000K, 218000K, 220000K, 222000K, 224000K, 226000K, 228000K, 230000K, 232000K, 234000K, 236000K, 238000K, 240000K, 242000K, 244000K, 246000K, 248000K, 250000K, 252000K, 254000K, 256000K, 258000K, 260000K, 262000K, 264000K, 266000K, 268000K, 270000K, 272000K, 274000K, 276000K, 278000K, 280000K, 282000K, 284000K, 286000K, 288000K, 290000K, 292000K, 294000K, 296000K, 298000K, 300000K, 302000K, 304000K, 306000K, 308000K, 310000K, 312000K, 314000K, 316000K, 318000K, 320000K, 322000K, 324000K, 326000K, 328000K, 330000K, 332000K, 334000K, 336000K, 338000K, 340000K, 342000K, 344000K, 346000K, 348000K, 350000K, 352000K, 354000K, 356000K, 358000K, 360000K, 362000K, 364000K, 366000K, 368000K, 370000K, 372000K, 374000K, 376000K, 378000K, 380000K, 382000K, 384000K, 386000K, 388000K, 390000K, 392000K, 394000K, 396000K, 398000K, 400000K, 402000K, 404000K, 406000K, 408000K, 410000K, 412000K, 414000K, 416000K, 418000K, 420000K, 422000K, 424000K, 426000K, 428000K, 430000K, 432000K, 434000K, 436000K, 438000K, 440000K, 442000K, 444000K, 446000K, 448000K, 450000K, 452000K, 454000K, 456000K, 458000K, 460000K, 462000K, 464000K, 466000K, 468000K, 470000K, 472000K, 474000K, 476000K, 478000K, 480000K, 482000K, 484000K, 486000K, 488000K, 490000K, 492000K, 494000K, 496000K, 498000K, 500000K, 502000K, 504000K, 506000K, 508000K, 510000K, 512000K, 514000K, 516000K, 518000K, 520000K, 522000K, 524000K, 526000K, 528000K, 530000K, 532000K, 534000K, 536000K, 538000K, 540000K, 542000K, 544000K, 546000K, 548000K, 550000K, 552000K, 554000K, 556000K, 558000K, 560000K, 562000K, 564000K, 566000K, 568000K, 570000K, 572000K, 574000K, 576000K, 578000K, 580000K, 582000K, 584000K, 586000K, 588000K, 590000K, 592000K, 594000K, 596000K, 598000K, 600000K, 602000K, 604000K, 606000K, 608000K, 610000K, 612000K, 614000K, 616000K, 618000K, 620000K, 622000K, 624000K, 626000K, 628000K, 630000K, 632000K, 634000K, 636000K, 638000K, 640000K, 642000K, 644000K, 646000K, 648000K, 650000K, 652000K, 654000K, 656000K, 658000K, 660000K, 662000K, 664000K, 666000K, 668000K, 670000K, 672000K, 674000K, 676000K, 678000K, 680000K, 682000K, 684000K, 686000K, 688000K, 690000K, 692000K, 694000K, 696000K, 698000K, 700000K, 702000K, 704000K, 706000K, 708000K, 710000K, 712000K, 714000K, 716000K, 718000K, 720000K, 722000K, 724000K, 726000K, 728000K, 730000K, 732000K, 734000K, 736000K, 738000K, 740000K, 742000K, 744000K, 746000K, 748000K, 750000K, 752000K, 754000K, 756000K, 758000K, 760000K, 762000K, 764000K, 766000K, 768000K, 770000K, 772000K, 774000K, 776000K, 778000K, 780000K, 782000K, 784000K, 786000K, 788000K, 790000K, 792000K, 794000K, 796000K, 798000K, 800000K, 802000K, 804000K, 806000K, 808000K, 810000K, 812000K, 814000K, 816000K, 818000K, 820000K, 822000K, 824000K, 826000K, 828000K, 830000K, 832000K, 834000K, 836000K, 838000K, 840000K, 842000K, 844000K, 846000K, 848000K, 850000K, 852000K, 854000K, 856000K, 858000K, 860000K, 862000K, 864000K, 866000K, 868000K, 870000K, 872000K, 874000K, 876000K, 878000K, 880000K, 882000K, 884000K, 886000K, 888000K, 890000K, 892000K, 894000K, 896000K, 898000K, 900000K, 902000K, 904000K, 906000K, 908000K, 910000K, 912000K, 914000K, 916000K, 918000K, 920000K, 922000K, 924000K, 926000K, 928000K, 930000K, 932000K, 934000K, 936000K, 938000K, 940000K, 942000K, 944000K, 946000K, 948000K, 950000K, 952000K, 954000K, 956000K, 958000K, 960000K, 962000K, 964000K, 966000K, 968000K, 970000K, 972000K, 974000K, 976000K, 978000K, 980000K, 982000K, 984000K, 986000K, 988000K, 990000K, 992000K, 994000K, 996000K, 998000K, 1000000K, 1002000K, 1004000K, 1006000K, 1008000K, 1010000K, 1012000K, 1014000K, 1016000K, 1018000K, 1020000K, 1022000K, 1024000K, 1026000K, 1028000K, 1030000K, 1032000K, 1034000K, 1036000K, 1038000K, 1040000K, 1042000K, 1044000K, 1046000K, 1048000K, 1050000K, 1052000K, 1054000K, 1056000K, 1058000K, 1060000K, 1062000K, 1064000K, 1066000K, 1068000K, 1070000K, 1072000K, 1074000K, 1076000K, 1078000K, 1080000K, 10820									



Domus 50 is one of the most versatile luminaires offered by Lumec. This classic shape was one of the first in a line of pioneering Lumec designs. Domus offers a subtly refined design that balances shape, dimension and proportion.

Ordering guide

Series	LED module	Gen	Globe material	Optical system	Voltage	Driver options
DMS50	600K	3000K	G3	Globe	120V	ACR
DMS50	Domus				208-240V	CLD
SWW10LEDK	10W	10W		LE3M	120V	CLD
SWW12LEDK	12W	12W		LE3M	120V	CLD
SWW14LEDK	14W	14W		LE3M	120V	CLD
SWW16LEDK	16W	16W		LE3M	120V	CLD
SWW18LEDK	18W	18W		LE3M	120V	CLD
SWW20LEDK	20W	20W		LE3M	120V	CLD
SWW24LEDK	24W	24W		LE3M	120V	CLD
SWW28LEDK	28W	28W		LE3M	120V	CLD
SWW32LEDK	32W	32W		LE3M	120V	CLD
SWW36LEDK	36W	36W		LE3M	120V	CLD
SWW40LEDK	40W	40W		LE3M	120V	CLD
SWW48LEDK	48W	48W		LE3M	120V	CLD
SWW50LEDK	50W	50W		LE3M	120V	CLD
SWW60LEDK	60W	60W		LE3M	120V	CLD
SWW70LEDK	70W	70W		LE3M	120V	CLD
SWW80LEDK	80W	80W		LE3M	120V	CLD
SWW100LEDK	100W	100W		LE3M	120V	CLD
SWW120LEDK	120W	120W		LE3M	120V	CLD
SWW140LEDK	140W	140W		LE3M	120V	CLD
SWW160LEDK	160W	160W		LE3M	120V	CLD
SWW180LEDK	180W	180W		LE3M	120V	CLD
SWW200LEDK	200W	200W		LE3M	120V	CLD
SWW240LEDK	240W	240W		LE3M	120V	CLD
SWW280LEDK	280W	280W		LE3M	120V	CLD
SWW320LEDK	320W	320W		LE3M	120V	CLD
SWW360LEDK	360W	360W		LE3M	120V	CLD
SWW400LEDK	400W	400W		LE3M	120V	CLD
SWW480LEDK	480W	480W		LE3M	120V	CLD
SWW500LEDK	500W	500W		LE3M	120V	CLD
SWW600LEDK	600W	600W		LE3M	120V	CLD
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SWW1000LEDK	1000W	1000W		LE3M	120V	CLD
SWW1200LEDK	1200W	1200W		LE3M	120V	CLD
SWW1400LEDK	1400W	1400W		LE3M	120V	CLD
SWW1600LEDK	1600W	1600W		LE3M	120V	CLD
SWW1800LEDK	1800W	1800W		LE3M	120V	CLD
SWW2000LEDK	2000W	2000W		LE3M	120V	CLD
SWW2400LEDK	2400W	2400W		LE3M	120V	CLD
SWW2800LEDK	2800W	2800W		LE3M	120V	CLD
SWW3200LEDK	3200W	3200W		LE3M	120V	CLD
SWW3600LEDK	3600W	3600W		LE3M	120V	CLD
SWW4000LEDK	4000W	4000W		LE3M	120V	CLD
SWW4800LEDK	4800W	4800W		LE3M	120V	CLD
SWW5000LEDK	5000W	5000W		LE3M	120V	CLD
SWW6000LEDK	6000W	6000W		LE3M	120V	CLD
SWW7000LEDK	7000W	7000W		LE3M	120V	CLD
SWW8000LEDK	8000W	8000W		LE3M	120V	CLD
SWW9000LEDK	9000W	9000W		LE3M	120V	CLD
SWW10000LEDK	10000W	10000W		LE3M	120V	CLD
SWW12000LEDK	12000W	12000W		LE3M	120V	CLD
SWW14000LEDK	14000W	14000W		LE3M	120V	CLD
SWW16000LEDK	16000W	16000W		LE3M	120V	CLD
SWW18000LEDK	18000W	18000W		LE3M	120V	CLD
SWW20000LEDK	20000W	20000W		LE3M	120V	CLD
SWW24000LEDK	24000W	24000W		LE3M	120V	CLD
SWW28000LEDK	28000W	28000W		LE3M	120V	CLD
SWW32000LEDK	32000W	32000W		LE3M	120V	CLD
SWW36000LEDK	36000W	36000W		LE3M	120V	CLD
SWW40000LEDK	40000W	40000W		LE3M	120V	CLD
SWW48000LEDK	48000W	48000W		LE3M	120V	CLD
SWW50000LEDK	50000W	50000W		LE3M	120V	CLD
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SWW180000LEDK	180000W	180000W		LE3M	120V	CLD
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SWW320000LEDK	320000W	320000W		LE3M	120V	CLD
SWW360000LEDK	360000W	360000W		LE3M	120V	CLD
SWW400000LEDK	400000W	400000W		LE3M	120V	CLD
SWW480000LEDK	480000W	480000W		LE3M	120V	CLD
SWW500000LEDK	500000W	500000W		LE3M	120V	CLD
SWW600000LEDK	600000W	600000W		LE3M	120V	CLD
SWW700000LEDK	700000W	700000W		LE3M	120V	CLD
SWW800000LEDK	800000W	800000W		LE3M	120V	CLD
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SWW1000000LEDK	1000000W	1000000W		LE3M	120V	CLD
SWW1200000LEDK	1200000W	1200000W		LE3M	120V	CLD
SWW1400000LEDK	1400000W	1400000W		LE3M	120V	CLD
SWW1600000LEDK	1600000W	1600000W		LE3M	120V	CLD
SWW1800000LEDK	1800000W	1800000W		LE3M	120V	CLD
SWW2000000LEDK	2000000W	2000000W		LE3M	120V	CLD
SWW2400000LEDK	2400000W	2400000W		LE3M	120V	CLD
SWW2800000LEDK	2800000W	2800000W		LE3M	120V	CLD
SWW3200000LEDK	3200000W	3200000W		LE3M	120V	CLD
SWW3600000LEDK	3600000W	3600000W		LE3M	120V	CLD
SWW4000000LEDK	4000000W	4000000W		LE3M	120V	CLD
SWW4800000LEDK	4800000W	4800000W		LE3M	120V	CLD
SWW5000000LEDK	5000000W	5000000W		LE3M	120V	CLD
SWW6000000LEDK	6000000W	6000000W		LE3M	120V	CLD
SWW7000000LEDK	7000000W	7000000W		LE3M	120V	CLD
SWW8000000LEDK	8000000W	8000000W		LE3M	120V	CLD
SWW9000000LEDK	9000000W	9000000W		LE3M	120V	CLD
SWW10000000LEDK	10000000W	10000000W		LE3M	120V	CLD
SWW12000000LEDK	12000000W	12000000W		LE3M	120V	CLD
SWW14000000LEDK	14000000W	14000000W		LE3M	120V	CLD
SWW16000000LEDK	16000000W	16000000W		LE3M	120V	CLD
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SWW20000000LEDK	20000000W	20000000W		LE3M	120V	CLD
SWW24000000LEDK	24000000W	24000000W		LE3M	120V	CLD
SWW28000000LEDK	28000000W	28000000W		LE3M	120V	CLD



STANDARD CONSTRUCTION DETAIL #1-11 EROSION CONTROL BLANKET INSTALLATION

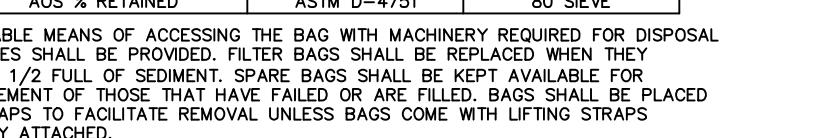
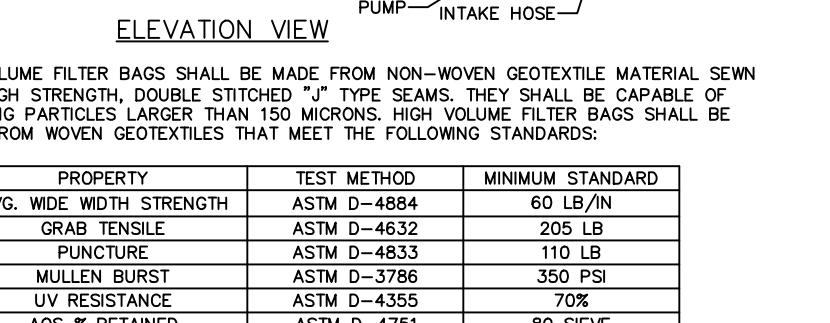
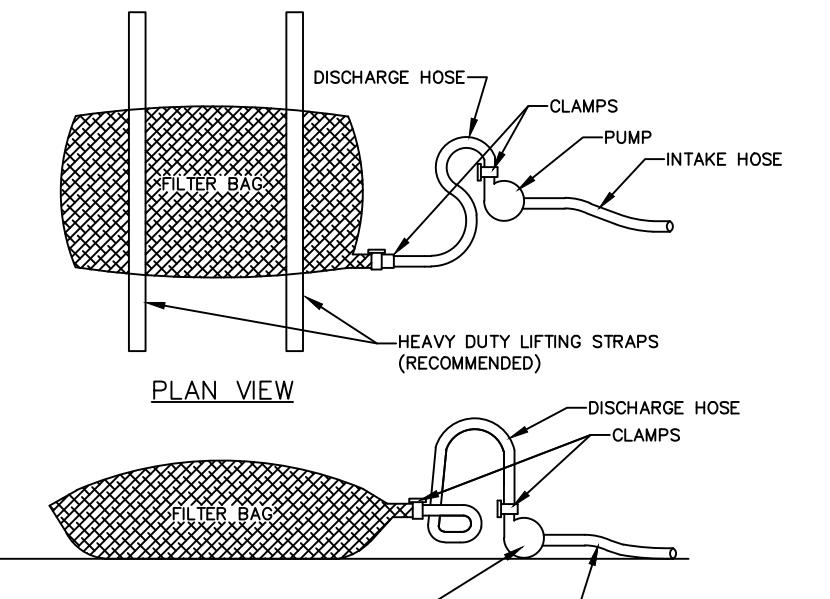
1. SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN.
2. DRAWDINGS PRIOR TO INSTALLING THE BLANKET.
3. PREPARE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE.
4. BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAT BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL.
5. THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
6. BLANKETED AREA SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR REMAINING AREA OF EXPOSURE IS STABILIZED TO A MINIMUM UNTIL 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGE OR DISPLACED BLANKETS SHALL BE REPAVED OR REPLACED WITHIN 4 CALENDAR DAYS.

STANDARD CONSTRUCTION DETAIL #4-16 FILTER BAG INLET PROTECTION

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

STANDARD CONSTRUCTION DETAIL #4-16 FILTER BAG INLET PROTECTION - TYPE M INLET

NOT TO SCALE



NOTES:
LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH STRENGTH, DOUBLE STITCHED "J" TYPE SEAMS. THEY SHALL BE CAPABLE OF TRAPPING PARTICLES LARGER THAN 150 MICRONS. HIGH VOLUME FILTER BAGS SHALL BE MADE FROM WOVEN GEOTEXTILES THAT MEET APPENDIX E OF THE PLAN.

TEST METHOD	TEST METHOD	MINIMUM STANDARD
Avg. Wgt. / in. width	ASTM D-4844	60 lb/in
GRAB TENSILE	ASTM D-4632	205 LB
PUNCTURE	ASTM D-4833	110 LB
MULLEN BURST	ASTM D-3786	350 PSI
UV STABIL.	ASTM D-4754	70%
ADS % RETAINED	ASTM D-4754	80% SEIVE

A SUITABLE MEASUREMENT OF THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE KEPT AVAILABLE FOR BECOMING 1/2 FULL OF SEDIMENT. BAGS SHALL BE KEPT AVAILABLE FOR REPAIRS IF NEEDED. FILTER BAGS SHALL HAVE FABRIC BAG STRAPS PLACED ON STRAPS TO FACILITATE REMOVAL IF NOT BAGS CAN BE CUT WITH LIFTING STRAPS ALREADY ATTACHED.

BAGS SHALL BE LOCATED IN WELL-VENTILATED (GRASSY) AREA, AND DISCHARGE ONTO UNDISTURBED OR ERODED AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON OR IN WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GROUNDWATER IS AVAILABLE.

NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO PROTECT IT FROM CONTACT WITH THE GROUND.

NO DOWNSLOPES (SEDIMENT BARRIER) IS REQUIRED FOR MOST INSTALLATIONS. COMPOST OR FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HO OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GROUNDWATER IS AVAILABLE.

THE PUMPING RATE FOR FILTER BAGS SHALL BE RECALCULATED IN THE MANNER SPECIFIED BY THE MANUFACTURER FOR THIS PURPOSE.

THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER. WHICHEVER IS LESS, PUMP INTAKES SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

STANDARD CONSTRUCTION DETAIL #3-16 PUMPED WATER FILTER BAG

(NOT TO SCALE)

STANDARD CONSTRUCTION DETAIL #4-1 COMPOST FILTER SOCK

(NOT TO SCALE)

NOTES:
SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL MANUAL. THE FABRIC SHALL MEET THE STANDARDS OF TABLE 4.2 OF THE PA DEP EROSION CONTROL MANUAL.

COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE, BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAX BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY CONSTRUCTION ACTIVITY SHALL BE AS SPECIFIED BY THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA.

TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS.

ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN PLAN.

COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. FILTER SOCKS SHALL BE REPAVED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION.

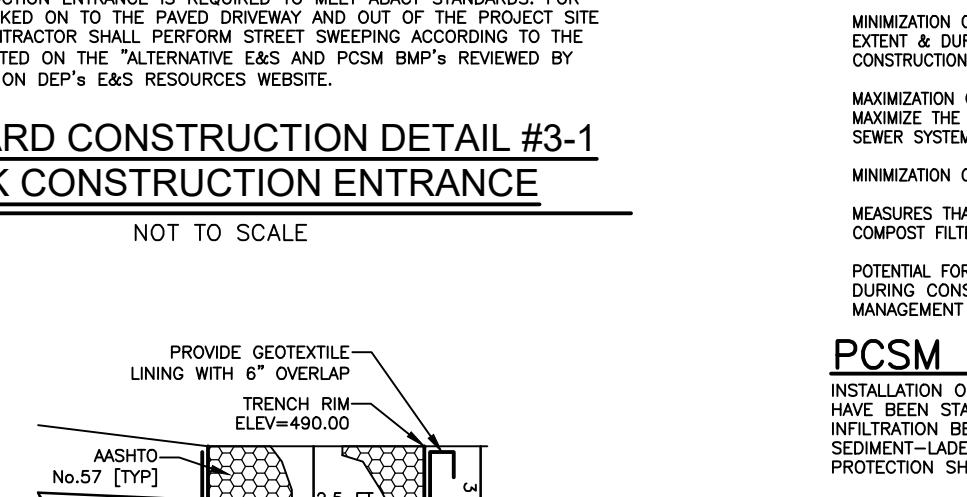
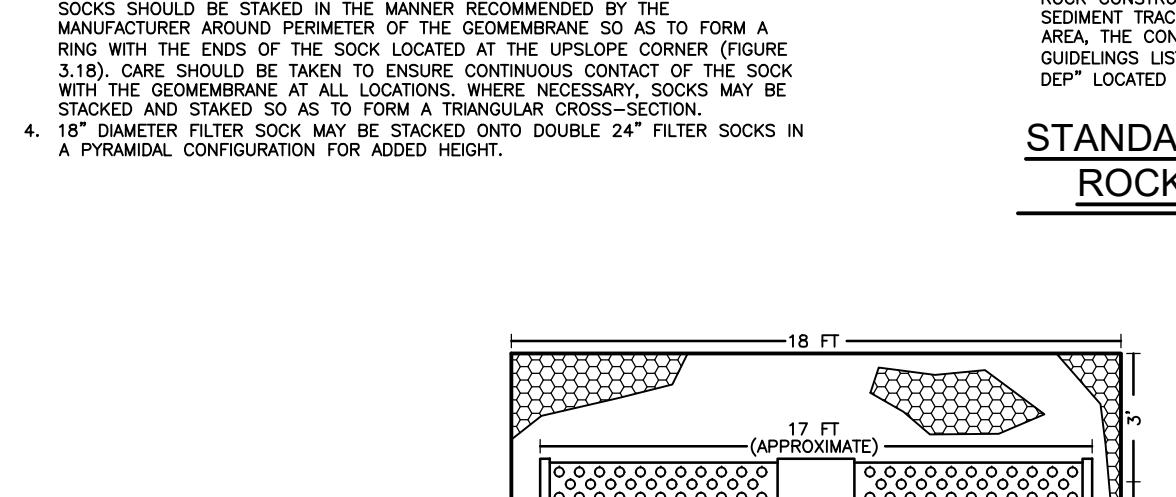
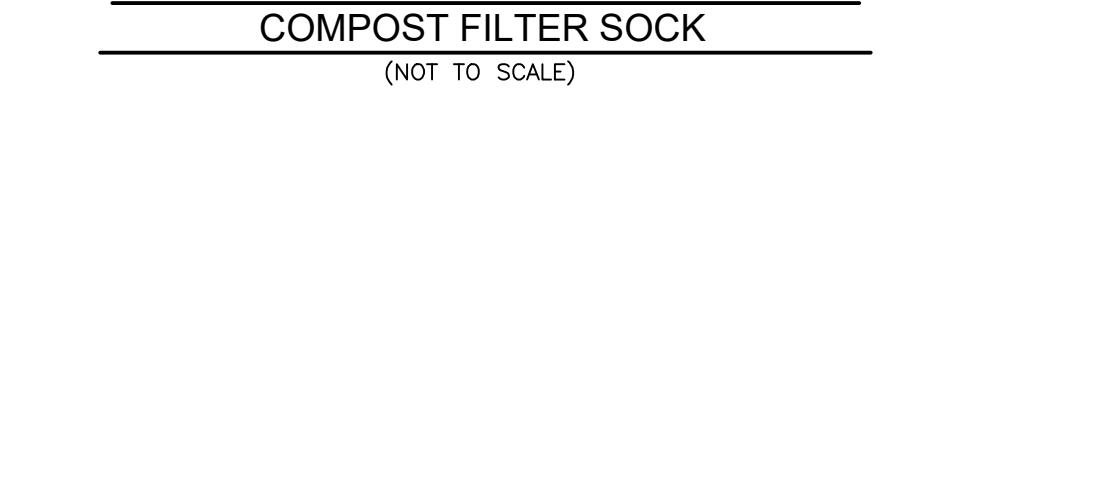
BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED AND THE BARRIER SHALL BE RECALCULATED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER. WHICHEVER IS LESS, PUMP INTAKES SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

STANDARD CONSTRUCTION DETAIL #3-1 ROCK CONSTRUCTION ENTRANCE

(NOT TO SCALE)



NOTES:
1. THE PERFORATED PIPE / STONE TRENCH LEVEL SPREADER SHALL BE CONSTRUCTED SO THAT THERE IS A MINIMUM OF ONE FOOT (1') OF COVER OVER THE TOP OF THE PIPE.

2. A STANDARD "T" JUNCTION SHALL BE USED TO CONNECT THE TWO PERFORATED LEVEL SPREADER PIPES TO THE STORMWATER FACILITY OUTLET PIPE AS SHOWN.

NOTES:
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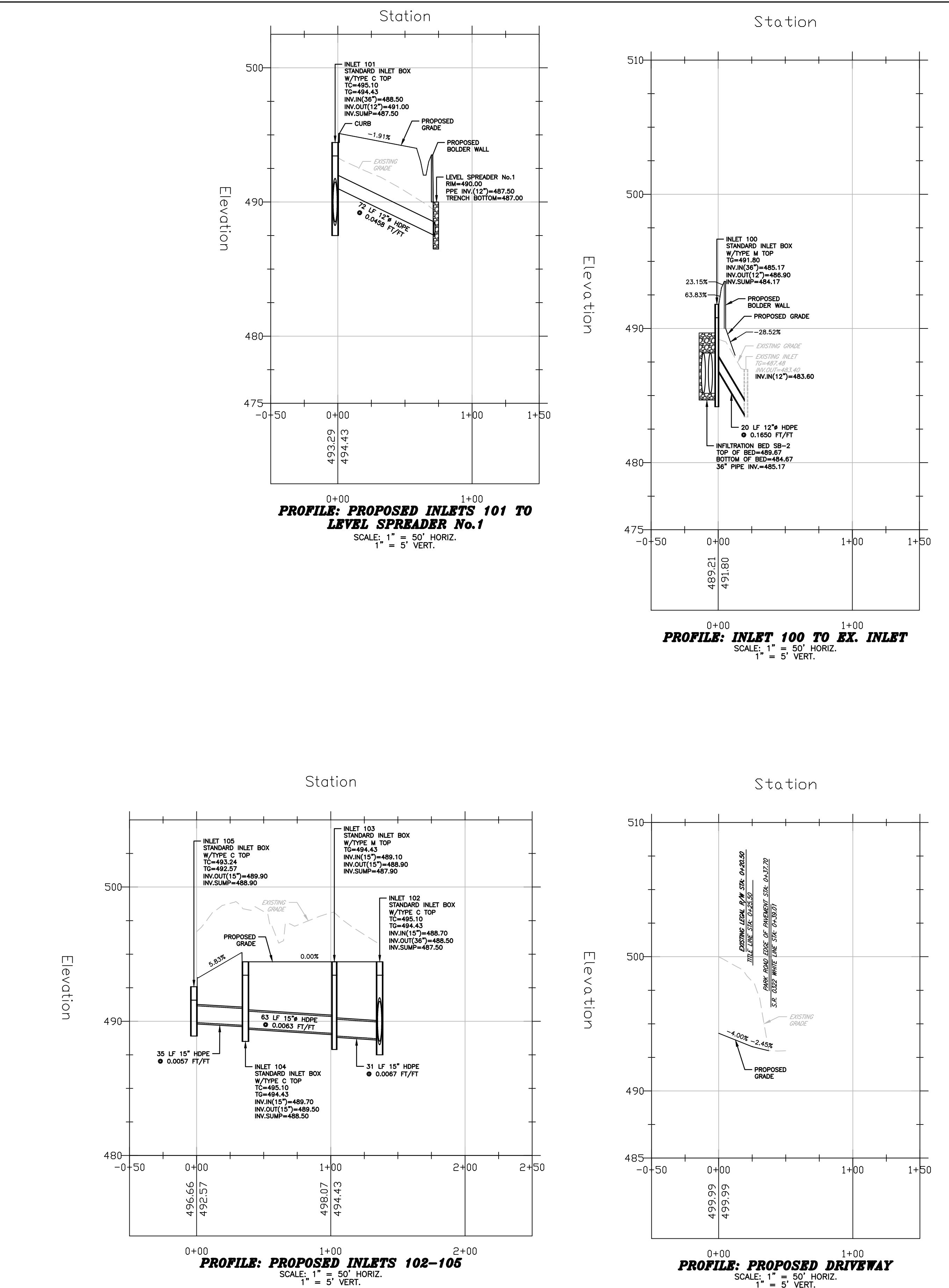
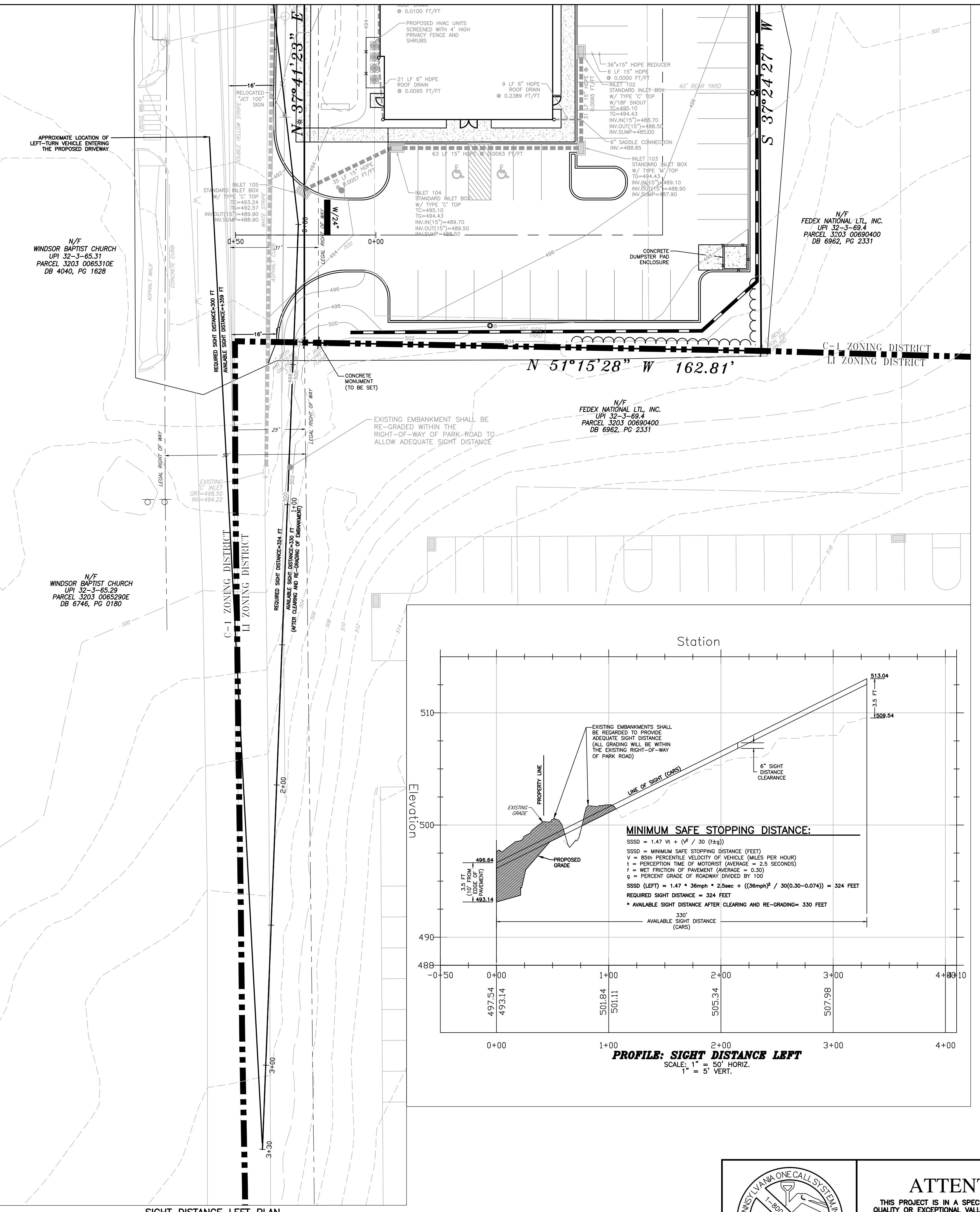
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PENNSYLVANIA ONE CALL
(PURSUANT TO ACT 287, 172, 38 AND ALL OTHER APPLICABLE AMENDMENTS) COMMONWEALTH
ENGINEERS, INC. DOES NOT GUARANTEE THE ACCURACY OF THE LOCATIONS FOR EXISTING SUBSURFACE
UTILITY LINES, STRUCTURES, ETC., SHOWN ON THE PLANS, NOR DOES COMMONWEALTH ENGINEERS, INC.
GUARANTEE THAT ALL SUBSURFACE UTILITY LINES, STRUCTURES, ETC., ARE SHOWN.

THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF ALL SUBSURFACE UTILITY LINES,
STRUCTURES, ETC., BEFORE THE START OF WORK. [TELEPHONE: (800) 242-1776]

TAX PARCEL No.: 32-04-37
UPI No.: 3204 0037 0000

GROUND UTILITY USERS

(PURSUANT TO ACT 287, 172, 38 AND ALL OTHER APPLICABLE AMENDMENTS) COMMONWEALTH
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COMPANY	COMCAST CABLE COMMUNICATIONS 1004 CORNERSTONE BLVD DOWNTOWN, PA. 19335 CONTACT: LEE MCGARRITY EMAIL: lee_mcgarrity@cable.comcast.com	VERIZON PENNSYLVANIA INC 6TH FLOOR 900 RACE STREET PHILADELPHIA, PA. 19107 CONTACT: SUZETTE WALKER EMAIL: suzette.e.walker@verizon.com
6	UGI UTILITIES INC 225 MORGANTOWN ROAD READING, PA. 19611 CONTACT: ENGINEERING EMAIL: kzielaskowski@ugi.com	

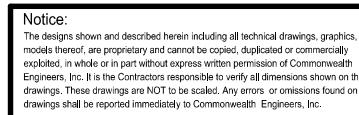
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BECAUSE THIS PROJECT IS IN A SPECIAL
HIGH QUALITY OR EXCEPTIONAL VALUE
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THIS PROJECT IS IN A SPECIALLY PROTECTED HIGH
QUALITY OR EXCEPTIONAL VALUE WATERSHED; EXTREME
CARE SHOULD BE EXERCISED IN ALL DISTURBANCE
ACTIVITIES TO PREVENT DEGRADATION TO THE WATERS OF
THE COMMONWEALTH.

BECAUSE THIS PROJECT IS IN A SPECIALLY PROTECTED HIGH QUALITY OR EXCEPTIONAL VALUE WATERSHED, UPON COMPLETION OR TEMPORARY CESSION OF EARTH DISTURBANCE ACTIVITIES, THE PROJECT SITE MUST BE IMMEDIATELY STABILIZED WITH THE APPROPRIATE TEMPORARY OR PERMANENT STABILIZATION

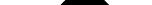
GRAPHIC SCALE 1" = 20'



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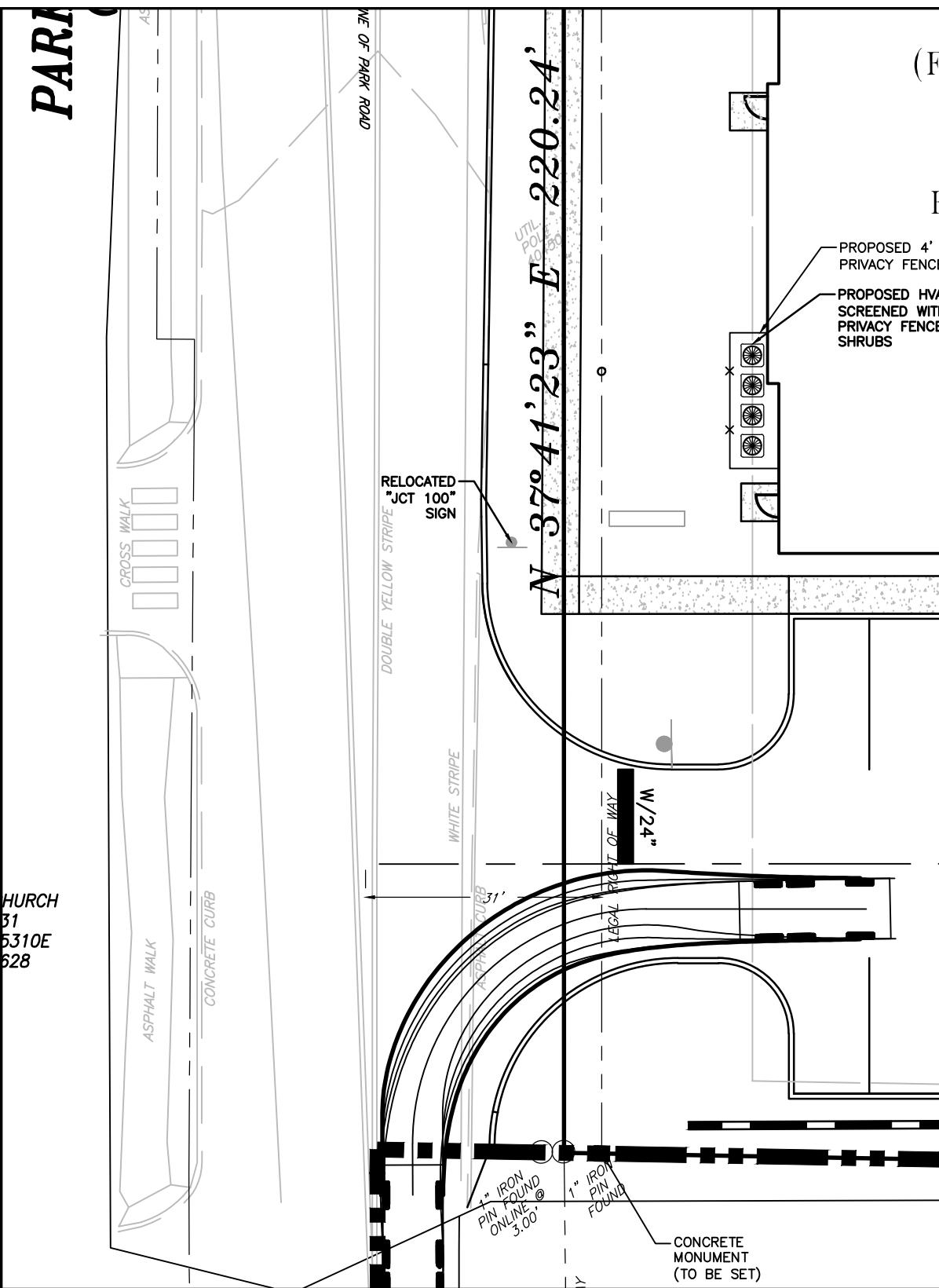
COMMONWEALTH ENGINEERS
REGISTERED PROFESSIONAL
VICTOR H. KIRK
ENGINEER
No. 041303
AIA



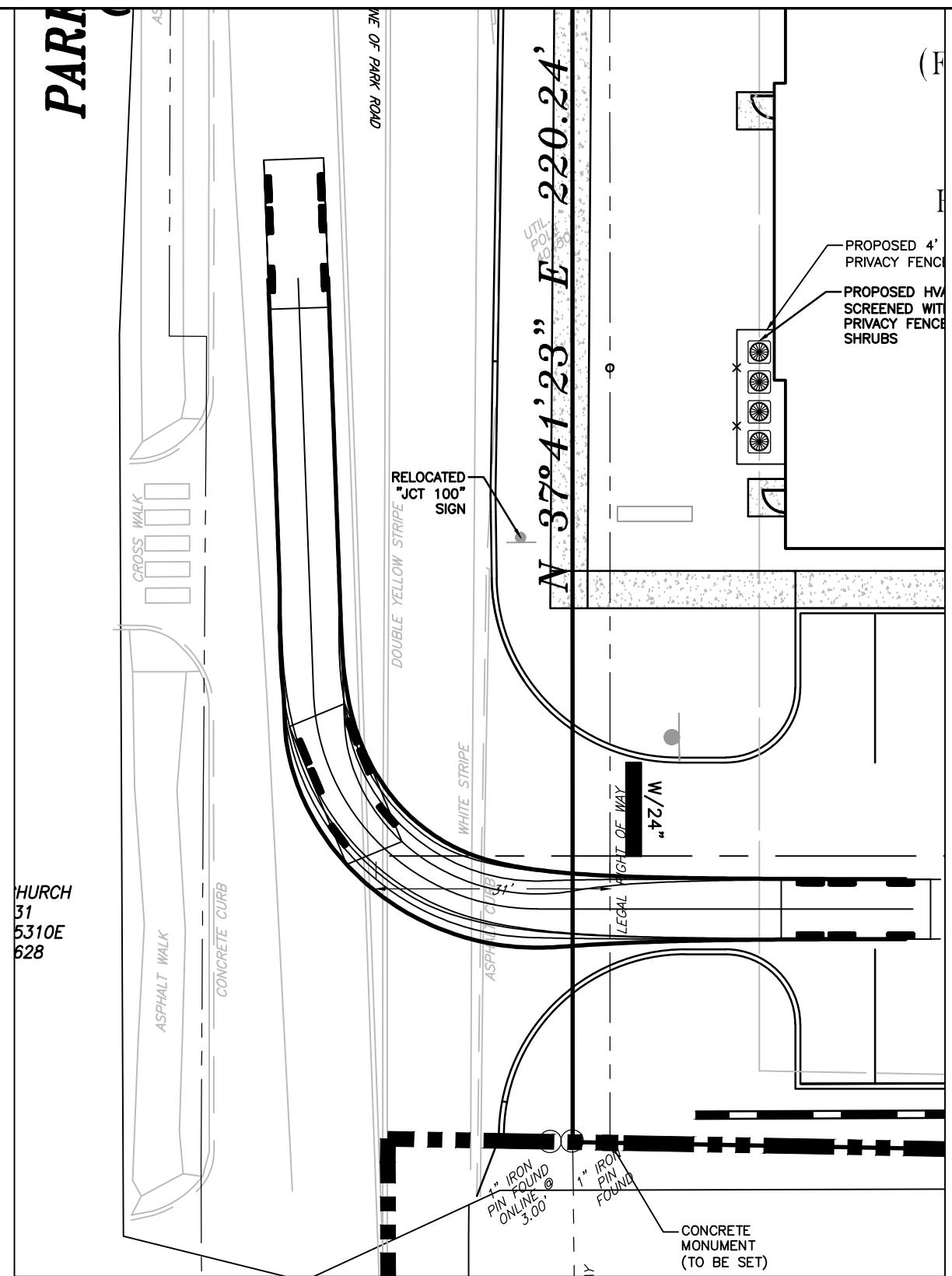
COMMONWEALTH ENGINEERS, INC.

114 EAST LANCASTER AVENUE ~ SECOND FLOOR
DOWNTONTOWN PA 19335

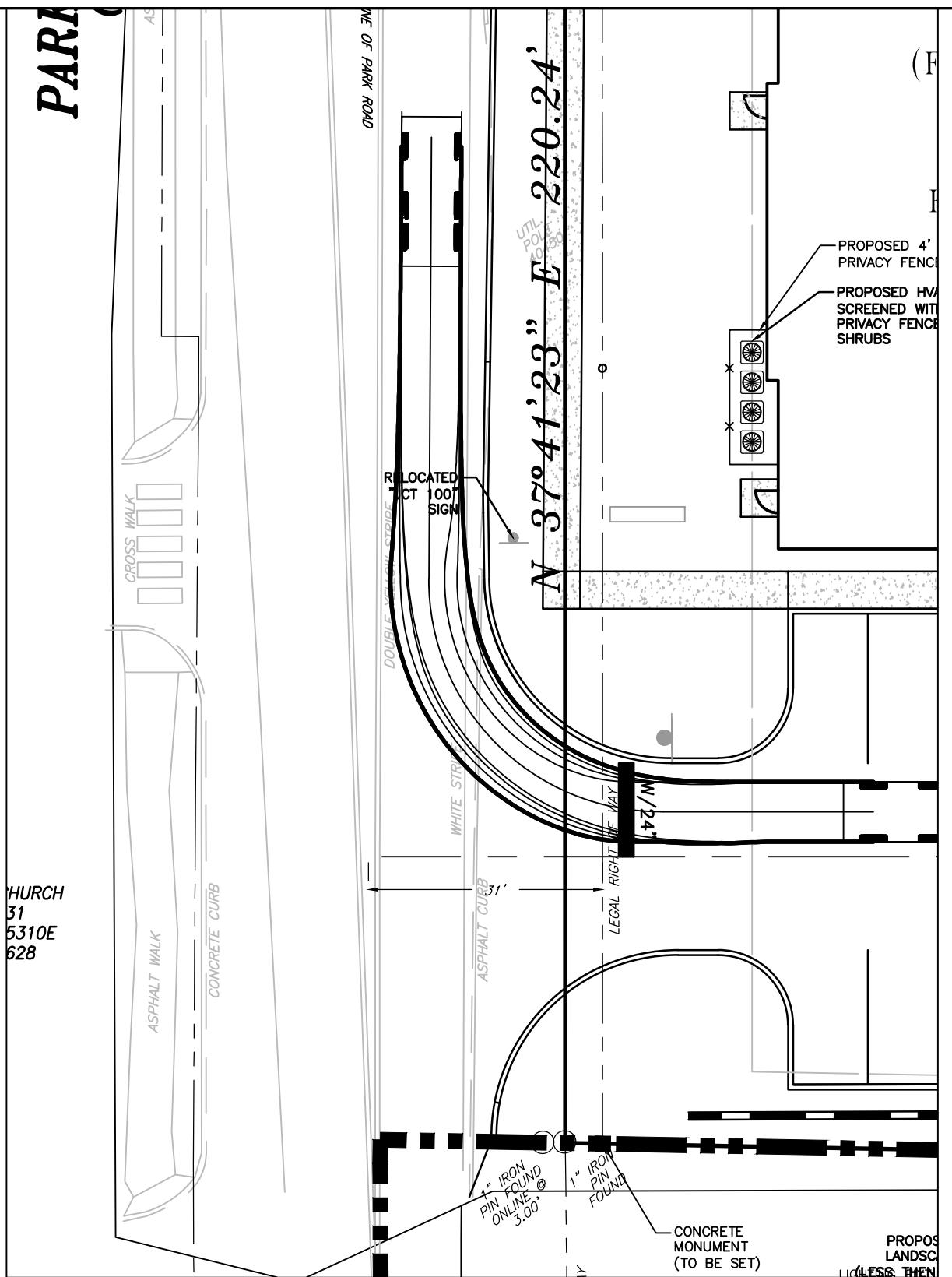
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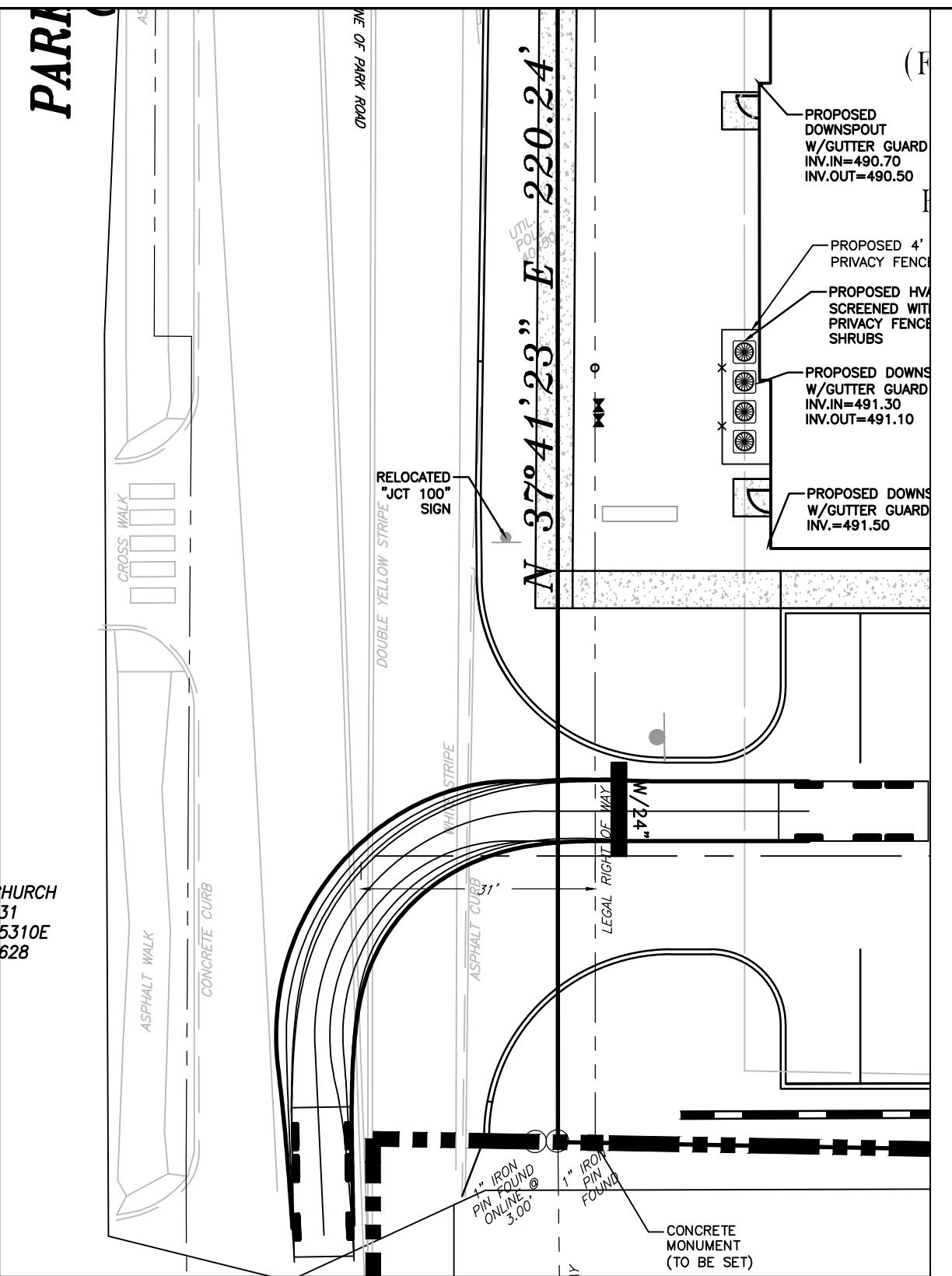
DELIVERY TRUCK ENTERING SITE TURNING RIGHT



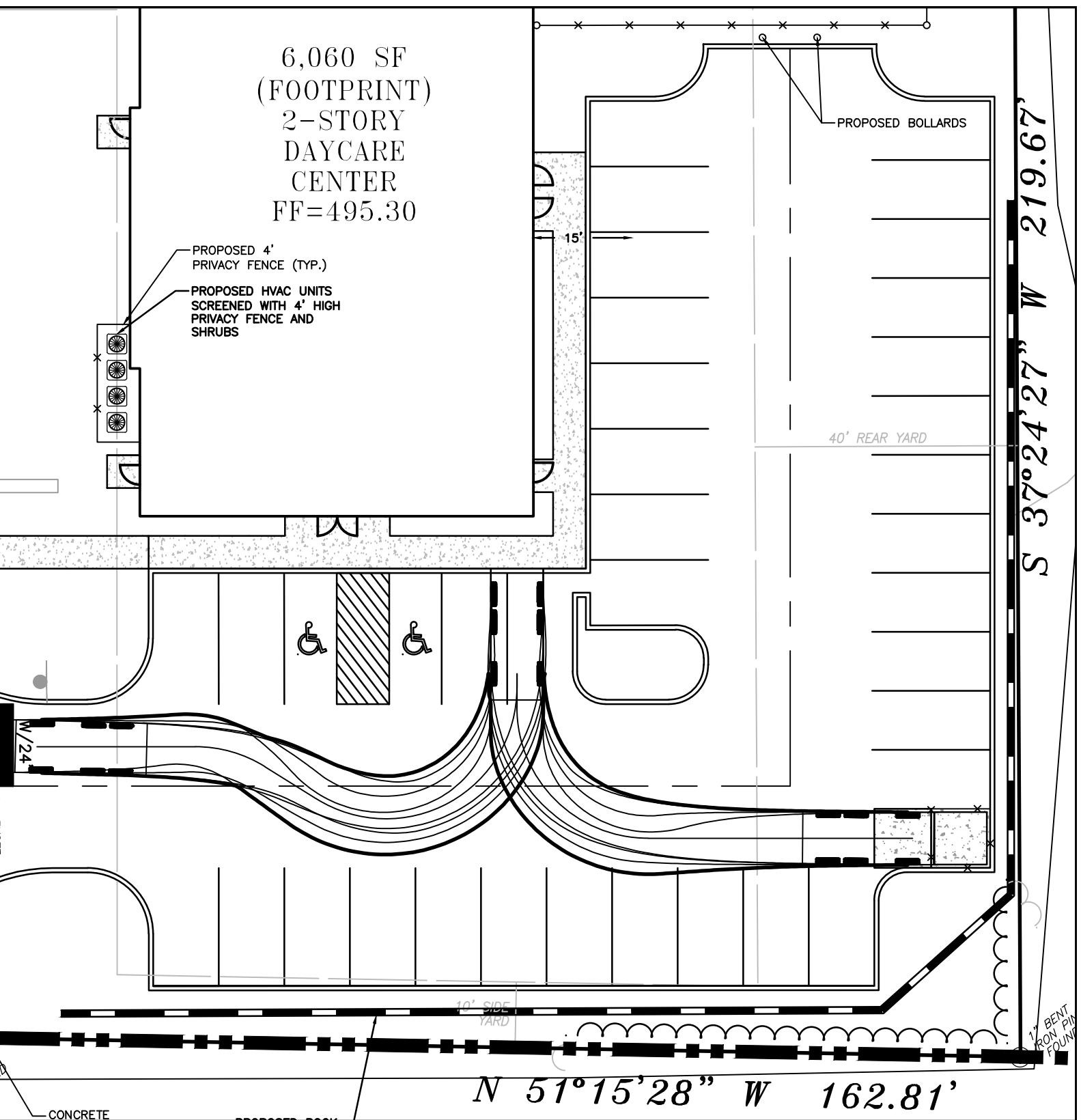
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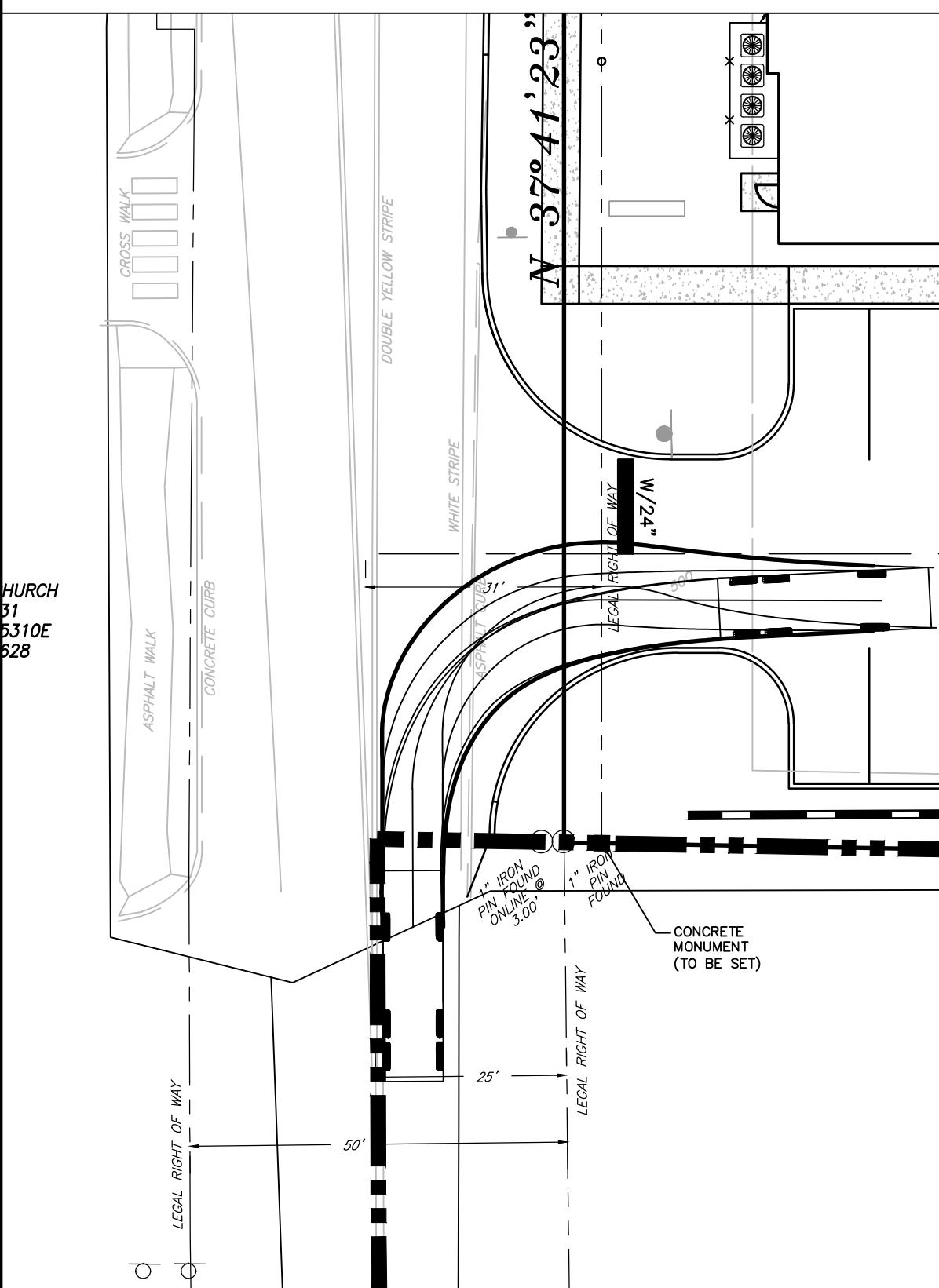
DELIVERY TRUCK LEAVING SITE TURNING RIGHT



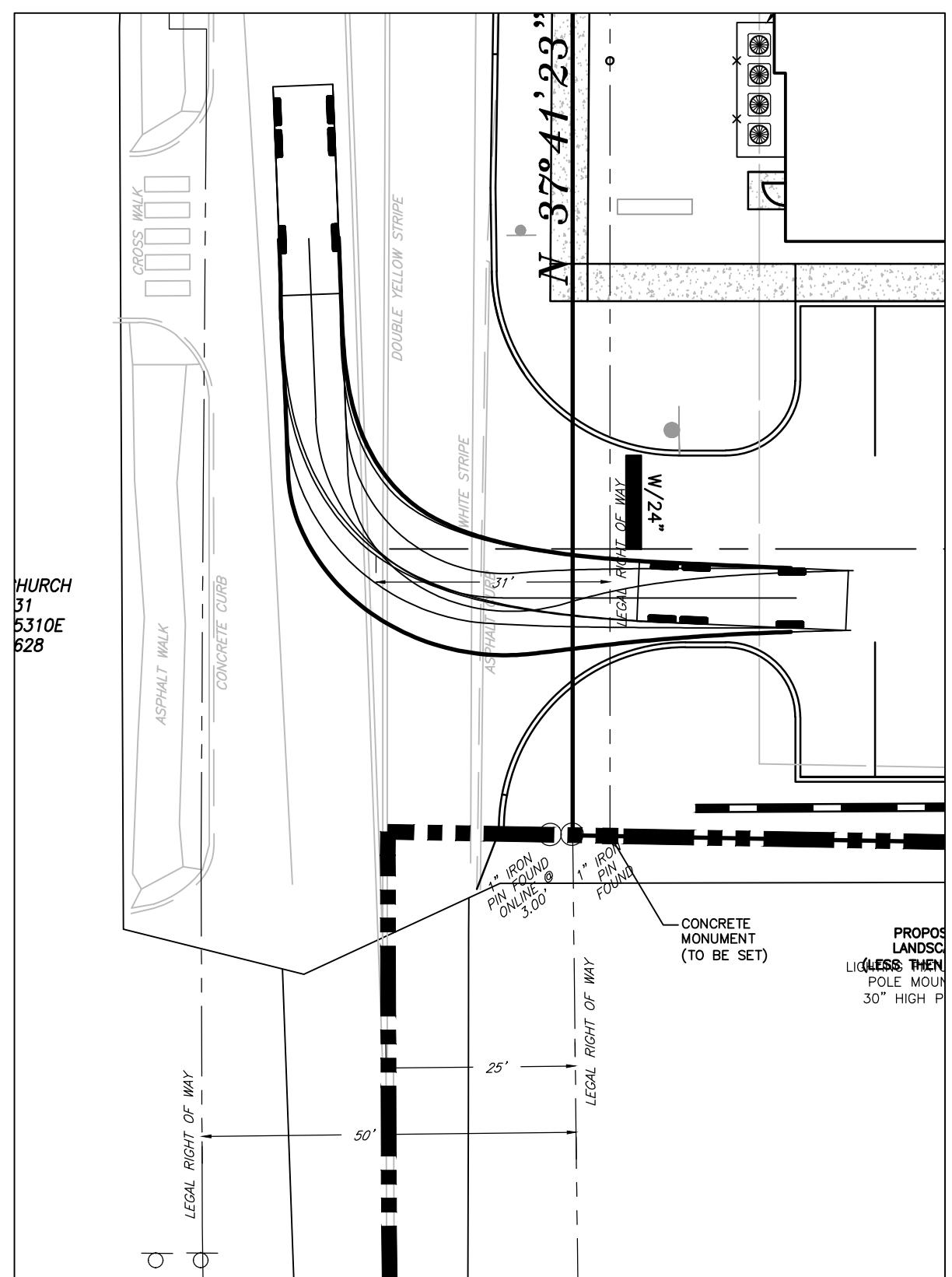
DELIVERY TRUCK LEAVING SITE TURNING LEFT



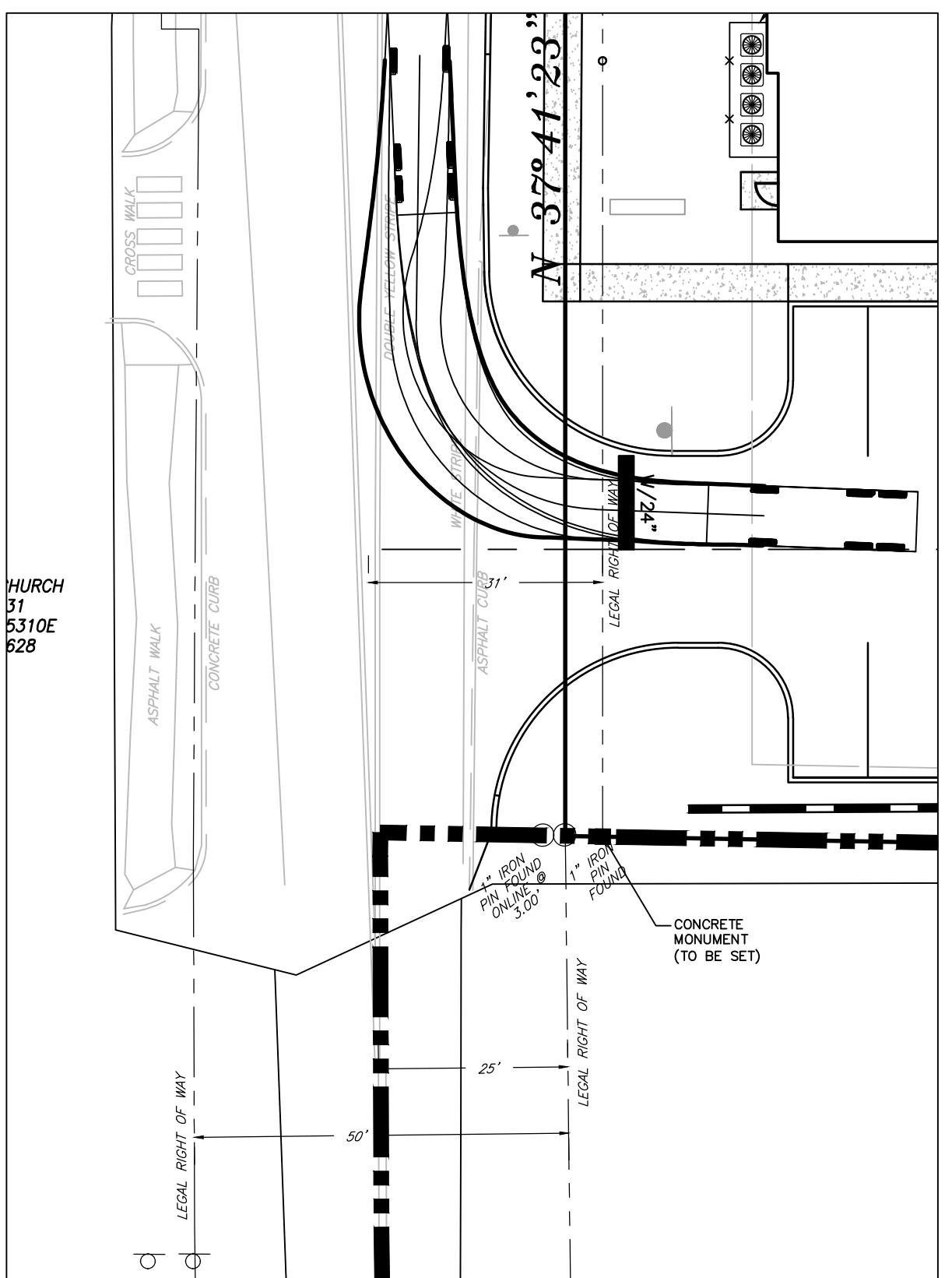
DELIVERY TRUCK TURN AROUND MOVEMENT



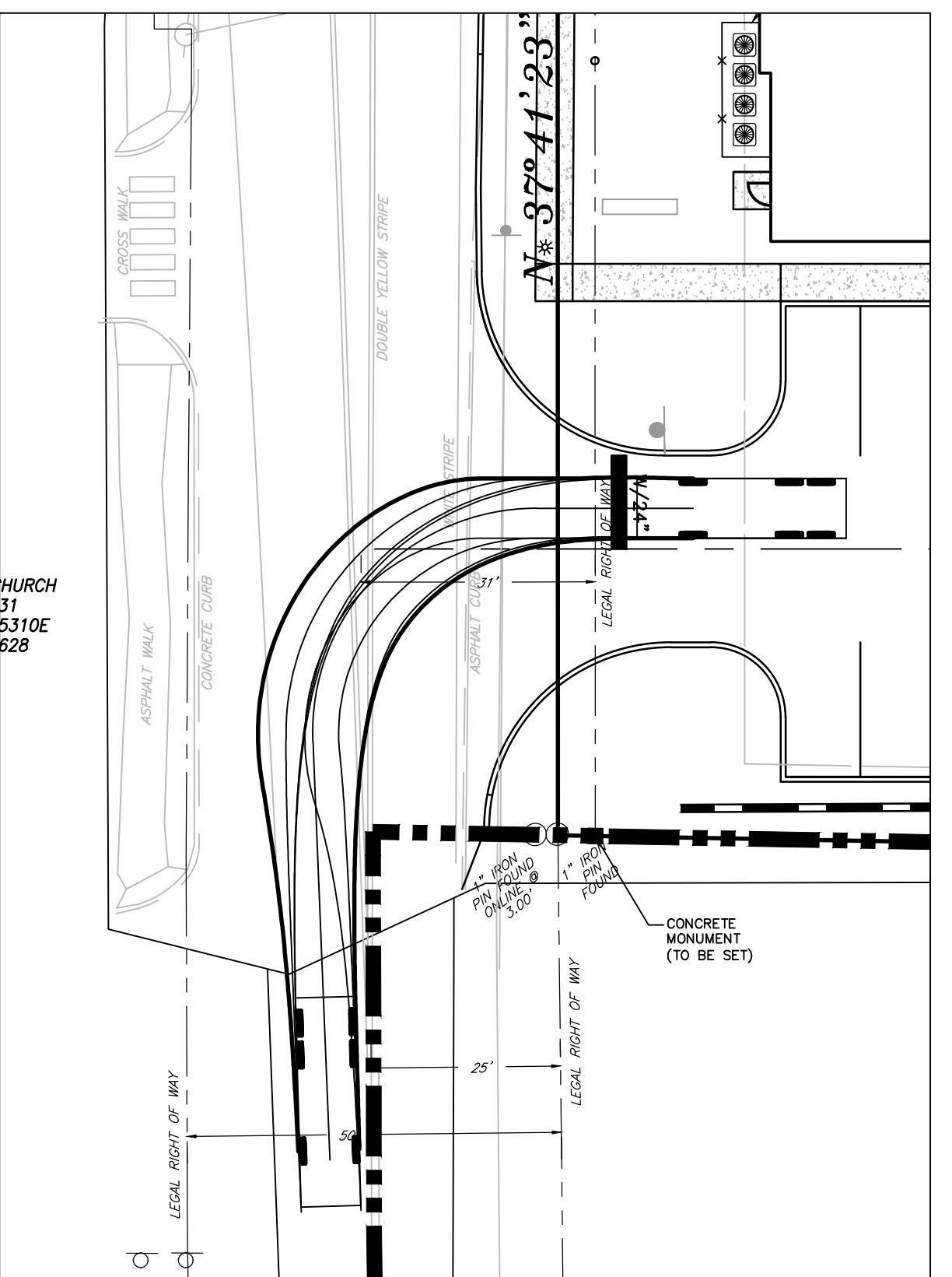
TRASH TRUCK ENTERING SITE TURNING RIGHT



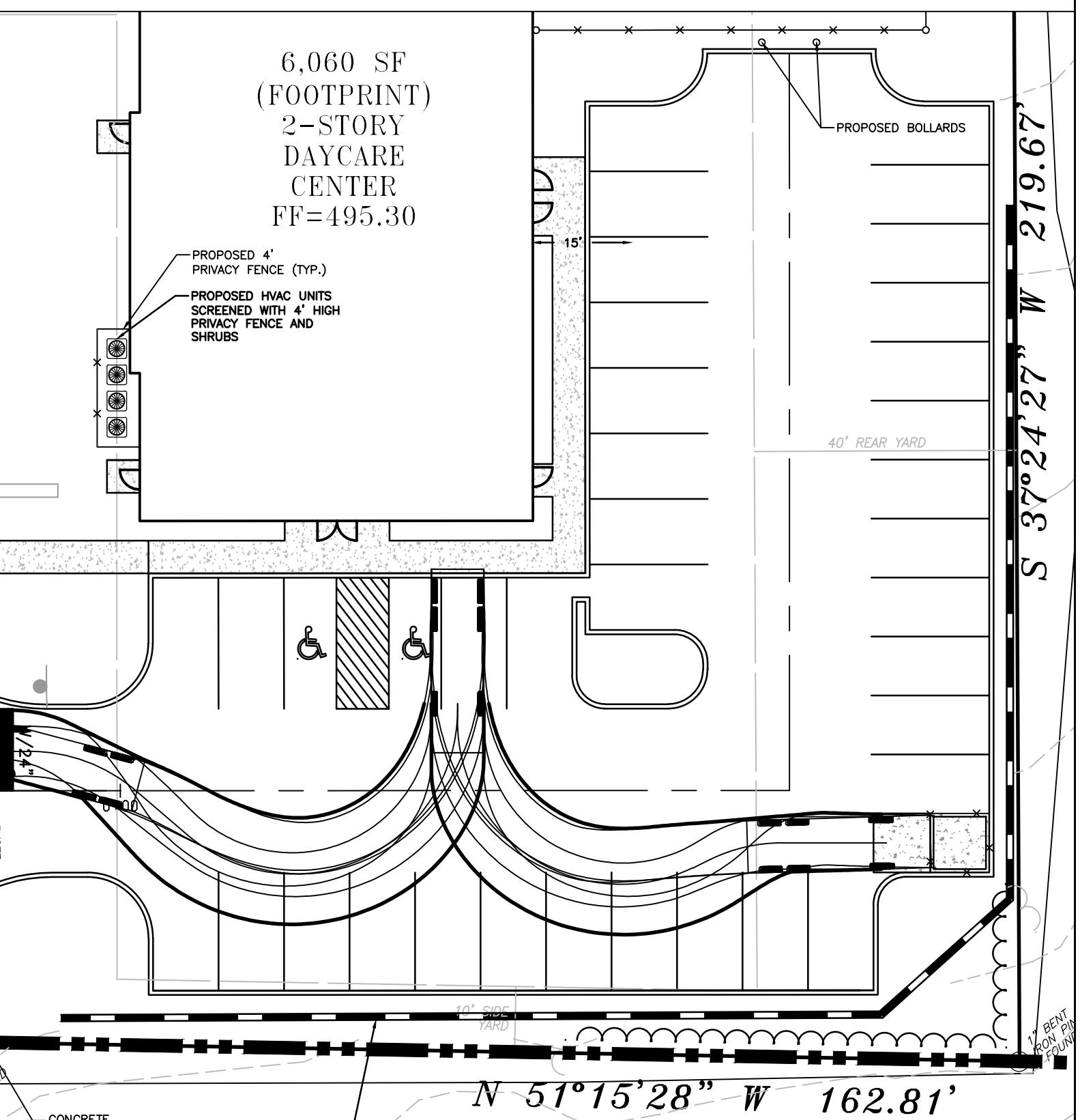
TRASH TRUCK ENTERING SITE TURNING LEFT



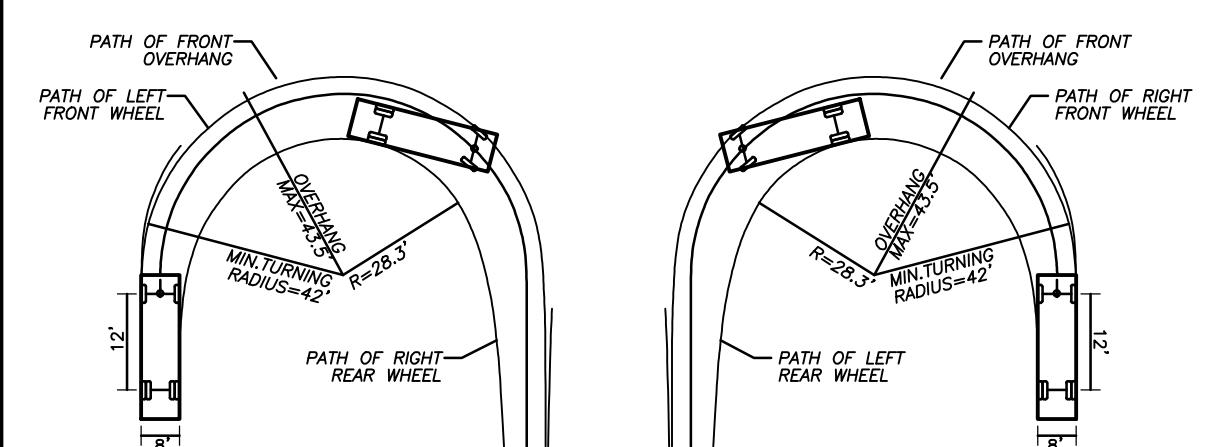
TRASH TRUCK LEAVING SITE TURNING RIGHT



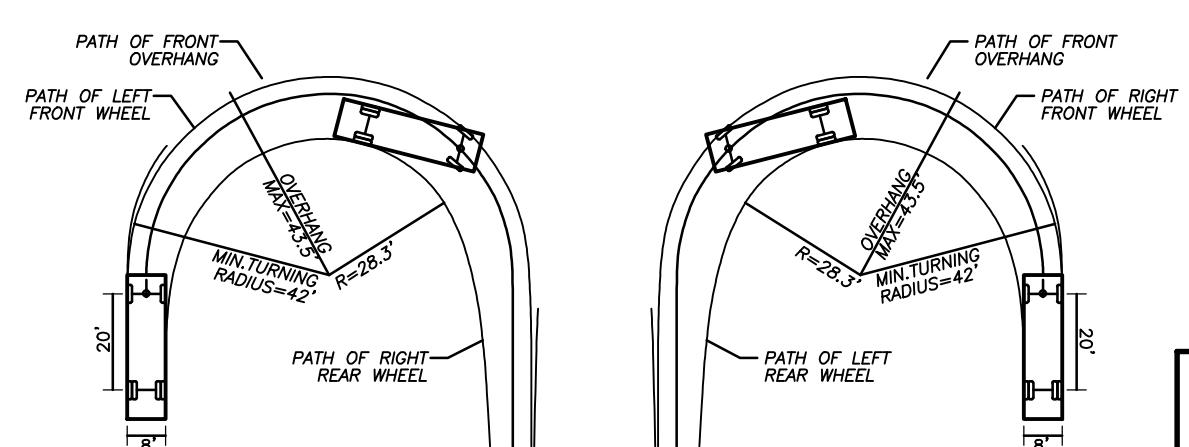
TRASH TRUCK LEAVING SITE TURNING LEFT



TRASH TRUCK TURN AROUND MOVEMENT



SINGLE UNIT TRUCK TURNING RADIUS TEMPLATE (SU DESIGN VEHICLE)

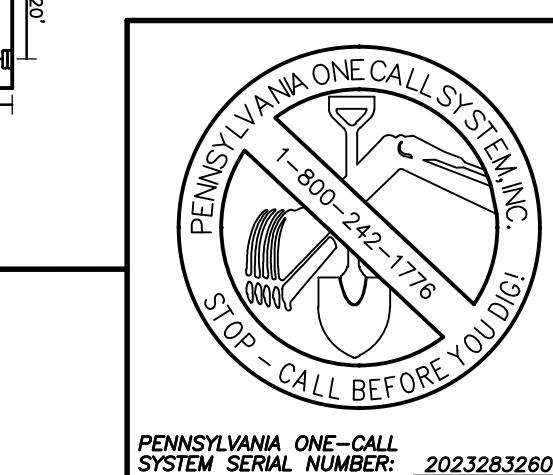


FRONT LOAD TRASH TRUCK TURNING RADIUS SPECIFICATIONS

PENNSYLVANIA ONE CALL
PURSUANT TO ACT 170, 38 AND ALL OTHER APPLICABLE AMENDMENTS, COMMONWEALTH
ENGINEERS, INC. DOES NOT GUARANTEE THE ACCURACY OF THE LOCATIONS FOR EXISTING SUBSURFACE
UTILITIES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXACT LOCATIONS OF ALL
SUBSURFACE UTILITY LINES, STRUCTURES, ETC. ARE SHOWN.
THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF ALL SUBSURFACE UTILITY LINES,
STRUCTURES, ETC. AND DETERMINE THE WORK TO BE DONE.
TAX PARCEL NO.: 32-04-37
UPI No.: 3204 0037 0000

UNDERGROUND UTILITY USERS
UPPER UCHLАН MUNICIPAL AUTHORITY
PO BOX 137, 1940
CONTACT: MATTHEW BROWN
COMCAST CABLE COMMUNICATIONS
1004 CORNERSTONE BLVD
CONTACT: STEVE PIZZI
VERIZON PENNSYLVANIA INC
1004 CORNERSTONE BLVD
CONTACT: LEE MCGARRY
PHILADELPHIA WATER
1000 W 18TH ST
CONTACT: WAYNE VINCENT
UGI UTILITIES INC
1000 W 18TH ST
CONTACT: WAYNE VINCENT
READING, PA 19601
EMAIL: wayne.vincent@ugiu.com

AQUA-PENNSYLVANIA WATER COMPANY
762 LANCASTER AVENUE
CONTACT: STEVE PIZZI
COMCAST CABLE COMMUNICATIONS
1004 CORNERSTONE BLVD
CONTACT: LEE MCGARRY
VERIZON PENNSYLVANIA INC
1004 CORNERSTONE BLVD
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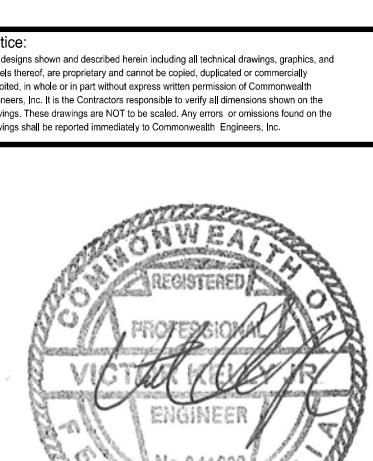
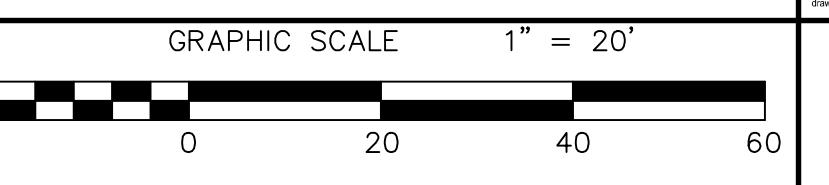


PENNSYLVANIA ONE CALL SYSTEM
SERIAL NUMBER: 20232832601

ATTENTION!

THIS PROJECT IS IN A SPECIALLY PROTECTED HIGH
QUALITY OR EXCEPTIONAL VALUE WATERSHED; EXTREME
CARE SHOULD BE EXERCISED IN ALL DISTURBANCE
ACTIVITIES TO PREVENT DEGRADATION TO THE WATERS OF
THE COMMONWEALTH.

BECAUSE THIS PROJECT IS IN A SPECIALLY PROTECTED
HIGH QUALITY OR EXCEPTIONAL VALUE WATERSHED, UPON
COMPLETION OR TEMPORARY CESSION OF EARTH
DISTURBANCE ACTIVITIES, THE PROJECT SITE MUST BE
IMMEDIATELY STABILIZED WITH THE APPROPRIATE
TEMPORARY OR PERMANENT STABILIZATION



UPPER UCHLАН TOWNSHIP
COMMONWEALTH OF
PENNSYLVANIA
REGISTERED
PROFESSIONAL
ENGINEER
No. 041503
114 EAST LANCASTER AVENUE ~ SECOND FLOOR
DOWNTOWN, PA 19335
TELEPHONE: (610) 518-9003 ~ FAX: (610) 518-9004
COMENG@CEI-1.COM

COMMONWEALTH ENGINEERS, INC.
114 EAST LANCASTER AVENUE ~ SECOND FLOOR
DOWNTOWN, PA 19335
TELEPHONE: (610) 518-9003 ~ FAX: (610) 518-9004
COMENG@CEI-1.COM

DATE: DECEMBER 4, 2023
SCALE: AS SHOWN
SURVEY: ----
DRAWN: MDC
CHECKED: VK
DRAWING NO. 202317-1(C)
OF: 16